

# Agenda

## Planning Committee

Wednesday, 14 April 2021 at 7.30 pm

Remote meeting via video link



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### Members:

#### S. Parnall (Chairman)

M. S. Blacker  
J. S. Bray  
P. Harp  
J. Hudson  
F. Kelly  
J. P. King  
S. A. Kulka

S. McKenna  
K. Sachdeva  
C. Stevens  
R. S. Turner  
S. T. Walsh  
C. T. H. Whinney  
Vacant

### Substitutes:

<b>Conservatives:</b>	N. C. Moses, C. M. Neame, J. Paul and S. J. G. Rickman
<b>Residents Group:</b>	G. Adamson, R. J. Feeney, R. Harper and N. D. Harrison
<b>Green Party:</b>	J. C. S. Essex and S. Sinden
<b>Liberal Democrats</b>	D. A. Ross

**Mari Roberts-Wood**  
Interim Head of Paid Service

For enquiries regarding this agenda;

Contact: 01737 276182

Email: [democratic@reigate-banstead.gov.uk](mailto:democratic@reigate-banstead.gov.uk)



- 1. Minutes** (Pages 7 - 10)  
To confirm as a correct record the Minutes of the previous meeting.
  - 2. Apologies for absence**  
To receive any apologies for absence.
  - 3. Declarations of interest**  
To receive any declarations of interest.
  - 4. Addendum to the agenda** (To Be Tabled)  
To note the addendum tabled at the meeting which provides an update on the agenda of planning applications before the Committee.
- PLANNING APPLICATIONS:**
- NOTES:**
1. The order in which the applications will be considered at the meeting may be subject to change.
  2. Plans are reproduced in the agenda for reference purposes only and are not reproduced to scale. Accordingly dimensions should not be taken from these plans and the originals should be viewed for detailed information. Most drawings in the agenda have been scanned, and reproduced smaller than the original, thus affecting image quality.
- To consider the following applications :
- 5. 20/02581/F - 94 Brighton Road Horley Surrey RH6 7JQ** (Pages 11 - 34)  
Extension, alteration and addition of residential accommodation to the existing building on 94 Brighton Road to provide 6 self contained flats.
  - 6. 20/02840/HHOLD - 9 Garden Close, Banstead, SM7 2QB** (Pages 35 - 42)  
Proposed two-storey side extension.
  - 7. 19/02559/F - The Epiphany House, Mansfield Drive, Merstham, Redhill, RH1 3JP** (Pages 43 - 76)

The demolition of the existing properties and the erection of ten houses, comprising 2 x 2 bed and 4 x 3 bed terraced houses, 2 x 3 bed semi-detached and 2 x 4 bed detached houses, with associated access and parking. As amended on 20/01/2020, 12/02/2020 and on 16/03/2020.

- 8. 19/0986/F - Land at The Croft, Meath Green Lane, Horley, RH6 8HZ** (Pages 77 - 118)

Erection of 10 dwellings with site access, private amenity space, garaging, parking and access to neighbouring development. As amended on 12/07/2019, 21/04/2020, 01/12/2020, 22/12/2020 and on 11/03/2021.

- 9. 20/02510/F - 10 West Drive, and land to the rear of 9,11, and 12, West Drive, Burgh Heath, KT20 5PA** (Pages 119 - 166)

Demolition of dwelling at 10 West Drive, and erection of 7 residential dwellings on land to the rear of 9-12 West Drive, associated landscaping, parking, access onto West Drive, and associated ancillary work. As amended on 05/02/2021.

- 10. 21/00050/F - 129 and 131 Bletchingley Road, Merstham** (Pages 167 - 176)

Proposed vehicle crossovers. As amended on 18/02/2021.

- 11. 21/00388/HHOLD - 20 Cheyne Walk, Horley, RH6 7PF** (Pages 177 - 188)

Single storey flat roof extension onto detached bungalow, extension will span the whole width of the property and extend out to a maximum of 4 metres.

- 12. Any other urgent business**

To consider any item(s) which, in the opinion of the Chairman, should be considered as a matter of urgency.





### **Our meetings**

As we would all appreciate, our meetings will be conducted in a spirit of mutual respect and trust, working together for the benefit of our Community and the Council, and in accordance with our Member Code of Conduct. Courtesy will be shown to all those taking part.



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**Notice is given** of the intention to hold any part of this meeting in private for consideration of any reports containing "exempt" information, which will be marked accordingly.

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## **BOROUGH OF REIGATE AND BANSTEAD**

### **PLANNING COMMITTEE**

Minutes of a meeting of the Planning Committee held remotely on 17 March 2021 at 7.30 pm.

Present: Councillors S. Parnall (Chairman), M. S. Blacker (Vice-Chair), J. S. Bray, P. Harp, J. Hudson, F. Kelly, J. P. King, S. A. Kulka, S. McKenna, K. Sachdeva, C. Stevens, R. S. Turner, S. T. Walsh and C. T. H. Whinney.

Also present: Councillor Harrison .

#### **113. MINUTES**

**RESOLVED** that the minutes of the previous meeting held on 17 February 2021 be approved as a correct record.

#### **114. APOLOGIES FOR ABSENCE**

There were no apologies for absence.

#### **115. DECLARATIONS OF INTEREST**

Councillor Walsh declared a pecuniary interest on item 5, 16 Downs Wood, Epsom Downs, as this was an application belonging to his client. Councillor Walsh was not present at the meeting for the duration of the item.

#### **116. ADDENDUM TO THE AGENDA**

**RESOLVED** that the addendum be noted.

#### **117. 20/01369/F - 16 DOWNS WOOD AND REAR OF 37, 39, 41, 43, 45 & 47 YEW TREE BOTTOM ROAD, EPSOM DOWNS, EPSOM, SURREY**

The Committee considered an application at 16 Downs Wood and Rear Of 37, 39, 41, 43, 45 & 47 Yew Tree Bottom Road, Epsom Downs, Epsom for the demolition of 16 Downs Wood and the erection of 8 dwellings on land to the rear, with associated landscaping and car parking. As amended on 14/10/2020.

Alex Mosely spoke in objection to the application on behalf his clients at 4 Kenmore Close and 35 Yew Tree Bottom Road. The L shaped development showed that there would be rear facing windows from 4 properties directly into habitable side windows of 4 Kenmore Close. The properties would overlook the garden and the distance from these homes to the shared boundary was 12 metres. The plans did not address the issues of privacy and overlooking, and the layout was not a typical back land development. This could be improved if all the properties were positioned in the same direction.

In respect of the residents of 35 Yew Tree Bottom Road, plot 8 would detract from the enjoyment of their property and plans were inaccurate as they did not show his client's summerhouse. There would be an unacceptable loss of light and sunlight, particularly in peak summertime and the applicant had not prepared a daylight and sunlight assessment. There would be an infringement of privacy from rear facing

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windows. The applicant had not considered the Planning Inspector's concerns around character and he urged the Committee to refuse the application.

Sarah Farrar spoke in objection to the application, stating that there had been more than 200 objections submitted against the application, a significantly larger number than the applications in 2016 and 2019 received. It was felt that the application had been rushed through with a total of 6 amendments since July 2020. The proposal did not provide affordable homes and there was no shortage of luxury homes in the area. The application was an overdevelopment, was not in keeping with the surrounding area and affected privacy. There would be a loss of trees with TPOs and the development would cause harm to wildlife. There was not enough room for bins in the designated refuse area and the volume of bins would create issues with odour. The bin lorry would also cause congestion. The development would be at the detriment of local residents.

Peter Rutter, the Architect for the development, spoke in support of the application stating the Planning Inspector's concerns and reasons for refusal the previous year had been considered. The application had been developed in consultation with Planning Officers and comments from neighbouring properties had been taken into account. The properties would be of red brick construction with catslide rooves. The properties were generously separated and were well landscaped. Objections from 3 Kenmore Close and 35 Yew Tree Bottom Road had been addressed. Refuse had been considered with Refuse Officers. If refuse became an issue, a commercial collection of bins could be arranged.

Councillor Harrison, a visiting Member for the ward, spoke on the application, noting that this was the 3<sup>rd</sup> application on the site in 5 years. The Planning Inspector rejected the previous scheme on 3 counts, some of which had been addressed, however this application was the same size as the application in 2016 and that had been dismissed. The proposal was an overdevelopment. Plot 8 would have a significant impact on 35 Yew Tree Bottom Road. Road access on Downs Wood was narrow and parking would difficult, as would access. Page 12 of the report highlighted the Planning Inspector's concerns regarding bin collection.

Reasons for refusal were proposed by Councillor Bray and seconded by Councillor Harp, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

1. The proposed development by virtue of the bulk, scale and massing of the dwellings, dominance of hard surfaces and parking areas to the front of the houses, together with the bin collection point on the access road, would appear cramped, overly dense, car dominated and out of character with the pattern of development in the locality, contrary to policies DES1 and DES2 of the Development Management Plan 2019 and the Reigate and Banstead Local Distinctiveness Design Guide SPD.
2. The proposed development by virtue of the mass of the flank wall of plot 8 and its proximity to the boundary, would have an overbearing impact on the southern end of the rear garden of 35 Yew Tree Bottom Road and would overshadow the existing summerhouse during late afternoon hours, harmful to the residential amenities of this dwelling contrary to policies DES1 and DES2 of the Development Management Plan 2019.

3. The proposed development, by virtue of the location of the refuse and recycling presentation point, would result a harmful impact upon the residential amenities of 14 and 18 Downs Wood by way of noise and disturbance. The proposal is thereby contrary to policies DES1 and DES2 of the Development Management Plan 2019.

**118. 20/01430/F - REDHILL AERODROME, KINGS MILL LANE, REDHILL, SURREY**

The Committee considered an application at Redhill Aerodrome, Kings Mill Lane, Redhill, for the retention of widened hard standing on Taxiway C/D, 14m width across the entire 490m stretch. As amended on 11/02/2021.

Wayne Clark, Chairman of Salfords and Sidlow Parish Council, spoke in objection to the application stating that the application was materially worse for residents than the application refused in 2017. This application proposed a substantial number of flights over homes that were previously unaffected. Noise nuisance should be minimised to an appropriate level. There had been unrestricted use of the unofficial runway (taxiway), however the number of flights from this runway should be capped at 45 per day and this would suit year-round business continuity. If the application was approved, the Parish Council requested a condition on the daily average rolling figure of 45 movements per day.

David Brown, a local resident, spoke in objection to the application, stating that he had no previous issues with the aerodrome until the taxiway redevelopment. Flights now flew over new properties and in closer proximity to others. 70% of aircraft departed from the taxiway. The noise and disturbance were monotonous, and this had been raised with the aerodrome. The taxiway was essentially a runway in winter. This application was similar to the application in 2017 and that was refused, in part to the detrimental effect on the community due to the loss of winter respite. The report showed an increase to the allowance of movements from 45 to 85 per day and this was an 88% increase. Members were asked to recognise the increase in noise disturbance already being experienced from the development and consider a cap in the number of movements in line with that proposed in 2017.

Michael Wood, a Planning Consultant for the Aerodrome, spoke in support of the application. He thanked the Enforcement and Development Management Teams at the Council for the positive attitude and time in order to resolve the matter of the taxiway. All had worked hard to arrive at a workable situation, with restrictions in place that were previously not a requirement. These would not assist the long-term viability of the Aerodrome, however the Aerodrome would make these work. There was open invitation to all, to visit the Aerodrome and an overview of the Consultative Committee was given. Minutes of these meetings were available on the Aerodrome's website.

A motion to refuse the application was proposed by Councillor McKenna and seconded by Councillor Whinney whereupon the Committee voted and the motion was not carried.

**RESOLVED** that, subject to completion of a Section 106 legal agreement, planning permission be **GRANTED** with conditions, as per the recommendation and addendum.

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**119. 20/02824/F - LITTLE THORNS, LONDON ROAD, REDHILL, SURREY, RH1 2JU**

The Committee considered an application at Little Thorns, London Road, Redhill, for the Demolition of a detached house and garage and construction of three terraced houses with associated parking and landscaping. As amended on 16/02/2021.

**RESOLVED** that planning permission be **GRANTED** subject to conditions as per the recommendation.

**120. 20/00315/F - 34 BRIGHTON ROAD, BANSTEAD, SURREY, SM7 1BS**

The Committee considered an application at 34 Brighton Road, Banstead for the demolition of existing surgery with the erection of 4 x 4 bedroom 3 storey houses. As amended on 20/04/2020, 05/02/2021, 10/02/2021 and on 15/02/2021.

Reasons for refusal were proposed by Councillor Harp and seconded by Councillor Bray, whereupon the Committee voted and **RESOLVED** that planning permission be **REFUSED** on the grounds that:

1. The proposed development would be located in an area of low accessibility and would provide insufficient off street parking to meet the parking standards as set out in Annex 4 of the Development Management plan 2019 This would result in additional pressures for on street parking in the local area to the detriment of the amenities of existing residents, contrary to the provisions of Policies CS1 and CS10 of the Reigate and Banstead Local Plan Core Strategy and Policies DES1, TAP1 and Annexe 4 of the Reigate and Banstead Development Management Plan 2019.

In view of the time, the Committee **RESOLVED** to consider item 9 of the agenda only. The meeting was adjourned at 10.14PM and resumed at 10.17PM.

**121. 20/01846/F - BENTING MEAD, LONESOME LANE, REIGATE, SURREY, RH2 7QT**

The Committee considered an at application at Benting Mead, Lonesome Lane, Reigate, for the removal of existing industrial and stable buildings, construction of 3 detached dwellings. As amended on 13/10/2020, 26/10/2020, 11/12/2020 and on 11/02/2021.

**RESOLVED** that planning permission be **GRANTED** subject to conditions as per the recommendation.

**122. 20/02581/F - 94 BRIGHTON ROAD, HORLEY**

This item was **DEFERRED** to next meeting due to lack of time.

**123. 20/02840/HHOLD - 9 GARDEN CLOSE, BANSTEAD, SM7 2QB**

This item was **DEFERRED** to next meeting due to lack of time.

**124. ANY OTHER URGENT BUSINESS**


There was none.

The Meeting closed at 10.35 pm

# Agenda Item 5

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 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE	
	<b>DATE:</b>	14 <sup>th</sup> April 2021	
	<b>REPORT OF:</b>	HEAD OF PLANNING	
	<b>AUTHOR:</b>	Matthew Sheahan	
	<b>TELEPHONE:</b>	01737 276010	
	<b>EMAIL:</b>	Matthew.Sheahan@reigate-banstead.gov.uk	
<b>AGENDA ITEM:</b>	5	<b>WARD:</b>	Horley Central and South

<b>APPLICATION NUMBER:</b>	20/02581/F	<b>VALID:</b>	18/12/2020
<b>APPLICANT:</b>	Veer Properties	<b>AGENT:</b>	Z Group Architects
<b>LOCATION:</b>	94 BRIGHTON ROAD HORLEY SURREY RH6 7JQ		
<b>DESCRIPTION:</b>	<b>Extension, alteration and addition of residential accommodation to the existing building on 94 Brighton Road to provide 6 self contained flats.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

**This application was deferred from the March meeting.**

## SUMMARY

This is a full application for a rear extension, alteration and the addition of a second storey to the existing building at 94 Brighton Road. The application adds an additional unit contained within the approved footprint of the previously approved application 20/00503/F. That additional unit being on the ground floor within what was formerly the ground floor storage area for the retained retail unit.

The proposal would provide an additional 6 No. flats. This includes 2 No. one-bedroom flats and 4 No. studio flats (2 x 1b2p and 4 x 1b1p). The existing flat at first floor and retail unit at ground floor of the existing building would be retained. The existing car park at the rear is also retained and this will provide space for parking, refuse and recycling which are all accessed from Lumley Road. A total of 7 parking spaces are proposed.

The application site occupies a highly visible location at the junction of Brighton Road and Lumley Road. The design is considered substantially the same as previously approved and is considered to integrate well with the existing building. Given the varied style and designs of neighbouring buildings in the locality, the proposal is considered to have an acceptable impact upon the visual amenities of the area. The traditional design would accord with local distinctiveness and the increase in height to the existing building would successfully mark the corner site location, whilst the reduction in scale along Lumley Road would gradually decrease

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towards the residential properties that neighbour the site and accord with the style of character of the streetscene.

The proposal is not considered to result in a harmful impact upon the amenities of neighbouring properties by virtue of appropriate window placements and separation distance. Whilst the increased depth would have some impact on the windows of the residential property at 92 Lumley Road to the south, given the nature of the rooms these windows serve the level of harm would be acceptable.

The proposed units would accord with the Nationally Described Space Standards for living space with regards their internal layout. Whilst there would be a shortfall of 1 parking space against the standards in Annex 4 of the DMP, it is considered that, given the sustainable location of the site, combined with the nature of the proposed unit and likely car ownership levels for the development as a whole, that this shortfall would not result in significant undue pressure on the existing on-street parking in the area. The proposal is therefore considered to comply with Policy TAP1 of the Development Management Plan 2019.

In conclusion the proposed development is considered to be acceptable in terms of design and the impact of this on the character of the area, scale and impact on neighbouring residential development, provide an appropriate living environment for future occupants, and provide an acceptable level of parking.

## **RECOMMENDATION**

Planning permission is **GRANTED** subject to conditions.



## Consultations:

Highway Authority: The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements subject to conditions and informatives.

Contaminated Land Officer – No objection raised subject to conditions and informatives relating to ground contamination and asbestos.

Horley Town Council – No objection raised

## Representations:

Letters were sent to neighbouring properties on 30<sup>th</sup> December 2020. No responses have been received.

### 1.0 Site and Character Appraisal

- 1.1 The site is located on the corner of Brighton Road and Lumley Road within the urban area and local shopping area and the premises are currently used as an A1 retail electrical shop selling to the trade and to the public at ground floor level and residential above. The main part of the building is a two storey detached building with a hipped roof. Towards the rear part of the site is a single storey flat roofed addition and a parking area. The contour of the site is flat and there are no trees affected by this proposal.
- 1.2 The surrounding area is mixed in character with the properties fronting Brighton Road to the south of the application site mainly in commercial use at ground floor level and residential above. To the north of the site and along Lumley Road, there are residential properties varying in style and scale.

### 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application advice was not sought prior to the submission of this application.
- 2.3 Further improvements could be secured: Additional benefits could be secured by way of appropriate conditions.

### 3.0 Relevant Planning and Enforcement History

There is a long planning history for the site, the most recent are detailed below

- |     |            |                               |               |
|-----|------------|-------------------------------|---------------|
| 3.1 | 11/01894/F | Proposed additional vehicular | Approved with |
|-----|------------|-------------------------------|---------------|

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- |     |            |   |  |
|-----|------------|---|--|
|     |            | crossover and provision of 2 gates to match the existing within existing secure boundary fence. To ease goods delivery.                         | conditions<br>22 December 2011                             |
| 3.2 | 08/02196/F | Raise pitch roof to suit street scene   | Approved with conditions<br>29 <sup>th</sup> December 2008 |
| 3.3 | 08/00081/F | Provision of basement to previously approved bungalow   | Approved with conditions<br>26 March 2008                  |
| 3.4 | 20/00503/F | Extension, alteration and addition of residential accommodation to the existing building on 94 Brighton Road to provide 5 self-contained flats. | Approved with conditions<br>12th June 2020                 |

## 4.0 Proposal and Design Approach

- 4.1 This is a full application for the extension, alteration and addition of residential accommodation to the existing building on 94 Brighton Road. The proposal would provide an additional 6 No. flats. This includes the 2 No. one-bedroom flats and 3 No. studio flats (2 x 1b2p and 3 x 1b1p) approved under application 20/00503/F and an additional 1 bed 1 person studio flat to the rear of the ground floor. The additional flat would be contained within the approved layout. The existing retail unit and existing flat above would be retained. The existing car park at the rear is also retained in part, and this will provide space for residents parking, refuse and recycling which are all accessed from Lumley Road.
- 4.2 A new storey is proposed to be added to the existing two storey frontage building, maintaining the hipped roof, and an extension to the rear, stepping down in height to two and half storeys, decreasing then to one and a half storeys as it extends down Lumley Road. At ground floor level seven car parking spaces are proposed and an area of storage to be used in conjunction with the existing retail shop. There would also be an internal bicycle store to the ground floor at the rear, whilst the refuse area would also be kept to the rear.
- 4.3 The external design of the scheme is the same as that approved under the previous scheme, following the form and design of the existing building. The proposed additional flat would be contained at the ground floor within what was proposed to be the storage area for the ground floor retail unit, contained within the approved layout with no additional form or massing proposed. The applicants no longer require the level of storage space previously approved.

- 4.4 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.5 Evidence of the applicant’s design approach is set out below:

Assessment	<p>The character of the surrounding area is assessed as predominantly residential, with a mixture of local shops located along Brighton Road. Most of these local shops are mixed-use, with shops at ground level and residential accommodation above. The design of these shops remains traditional, for example No.84-92 is a two-storey block with facing brickwork and a 45-degree pitched roof with large dormers...Along Lumley Road the area becomes fully residential. This area has a clear mixture of flats and houses, meaning there is also a mixture of housing character. Lumley Road includes large housing developments, for example No.7-12 (Lumley court) is a modern three-storey block of flats constructed from brickwork with a hip roof. In contrast, Lumley Road is predominately fronted by Victorian/Edwardian semi-detached houses and a handful detached houses modern in character. The mixture of characters creates an attractive and diverse district for residents in the area</p> <p>Site features meriting retention are the existing retail unit and flat and the existing car park at the rear of the site.</p>
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The applicant’s reasons for choosing the proposal from the available options were informed by pre-application advice. The design takes its cues from residential development in the area. The proposal has been designed to respect the existing buildings vernacular and enhance the local distinctiveness of the area. The proposal’s mass and scale has been considered in relation to the neighbouring buildings to ensure the mass and scale of the proposal does not have a detrimental impact on the quality of neighbour’s amenity both in terms of access to daylight and the feeling of overbearingness.

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4.6 Further details of the development are as follows:

Site area	0.04 hectares
Proposed parking spaces	7
Parking standard	8 residential 6 (maximum) retail
Net increase in dwellings	6
Proposed site density	125 dwellings per hectare
Density of the surrounding area	125 dwellings per hectare – Lumley Court

## 5.0 Policy Context

### 5.1 Designation

Urban area  
Local Shopping Centre

### 5.2 Reigate and Banstead Core Strategy

CS1 (Sustainable Development)  
CS4 (Valued Townscapes and Historic Environment)  
CS5 (Valued People/Economic Development),  
CS7 (Town/Local Centres),  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),  
CS14 (Housing Needs)  
CS15 (Affordable Housing)

### 5.3 Development Management Plan

DES1 (Design of new development),  
DES4 (Housing mix),  
DES5 (Delivering high quality homes),  
DES6 (Affordable housing),  
DES8 (Construction management),  
DES9 (Pollution and contaminated land),  
TAP1 (Access, parking and servicing),  
CCF1 (Climate change mitigation),  
INF3 (Electronic communication networks),  
RET3 (Local Centres)

### 5.4 Other Material Considerations

National Planning Policy Framework  
2019

## National Planning Practice Guidance

### Supplementary Planning Guidance

### Surrey Design

Local Distinctiveness Design Guide  
A Parking Strategy for Surrey  
Parking Standards for Development  
Affordable Housing

### Other

Human Rights Act 1998  
Community Infrastructure Levy  
Regulations 2010

## 6.0 Assessment

6.1 The site is located within the urban area where there is a presumption in favour of sustainable development and where the principle of residential development is acceptable.

6.2 The main issues to consider are:

- Design appraisal
- Neighbour amenity
- Access and parking
- Amenity for future occupants
- Sustainability, Climate Change and infrastructure
- Affordable Housing
- CIL

### Design appraisal

6.3 The application proposes the addition of a second storey to the existing building and part 2.5 storey, part 1.5 storey extension to the rear of the site, along Lumley Road. The application site occupies a highly visible location at the junction of Brighton Road and Lumley Road. The parade of shops to the south do have roof accommodation served by dormers and the flatted development to the north on the opposite side of the junction is a collection of 3 storey, 2.5 storey and 2 storey residential buildings. Heading along Lumley Road the scale of development decreases to two storey residential houses, and also includes a bungalow, immediately adjacent to the site. The scale and design of the proposed development is considered to be in accordance with the locality, following the principles of good design practice in marking the corner site and defining the location of a junction. The height of the building would be similar to that on the opposite side of the junction which is also a three storey building.

6.4 Turning to the rear extension, this element of the proposal would decrease in height as it progresses south eastwards along Lumley Road towards the neighbouring residential dwellings. This reduction in scale to the rear respects the pattern of development where the character of the locality changes from

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that fronting Brighton Road to a residential nature, formed largely by two storey houses.

- 6.5 The design of the extensions are informed by the existing building. The additional storey to the existing building would have a hipped roof and this design would be mirrored in the rear extension with hipped roofs and matching fenestration with the exception of a glazed staircase that would have a more contemporary appearance.
- 6.6 The design is considered to integrate well with the existing building. Given the varied style and designs of neighbouring buildings in the locality, the proposal is considered to have an acceptable impact upon the visual amenities of the area. The traditional design would accord with local distinctiveness and the increase in height would successfully mark the corner site location. Overall, the design is considered acceptable.
- 6.7 It is therefore considered that the proposal would comply with Policy DES1 of the Development Management Plan (DMP) 2019 and Local Distinctiveness Guide.

## Neighbour amenity

- 6.8 To the south east of the site is a detached bungalow, 147 Lumley Road. The proposed rear extension would retain a gap to the shared boundary 3.4m and the addition would reduce in scale as it becomes closer to no. 147. The eaves height of the proposed building at this nearest point would be 4.4m. There is one side facing window in no. 147 that looks towards the application site. Plans (reference 08/00081/F) show this window serves a bathroom and the proposal would pass the 45 degree assessment, as it would not intersect a 45 degree vertical plane measured from this window. The proposal would not therefore result in unacceptable loss of light to this neighbour. Given the reduction in scale and level of separation between the two properties, the proposal is not considered to result in an overbearing or dominating impact upon the dwelling. No windows are proposed to face No.147 with the exception of a single ground floor window; however this would not face any windows serving neighbouring habitable rooms. It is not considered therefore to result in a harmful impact in terms of overlooking or loss of privacy.
- 6.9 To the north of the site on the opposite side of Lumley Road is Lumley Court, a collection of three buildings containing a total of 25 flats. Flats 1 – 6 would be sited between 15.5m and 17.5m from the proposal and 154 and 152 Lumley Road would be sited approximately 18.6m from the proposed rear extension. Given the level of separation the proposal is not considered to result in a harmful impact upon the amenities of the neighbouring dwellings on the north eastern side of Lumley Road in terms of overbearing, domination or overlooking and loss of privacy.
- 6.10 To the south of the site lies a terraced building made up of commercial uses at ground floor and residential at first and second floor. The nearest neighbour to the application site is 92 Brighton Road. At ground floor level

there is a takeaway business and a maisonette above at first and second floor, 92a. To the rear of the building there are two first floor windows and a dormer window. There are also two smaller side facing windows at first floor and a side facing dormer window at second floor level. Looking at the planning history for this neighbouring building, plans ref: 55/0541 show the layout of the maisonette, no. 92A. The first floor rear facing windows serve a kitchen and bathroom. The first floor side facing windows serve a pantry off to the side of the kitchen and the hallway. At second floor, the side facing dormer window serves the hallway area. The rear facing dormer window serves a bedroom.

- 6.11 The proposal would have some impact upon light to the rear facing kitchen window and the outlook, with the 2.5 storey element of the proposal extending approximately 3m beyond the rear elevation of 92a before stepping down in height. Whilst there would be some impact upon this window as described above, the kitchen is relatively modest in size and therefore it is considered reasonable to conclude that dining would take place in one of the two reception rooms that are served by front facing windows and set further away from the proposed development. Due to the less habitable nature of the room, the impact upon this window is thus not considered so harmful as to warrant refusal of the application on this basis. The side facing windows do not serve habitable spaces and therefore the proposal is not considered to result in a harmful impact upon the amenities of these spaces. The side facing windows proposed to the second floor extension to the existing building would look more directly towards the front of No. 92 and the proposal is not therefore considered to result in a harmful impact in terms of overlooking or loss of privacy.
- 6.12 Overall, the proposal is not considered to result in a harmful impact upon the amenities of neighbouring properties and complies with policy DES1.

### Highway matters

- 6.13 The application proposes a total of 7 parking spaces to the rear of the site, accessed from Lumley Road. The application proposes 6 new flats along the with retention of 1 existing flat. The site is located within an area of medium accessibility as defined in Annex 4 of the DMP. This requires 1 parking space per unit, and 2 visitor parking spaces. The Application also proposes 2 parking spaces to the front of the site for the retail unit. There is no dropped kerb which currently allows for access to this area, however it is noted that vehicles do park in front of the building informally. In considering the application the County Highways Authority has the following view:
- 6.14 *This application was previously approved for 5 dwellings and maintaining some element of retail use. The current proposal is to add an additional studio (1 no). This will require 1 additional parking space. There is on-street parking allowed in the vicinity of the site, and from site observation, it is possible to meet the shortfall for one parking space within reasonable walking distance from the site. There is concern about the location of the two existing parking spaces shown at the western edge of the site, at the junction of*

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*Brighton Road and Lumley Road. Hence the requirement for Condition 2, which is the same condition for the previous approved application Ref 20/00503/F. However, it is noted that these parking spaces are currently used by QVS customers, even though there are no dropped kerbs to provide access to the parking spaces.*

- 6.15 As with the previous application, a condition preventing the creation of any means of access from the development to Brighton Road or the service road next to Brighton Road would be included in the event of planning permission being granted. The site fronts Brighton Road within a Local Centre, in nearby proximity to bus stops well served by bus routes to the north and south and shops, services and facilities. The site is also located approximately 650m from High Street Horley whereby local amenities and key services can be accessed. On this basis it is considered that the shortfall of one parking space would not warrant refusal of the application in this instance, given the sustainable nature of the location. With regard to parking requirements for the retail element of the proposal, maximum parking standards apply and on this basis the proposal is not considered to warrant refusal.
- 6.16 The County Highway Authority therefore has no highway requirements subject to conditions. The recommended conditions require the existing access to Lumley Road to be modified to serve the proposed car parking spaces on Lumley Road in accordance with a scheme to be submitted, the means of access to the development to be from Lumley Road only, no means of access from London Road or the service road next to London Road, plans for a parking scheme, bicycle parking, a construction transport management plan and fast charge parking sockets. Subject to compliance with these conditions the proposed development is considered to comply with the requirements of the NPPF 2019 and Policy TAP1 of the DMP 2019.

## Amenity for future occupants

- 6.17 The application proposes 2 x 1 bedroom 2 person flats and 4 x 1 bedroom one person studio flats. All units would meet the minimum internal space standards, as defined within the Nationally Described Space Standards. Policy DES5 of the DMP requires new residential development to comply with these standards. In assessing the development each of the proposed units would meet the requirements of these standards in terms of internal living space. Primary living areas such as living rooms and bedrooms would be well served by appropriately placed windows and each flat would be conveniently laid out.
- 6.18 In this instance the proposal does not include the provision of private or communal outdoor space. This is similar to the neighbouring properties to the south fronting Brighton Road and to the existing flat which does not currently have outdoor amenity space. The site is located approximately 250m north east of Horley Recreation Ground where open space and sports courts can be found. The close proximity to recreational space nearby to the application site whereby residents would have access to open space is considered to provide adequate access to outdoor amenity space for future occupants.



- 6.19 In light of this the proposal is considered to comply with the requirements of the Nationally Described Space Standards and Policy DES5 of the DMP 2019.

### Sustainability, climate change and infrastructure

- 6.20 Policy CCF1 of the DMP 2019 seeks to ensure that all new development contributes to reducing carbon emissions. New development will be encouraged to incorporate passive and active energy efficiency measure and climate change resilience measures and renewable energy technologies. In order that the proposed development contributes to achieving these aims, in the event that planning permission is granted, conditions requiring demonstration that it will meet the national water efficiency standard of 110litres/person/day and achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations would be attached.
- 6.21 Additionally Policy INF3 requires all new development to be connected with high speed and reliable broadband. A suitable condition to ensure that this is secured would be included in the event of planning permission granted.
- 6.22 Subject to compliance with the above conditions, the proposal is considered to be acceptable and in accordance with Policies CCF1 and INF3 of the DMP 2019.

### Community Infrastructure Levy

- 6.23 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, roads, public transport and community facilities which are needed to support new development. This development would be CIL liable although, the exact amount would be determined and collected after the grant of planning permission.

### Affordable Housing

- 6.24 Policy CS15 of the Core Strategy states that the Council will negotiate to achieve affordable housing taking account of the mix of affordable units proposed and the overall viability of the proposed development at the time the application is made.
- 6.25 DMP Policy DES6 relates to the provision of affordable housing. This states that on all sites which provide 11 or more homes, 30% of the homes on the site should be affordable housing. This proposal would not therefore qualify for the provision of affordable housing.

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## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Floor Plan	VOC1		20.11.2020
Proposed Plans	VOC2		20.11.2020
Proposed Plans	VOC3		20.11.2020
Proposed Plans	VOC4		20.11.2020
Combined Plan	Shadow01		20.11.2020
Existing Plan	SURV01		20.11.2020
Elevation Plan	SURV02		20.11.2020
Existing Plans	SURV03		20.11.2020

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason:

To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Development Management Plan 2019 policy DES1.

4. No development shall take place until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and the development shall be carried out in accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved for the development with regard to Development Management Plan 2019 policy DES1.

5. No part of the development shall be occupied unless and until the existing access to Lumley Road has been modified to serve the proposed car parking spaces on Lumley Road in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and thereafter the visibility zones shall be kept permanently clear of any obstruction over 0.6 metres high above the ground.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

6. (a) The means of access to the development hereby approved shall be from Lumley Road only.  
(b) There shall be no means of access from the development hereby approved to London Road or the service road next to London Road.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan 2019.

7. Notwithstanding the submitted plans the development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with a revised scheme to be submitted to and approved in writing by the Local Planning Authority for vehicles to be parked. The parking spaces shall be unallocated and for residents purposes only. Thereafter the parking area shall be retained and maintained for its designated purpose.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

8. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for bicycles to be stored in a secure and covered location. Thereafter the bike parking area shall be retained and maintained for its designated purpose.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan

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Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

9. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) provision of boundary hoarding behind any visibility zones
  - (e) measures to prevent the deposit of materials on the highway
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

10. The development hereby approved shall not be occupied unless and until a minimum of one of the available parking spaces has been provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) and one of the parking spaces has been fitted with an electrical supply to fit a future fast charge socket in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority.

Reason:

The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing TAP2 of the Reigate and Banstead Local Plan Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

11. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason:

To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the Reigate and Banstead Borough Local Plan 2005 policies Ho9 and Pc4.

12. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
  - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason:

To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

13. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
  - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason:

To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

14. The developer must either submit evidence that the building was built post 2000 or provide an intrusive pre-demolition and refurbishment asbestos survey in accordance with HSG264 supported by an appropriate mitigation scheme to control risks to future occupiers. The scheme must be written by a suitably qualified person and shall be submitted to the LPA and must be approved prior to commencement to the development. The scheme as submitted shall identify potential sources of asbestos contamination and detail removal or mitigation appropriate for the proposed end use. Detailed working methods are not required but the scheme of mitigation shall be independently verified to the satisfaction of the LPA prior to occupation. The development shall then be undertaken in accordance with the approved details.

Reason:

To ensure that a satisfactory strategy is put in place for addressing contaminated land before development commences and to make the land

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suitable for the development without resulting in risk to construction workers, future users of the land, occupiers of nearby land and the environment with regard to Reigate and Banstead Development Management Plan 2019 Policy DES9 and the NPPF.

15. If, prior to or during development, ground contamination is suspected or manifests itself then no further development (unless otherwise agreed in writing by the Local Planning Authority) shall be carried out until the developer has submitted an appropriate remediation strategy to the Local Planning Authority and written approval of the Local Planning Authority has been received. The strategy should detail how the contamination shall be managed.

The remediation strategy shall be implemented in accordance with such details as may be approved and a remediation validation report shall be required to be submitted to the Local Planning Authority to demonstrate the agreed strategy has been complied with.

Should no ground contamination be readily identified during the development, confirmation of this should be provided in writing to the Local Planning Authority.

Reason:

To comply with the NPPF 2019 and Reigate and Banstead Local Plan Development Management Plan 2019 Policy DES9.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.org.uk](http://www.firesprinklers.org.uk).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any

appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.

4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

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7. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
8. In seeking to address and discharge the 'contamination remediation' condition above, the applicant's attention is drawn to the fact that the application site is situated on or in close proximity to land that could be potentially contaminated by virtue of previous historical uses of the land. Visual and olfactory evidence of contamination can take many forms including hydrocarbon or solvent odours, ash and clinker, buried wastes, burnt wastes/ objects, metallic objects, staining and discolouration of soils, oily sheen on ground water and fragments of asbestos containing materials (ACMs) (Note: this list is intended to be used as a guide to some common types of contamination and is not exhaustive).

In seeking to address this condition a photographic record of works should be incorporated within the validation report. Should no ground contamination be identified then a brief comment to this effect shall be required to be provided in writing to the Local Planning Authority.

The Local Planning Authority cannot confirm that the condition has been fully discharged until any validation report has been agreed.

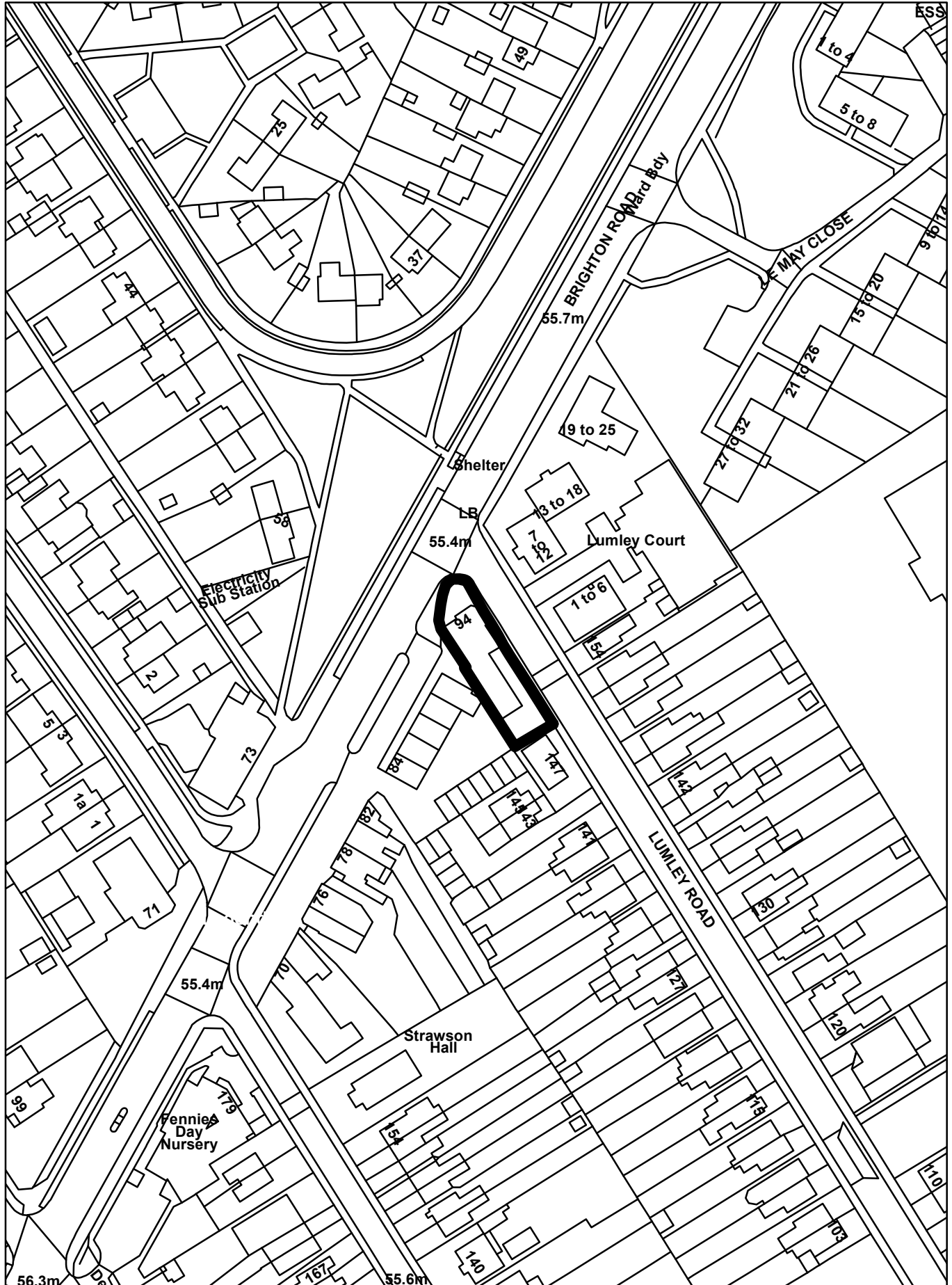
## **REASON FOR PERMISSION**

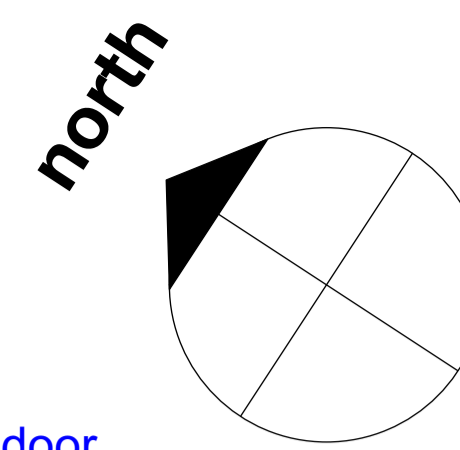
The development hereby permitted has been assessed against development plan policies DES1, DES5, DES6, DES8, DES9, TAP1, CCF1, INF3, RET3 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

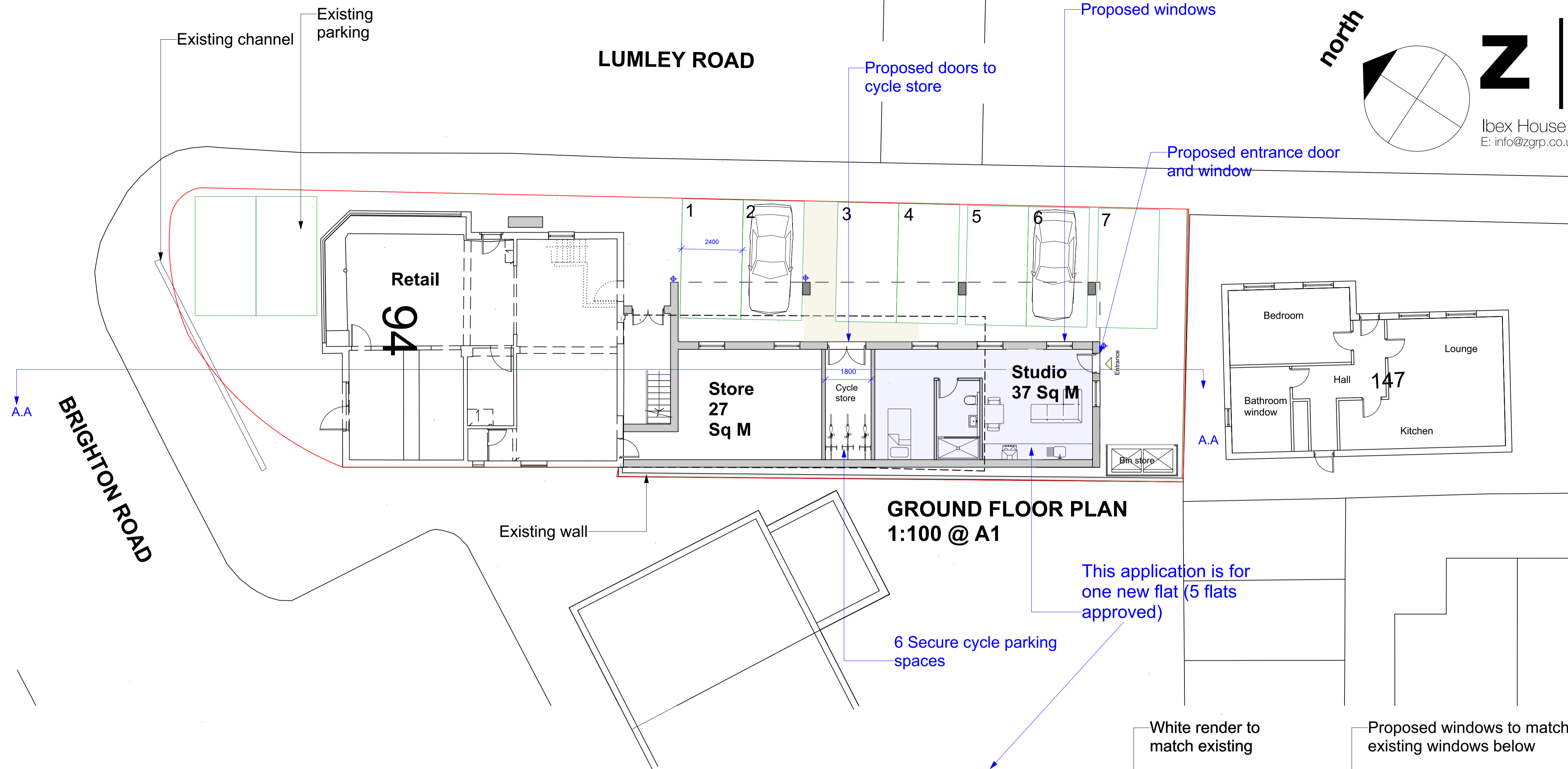






LUMLEY ROAD

BRIGHTON ROAD



**GROUND FLOOR PLAN**  
1:100 @ A1

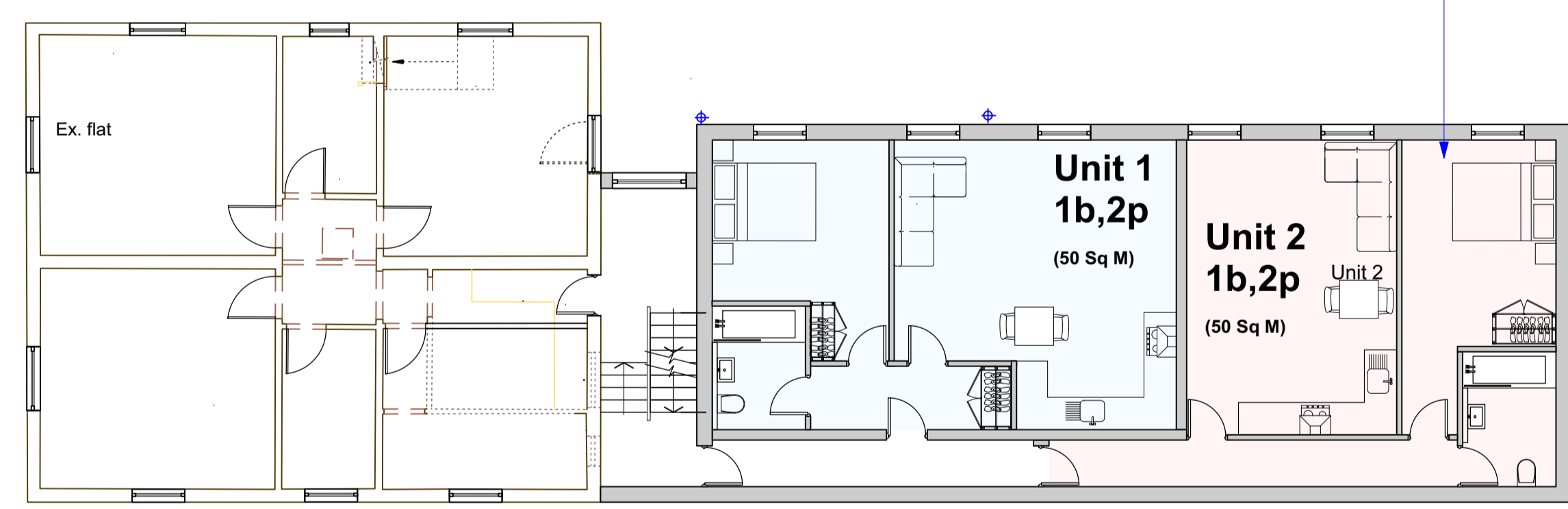


Cycle parking system for 6 bikes

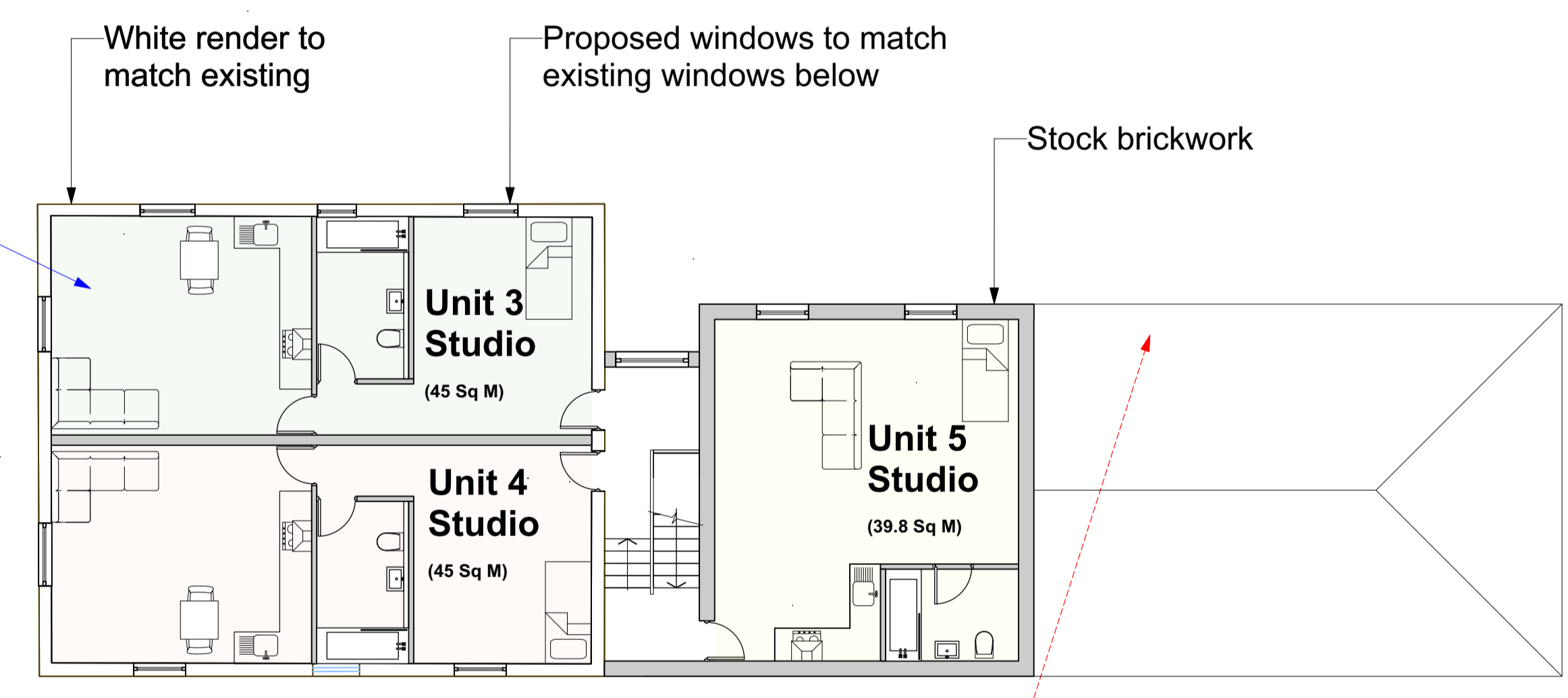
This application is for one new flat (5 flats approved)

6 Secure cycle parking spaces

No.5 Flats approved June 2020 (Ref: 20/00503/F)

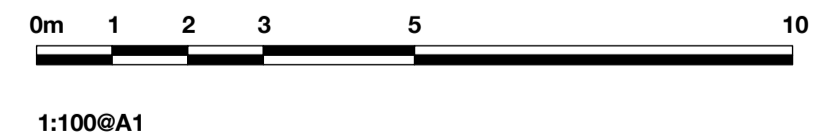


**FIRST FLOOR PLAN**  
1:100 @ A1



**SECOND FLOOR PLAN**  
1:100 @ A1

45 degree rule



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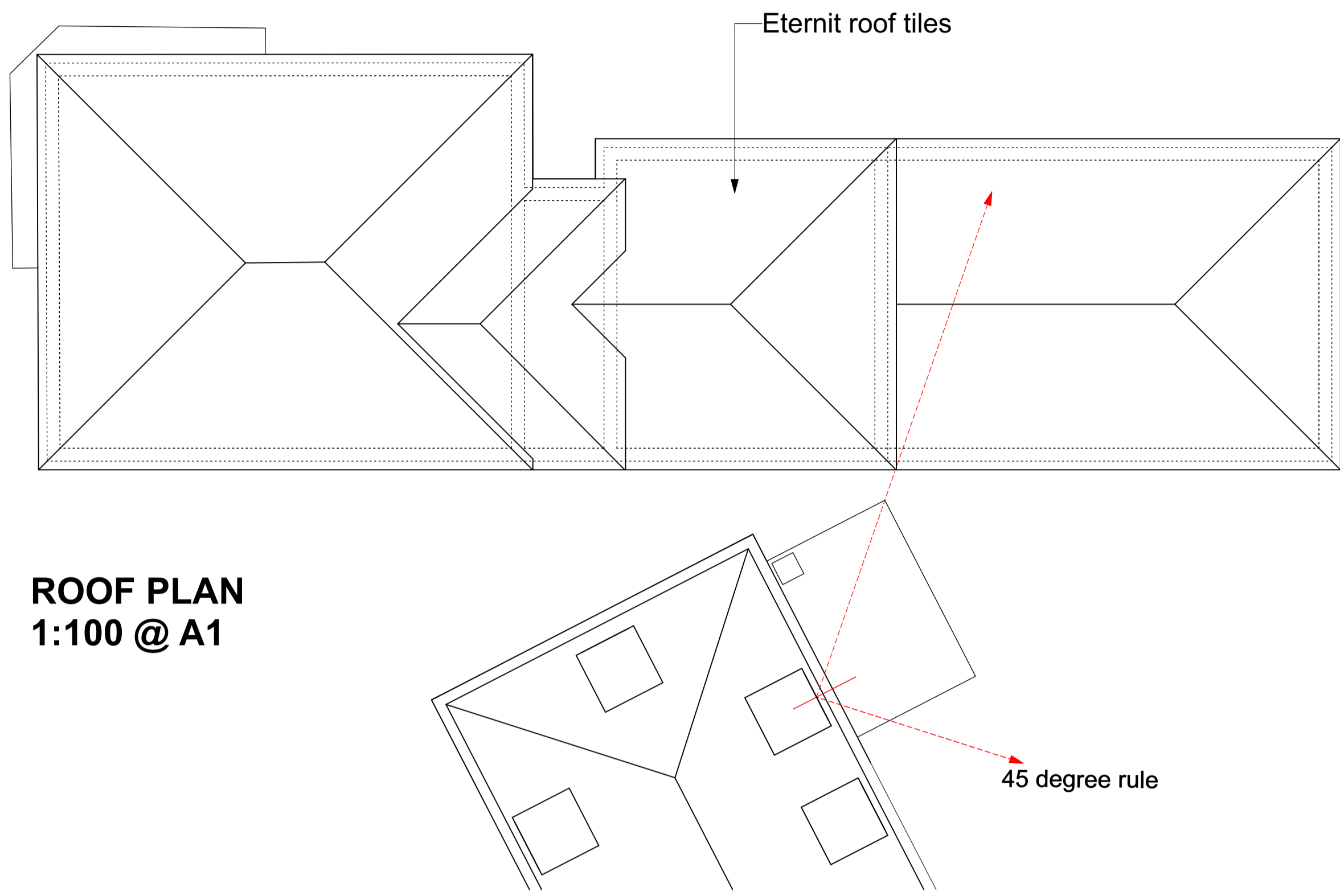
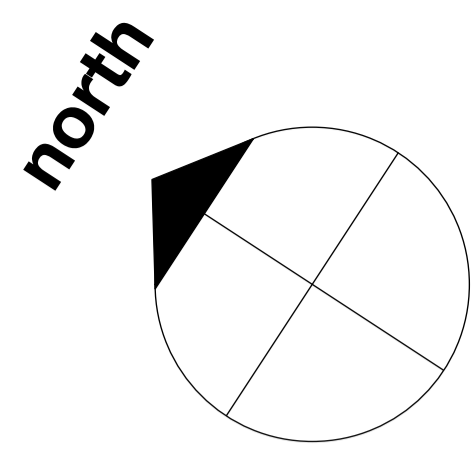
project: 94 Brighton Road, Horley, RH6 7JQ

client: Veerproperties

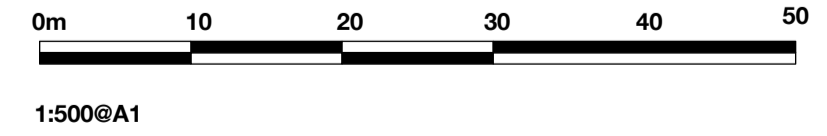
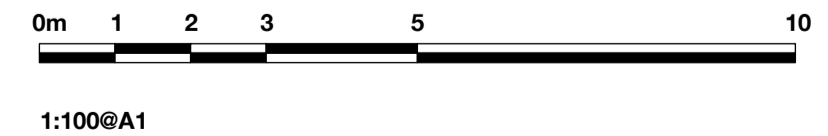
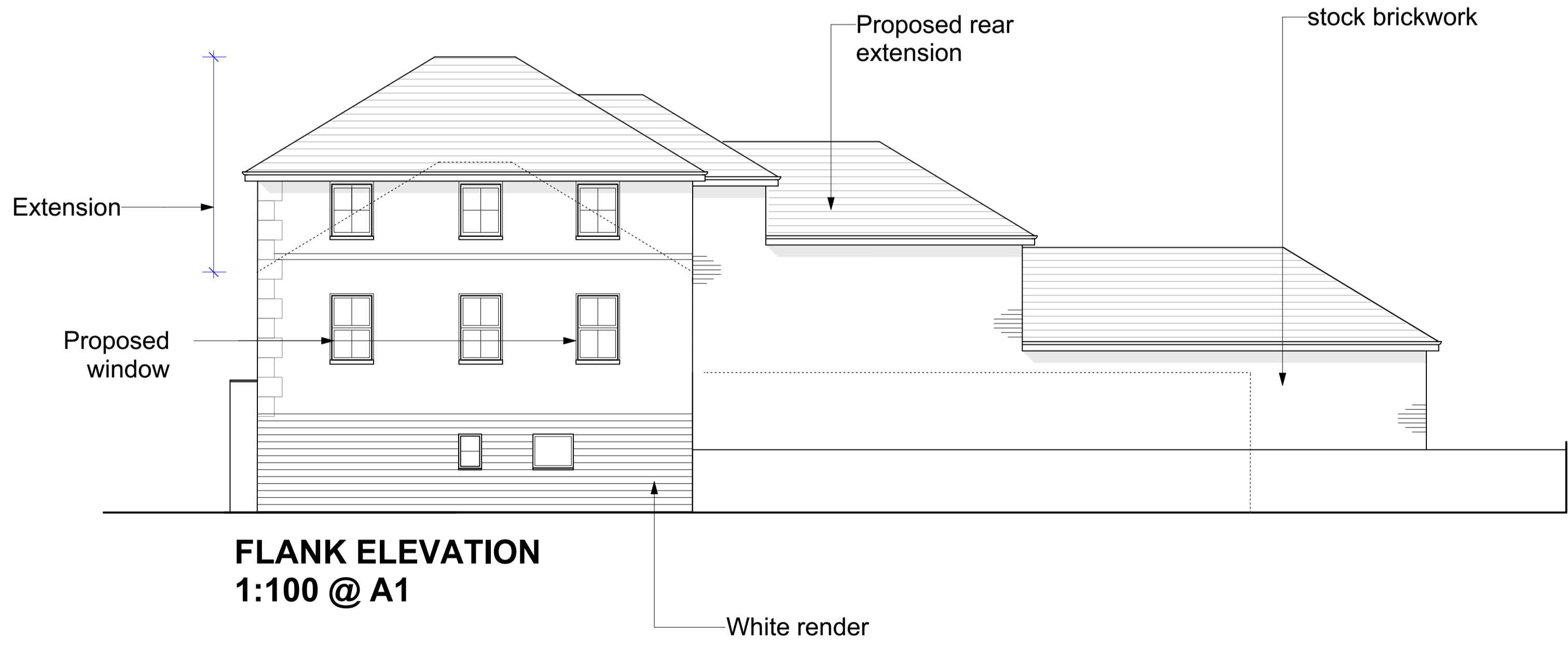
drawing title: Plans as Proposed

date	scale	project no	drawing no	revision
FEB 2020	1:100 @ A1	2470	VOC1	-





**3D PERSPECTIVE FROM BRIGHTON ROAD**



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project: 94 Brighton Road, Horley, RH6 7JQ

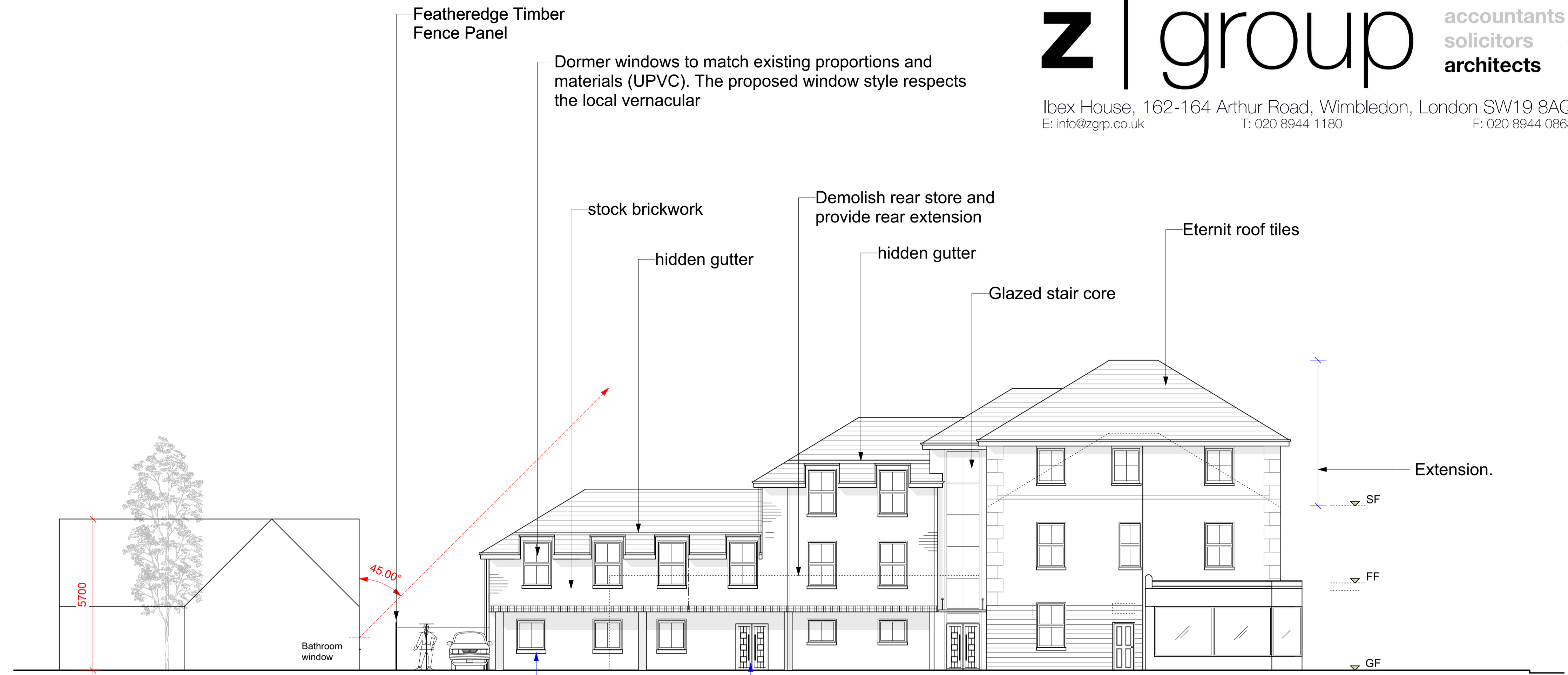
client: Veerproperties

Revision notes:

**drawing title:** Roof Plan and Elevations as Proposed

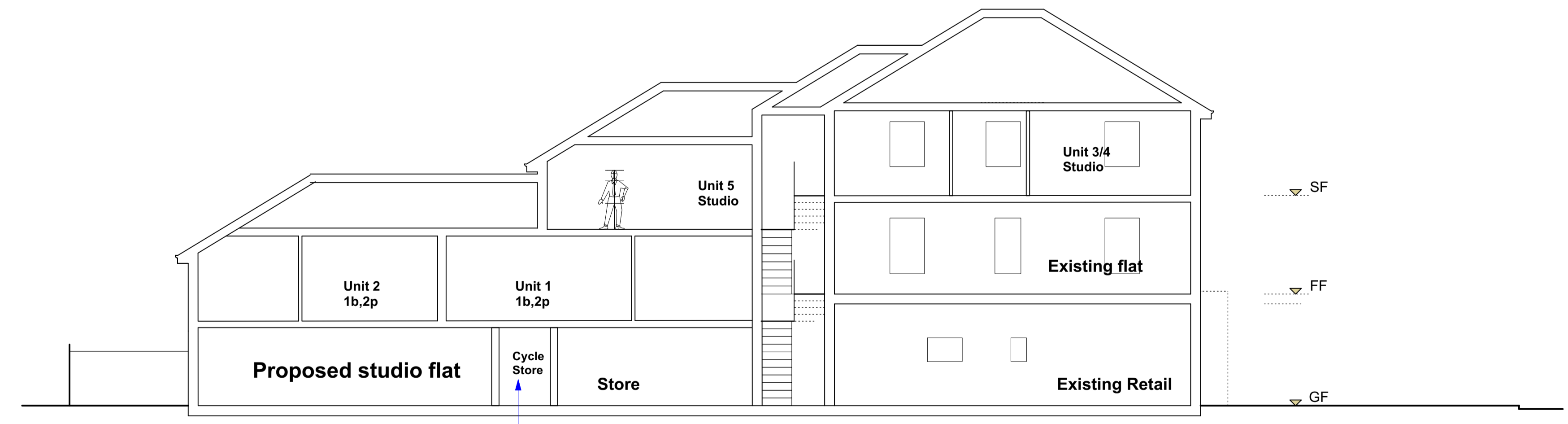
date	scale	project no	drawing no	revision
FEB 2020	1:100 @ A1	2470	VOC2	-





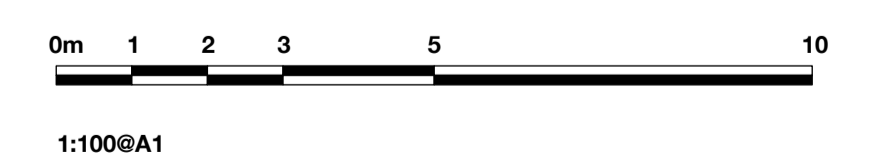
**FLANK ELEVATION (Facing Lumley Road)**  
1:100 @ A1

Proposed window  
Proposed doors to cycle store



**SECTION A-A**  
1:100 @ A1

Area for 6 secure cycle parking spaces



**3D PERSPECTIVE FROM LUMLEY ROAD**

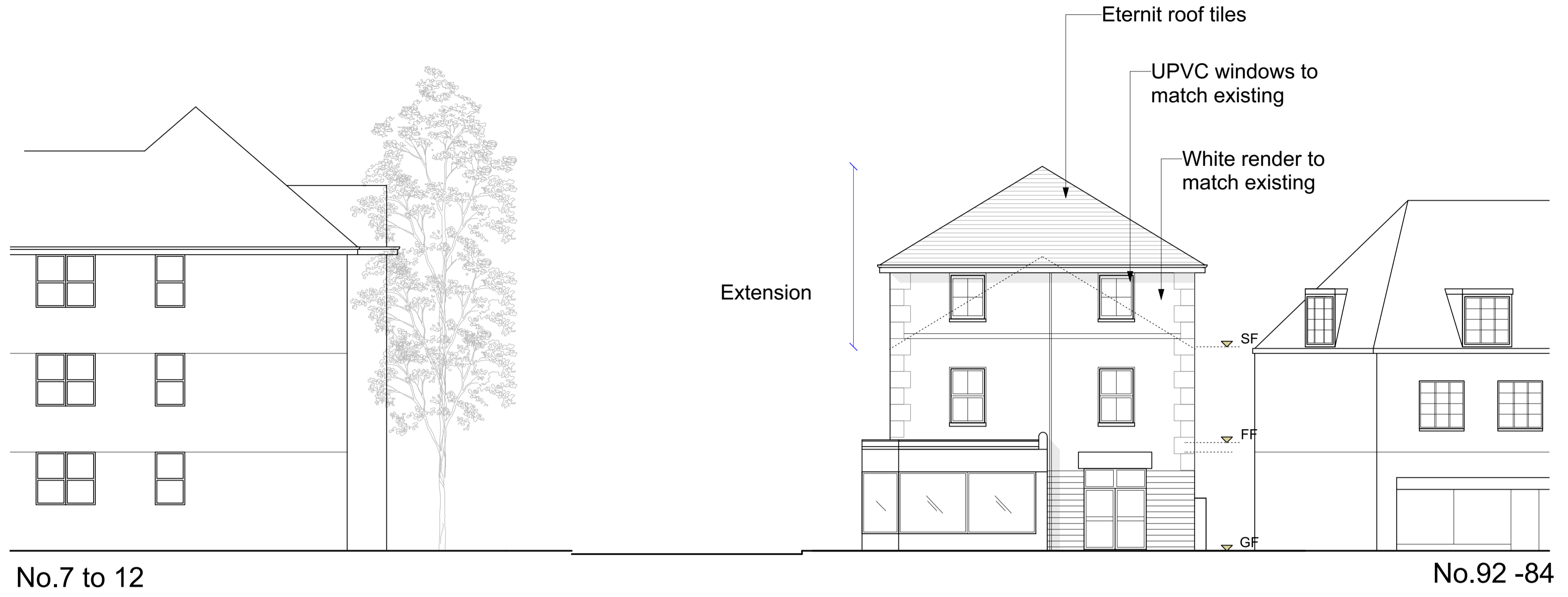
**drawing notes:** This Drawing must not be scaled. Only figured dimensions should be used. Critical site dimensions must be checked on site before proceeding. Any adjoining buildings or features are by way of illustration only and dimensions have not been checked unless specifically noted otherwise. This drawing has been produced for our client for the purposes noted on it and it is not intended for use by any other person for any other purpose. The copyright in this drawing belongs to ZGRP LTD ©. This drawing must not be reproduced in whole or in part or disclosed to any third party unless expressly authorised in writing by a Director of ZGRP LTD.

project: 94 Brighton Road, Horley, RH6 7JQ

client: Veerproperties

Revision notes:				
drawing title: Flank elevation and Section as Proposed				
date	scale	project no	drawing no	revision
FEB 2020	1:100 @ A1	2470	VOC3	-

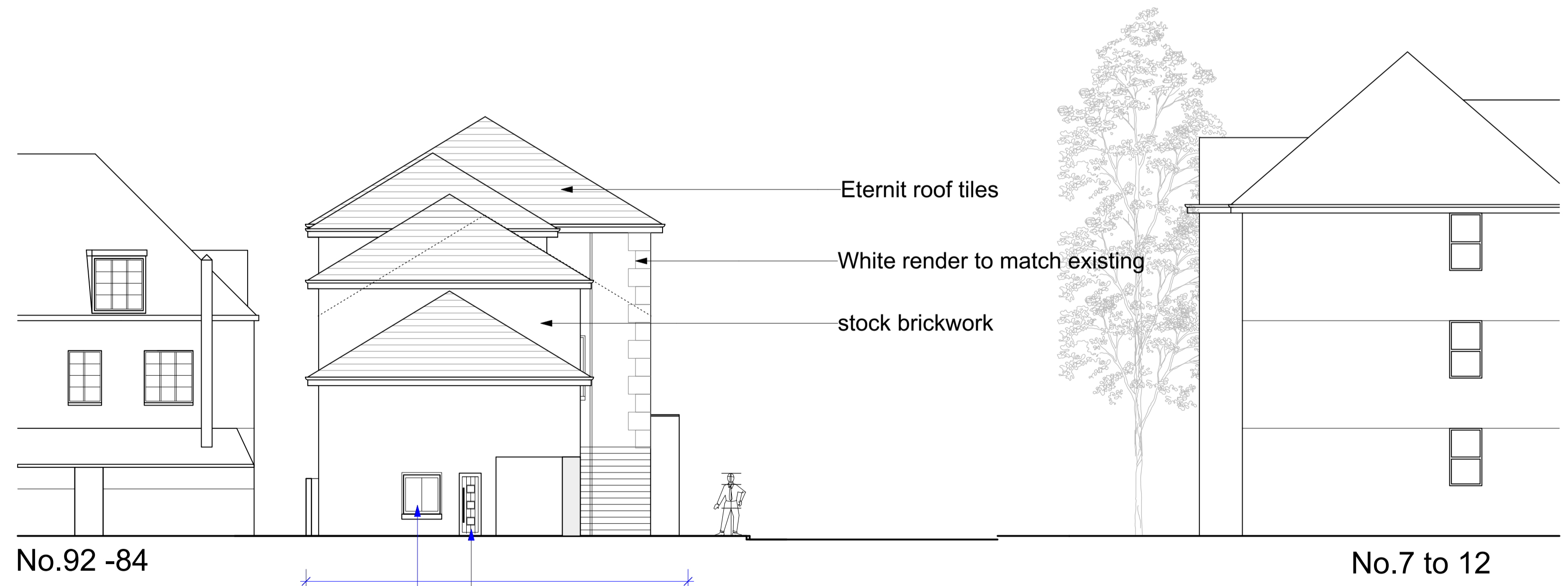




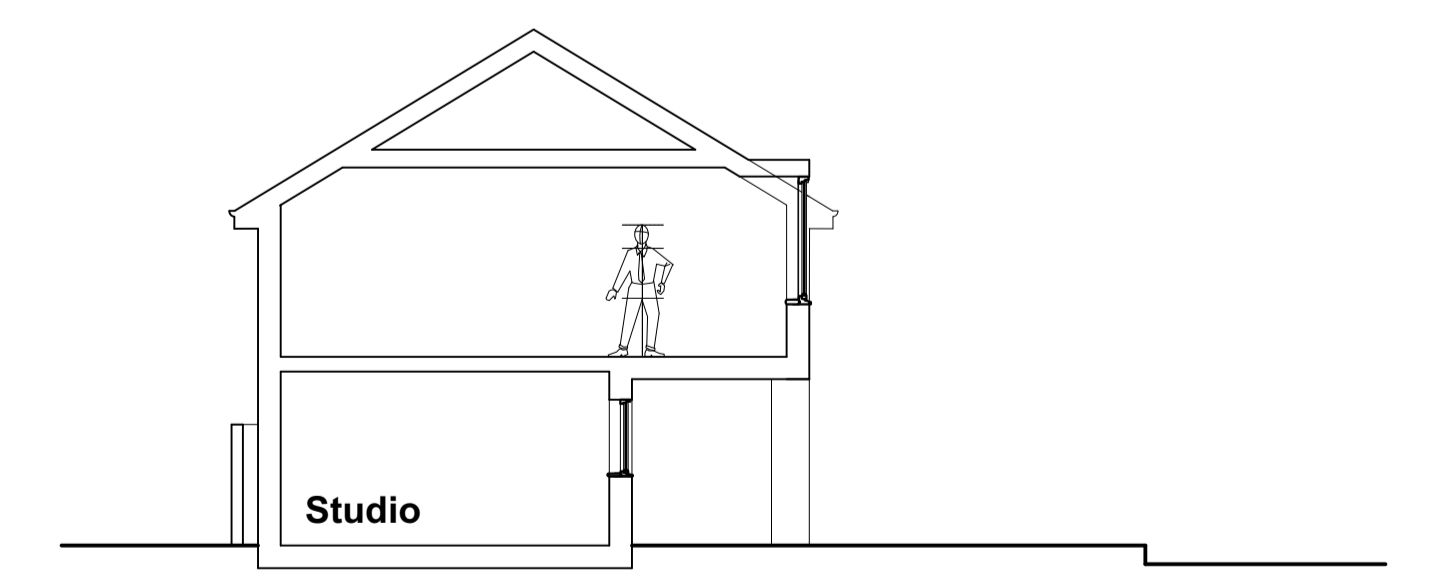
**FRONT ELEVATION (Facing Brighton Road)**  
1:100 @ A1



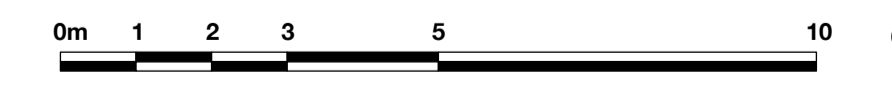
**3D PERSPECTIVE FROM BRIGHTON ROAD**



**REAR ELEVATION**  
1:100 @ A1



**TYPICAL SECTION**  
1:100 @ A1



drawing notes: This Drawing must not be scaled. Only figured dimensions should be used. Critical site dimensions must be checked on site before proceeding. Any adjoining buildings or features are by way of illustration only and dimensions have not been checked unless specifically noted otherwise. This drawing has been produced for our client for the purposes noted on it and it is not intended for use by any other person for any other purpose. The copyright in this drawing belongs to ZGRP LTD ©. This drawing must not be reproduced in whole or in part or disclosed to any third party unless expressly authorised in writing by a Director of ZGRP LTD.

project: 94 Brighton Road, Horley, RH6 7JQ

client: Veerproperties

Revision notes:				
drawing title: Front / rear elevation and typical section as Proposed				
date	scale	project no	drawing no	revision
FEB 2020	1:100 @ A1	2470	VOC4	-

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# Agenda Item 6

Planning Committee  
14<sup>th</sup> April 2021

Agenda Item: 6  
20/02840/HHOLD

 <b>Reigate &amp; Banstead</b> <b>BOROUGH COUNCIL</b> Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	14 April 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Matthew Lambert
	<b>TELEPHONE:</b>	01737 276659
	<b>EMAIL:</b>	Matthew.Lambert@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	6	<b>WARD:</b> Banstead Village

<b>APPLICATION NUMBER:</b>	20/02840/HHOLD	<b>VALID:</b>	14 January 2021
<b>APPLICANT:</b>	Mr & Mrs M Trenaman	<b>AGENT:</b>	Wad Associates Ltd
<b>LOCATION:</b>	<b>9 GARDEN CLOSE, BANSTEAD, SM7 2QB</b>		
<b>DESCRIPTION:</b>	<b>Proposed two-storey side extension</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

**This application is referred to Committee in accordance with the Constitution as the applicant is a member of staff. It was deferred from the March meeting.**

## SUMMARY

The proposed development seeks permission for the erection of a two-storey side extension to the western flank of the existing dwelling, following the demolition of the existing garage.

The proposal would be constructed out of matching materials, would be subservient in size and scale to the dwelling, also reflecting its design approach. It would not extend beyond the front of the house, and would be set 0.45m from the boundary on the neighbouring side. Whilst this would not preserve a 1 metre gap at first floor level to the boundary, as recommended by the Council's Householder Extensions SPD, that is not considered harmful given the surrounding context which includes similar examples of the 1-metre gap to boundary not being preserved. The use at ground floor level would be a garage to replace the existing, a cloakroom, and utility room. At first floor, the existing third bedroom would be enlarged. It is considered that the change to the dwelling would be appropriate given the context of the site and its surroundings, and the addition would not harm the character and appearance of the area.

No material harm to the neighbouring properties would occur as a result of the proposed development and the proposal is therefore considered acceptable in this regard.

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14<sup>th</sup> April 2021

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## RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

### Representations:

Letters were sent to neighbouring properties on 25 January 2021. No representations have been received.

### 1.0 Site and Character Appraisal

- 1.1 The application site is a semi-detached dwelling house built in approximately the 1920s/30s and set in a rectangular shaped that is fairly flat throughout. There are no trees likely to be affected by the proposal.
- 1.2 The surrounding area consists of residential properties of a similar age and slightly varying styles; a number of properties have been extended, both to the side and the rear.

### 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: None sought.
- 2.2 Further improvements could be secured: Materials to match existing.

### 3.0 Relevant Planning and Enforcement History

- 3.1 None

### 4.0 Proposal and Design Approach

- 4.1 This is a full application for a two-storey side extension to the dwelling. The addition would lie in line with the existing front building line, would incorporate a garage and utility room at ground floor level, and a bedroom extension at first floor.
- 4.2 The proposed extension would be built out of matching materials and would be provided with a hipped roof.

### 5.0 Policy Context

#### 5.1 Designation

Urban Area

#### 5.2 Reigate and Banstead Core Strategy

CS1 (Presumption in favour of sustainable development)



## 5.2 Reigate & Banstead Development Management Plan 2019

DES1 (Design of new development)

## 5.3 Other Material Considerations

National Planning Policy Framework

National Planning Policy Guidance

Supplementary Planning Guidance

Householder Extensions and Alterations

Other

Human Rights Act 1998

## 6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development.

6.2 The main issues to consider are:

- Impact on local character
- Neighbour amenity

### Impact on local character

6.3 The Council's Development Management Plan Policy DES1 expects proposals to have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site. The Householder Extensions and Alterations Supplementary Planning Guidance (SPG) 2004 states that two-storey side extensions should employ a suitable design approach, in order to harmonise with the character and appearance of the host property and appear suitably subservient when viewed from the streetscene.

6.4 The proposal is considered acceptable in terms of design. The two-storey side extension would project 5.75m of the depth of the house at ground floor level, and 3.85m at first floor. It would observe the same front building line. It would be 2.65m wide, resulting in a new width of 9m, set 0.45m from the neighbouring boundary. It would take a similar design approach; with closely matching fenestration, render and roof tiles. The proposed roof pitch, angle and style would be clearly subservient to that of the existing house and given the variation of two-storey side extensions in the area; would be harmonious with the street context.

6.5 The Council's Householder Extensions and Alterations SPG recommend that proposals of this nature demonstrate a set-back of at least one metre from the original front wall of the house. The proposal would not feature a set-

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14<sup>th</sup> April 2021

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back. This does not correspond with the guidance. However, given the positioning of the extension, its modest width and depth, alongside its set-in, which matches that of the current set-in, the overall design and scale would not result in the onset of an unsatisfactory terracing effect, when viewed in the surrounding context with other examples in the road.

## Neighbour amenity

- 6.6 Both the council's Householder Extensions and Alterations SPG in addition to Policy DES1 of the Development Management Plan expect any proposal to have due regard to the amenity of neighbouring properties. The key residential amenity to consider in this instance would be the detached neighbour to the west, no.7 Garden Close, and the adjoining neighbour to the east, no.11. The neighbours to the rear, 7 and 8 Sandersfield Gardens are situated over 30m from the rear of the proposal, such that the existing relationship would not be subject to significant change.
- 6.7 The existing garage is situated adjacent to the boundary with the neighbour to the western side no.7, where there is a separation distance of approximately 0.45m from the boundary to the neighbour's flank wall. This relationship with this neighbour in terms of the built form would remain unchanged in that the side extension would feature the same distance set-in. Whilst it is acknowledged that the addition of a second storey and roof would increase the bulk and massing on this side, the proposal would not project beyond either building line, would not impact upon any side-facing windows, and would not itself feature any first-floor side facing windows. As such, whilst the existing relationship would be subject to some change, there would not be any overlooking, loss of privacy, nor an overbearing or overshadowing impact toward this neighbour.
- 6.8 Given that the proposal would not extend beyond the existing building lines, there is unlikely to be any greater impact upon the adjoining dwelling, no.11 than at the current time. Whilst construction traffic and noise may result in a temporary impact, statutory legislation is in place to deal with this. The proposal would therefore accord with policy DES1 of The Council's Development Management Plan and the Householder Extensions and Alterations SPG with regard to residential amenity.

## **CONDITIONS**

1. The development hereby permitted shall be carried out in accordance with the following approved plans.  
Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for

minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

Plan Type	Reference	Version	Date Received
Existing Plans	1829 01		22.01.2021
Location Plan	1829 LPR		21.12.2020
Proposed Plans	1829 02		18.12.2020
Block Plan	1829 BP500		22.01.2021

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.  
Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004
  
3. The materials to be used in the construction of the external surfaces of the extension (other than materials used in the construction of a conservatory) must be of similar appearance to those used in the in the construction of the exterior of the existing building.  
Reason: To ensure that the development hereby permitted is only constructed using the appropriate external facing materials or suitable alternatives in the interest of the visual amenities of the area with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

## INFORMATIVES

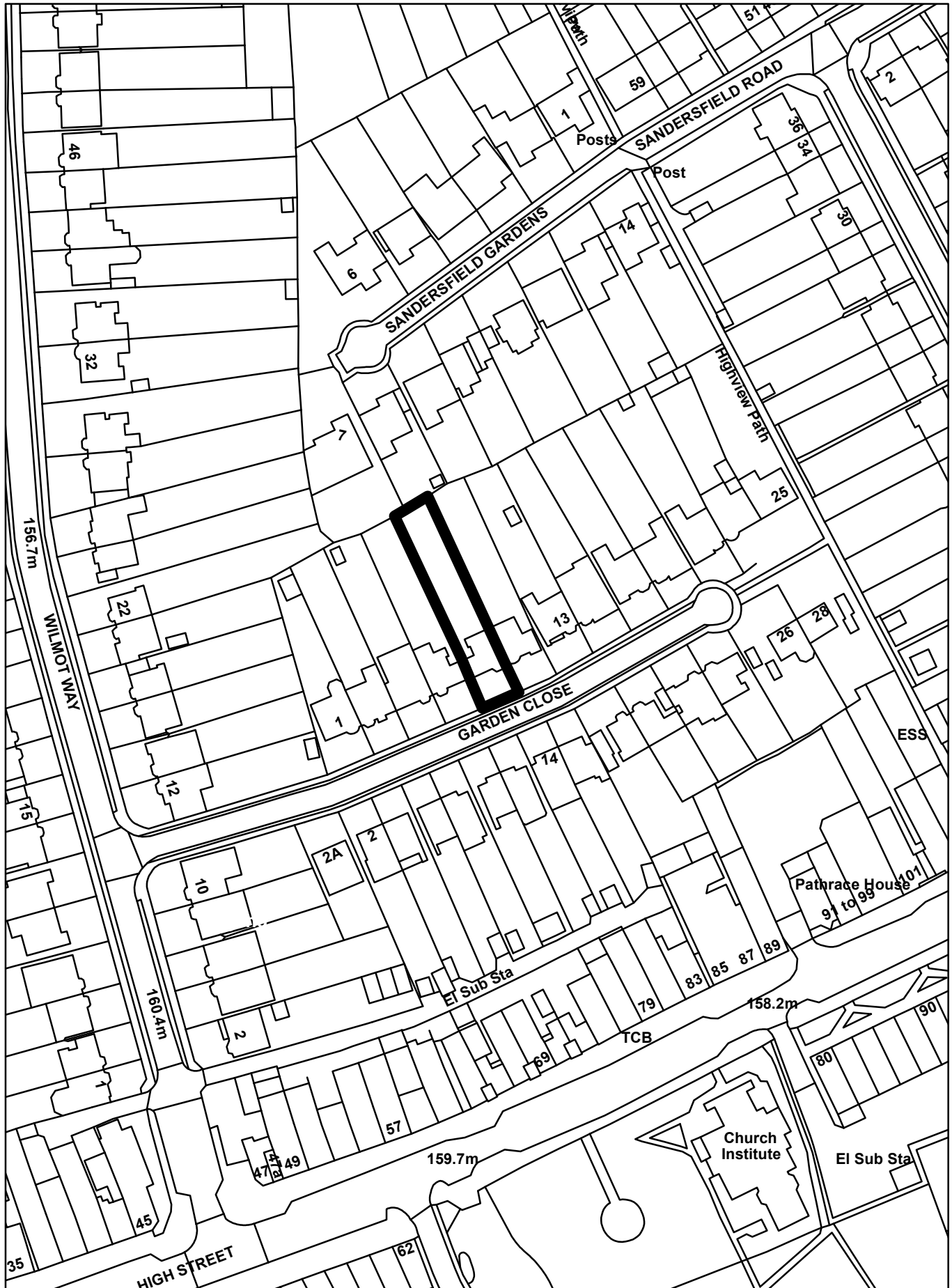
1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.org.uk](http://www.firesprinklers.org.uk).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).

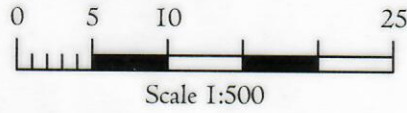
## REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policy DES1 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

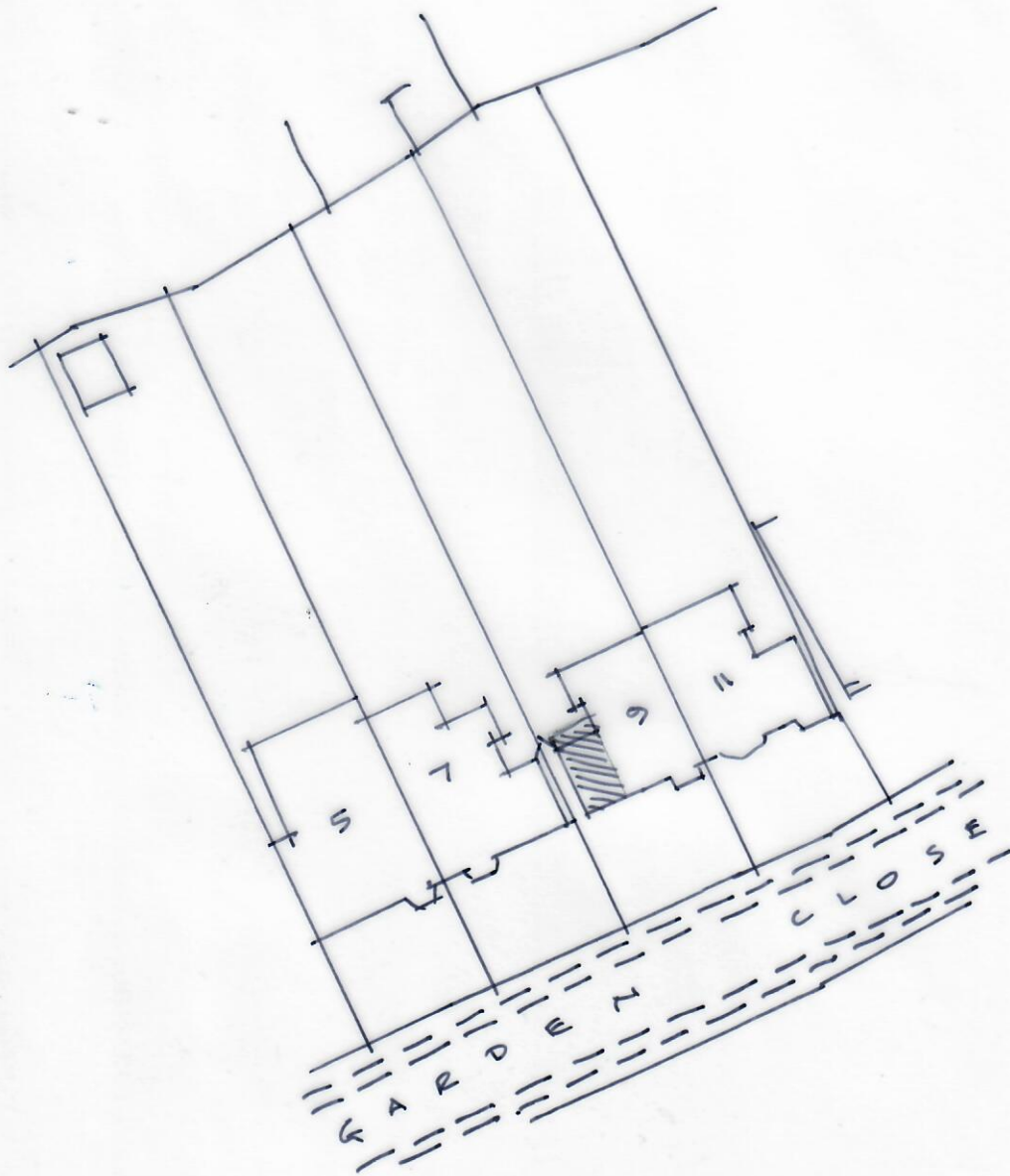
Agenda Item 6  
20/02840/HHOLD - 9 Garden Close, Banstead





**WAD**  
Associates

SBC House,  
Restmor Way,  
Wallington,  
Surrey. SM6 7AH.  
020. 8715. 9300



Drawing No. 1829/T/00.1      Date: 03.i.21      Scale: 1:500      By: GPW

Title: Supplementary Drawing: Block Plan

Address: 9, Garden Close, Banstead,  
Surrey. SM7 2QB.

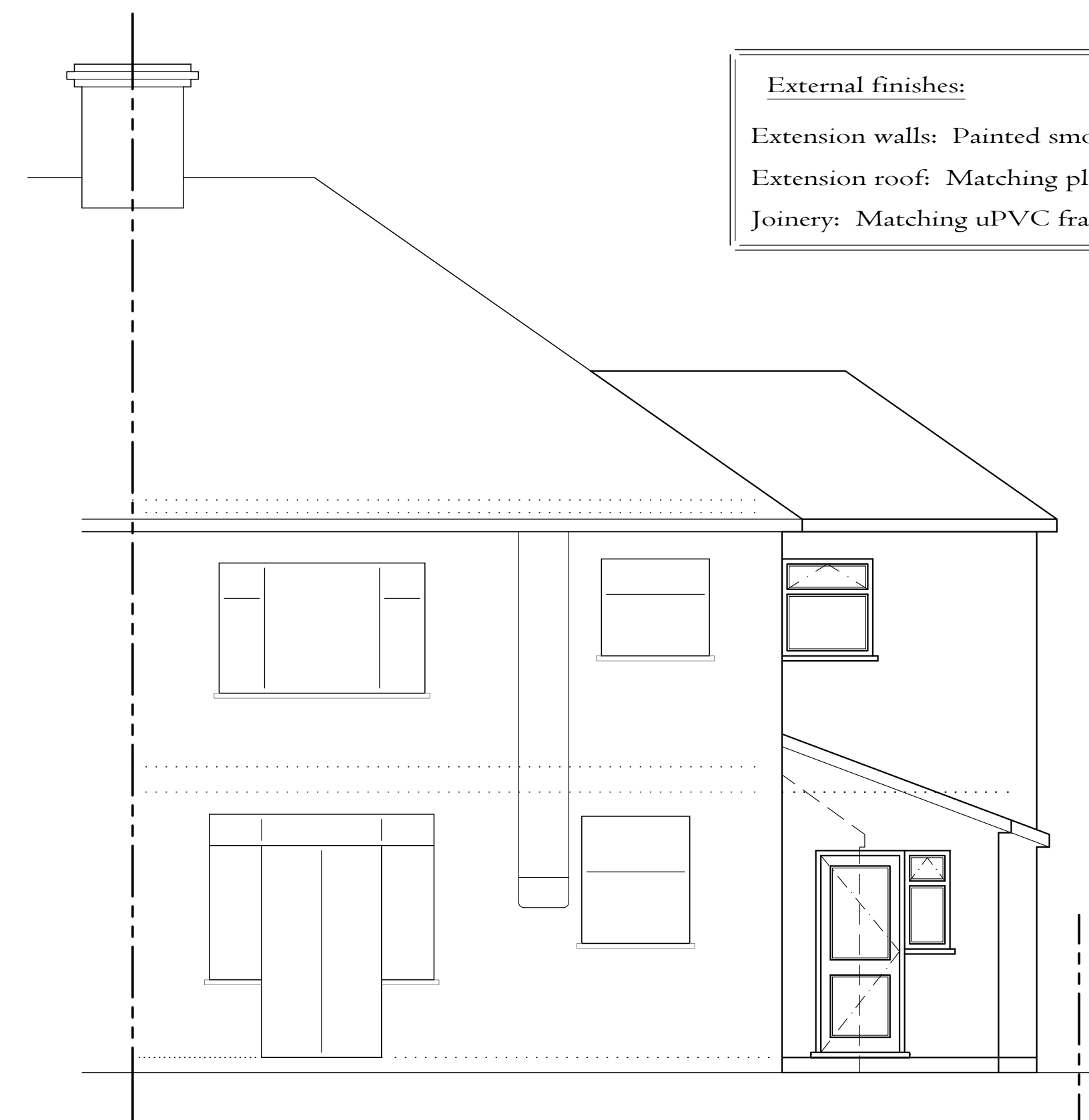
For: Mr. & Mrs. M. Trenaman.



PROPOSED FRONT ELEVATION



PROPOSED SIDE ELEVATION

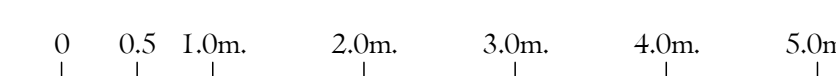


PROPOSED REAR ELEVATION  
Existing projection shown hidden for clarity

External finishes:  
Extension walls: Painted smooth render.  
Extension roof: Matching plain tiling, interlocking to rear.  
Joinery: Matching uPVC frames.

N.B. Party Wall etc. Act 1996:  
Construction of the extension shown is limited by the rights and responsibilities laid down in the Party Wall Act. In particular advance notice must be given to the owner of the adjoining property of the proposed work, together with limitations related to works close to the boundary. It is recommended that the Builder is familiar with the requirements of the Act.

© WAD Associates Ltd - MMXX. Use figured dimensions given in preference to scaling. All dimensions to be checked on site. This drawing for Planning applications purposes only. Any discrepancies to be reported immediately.

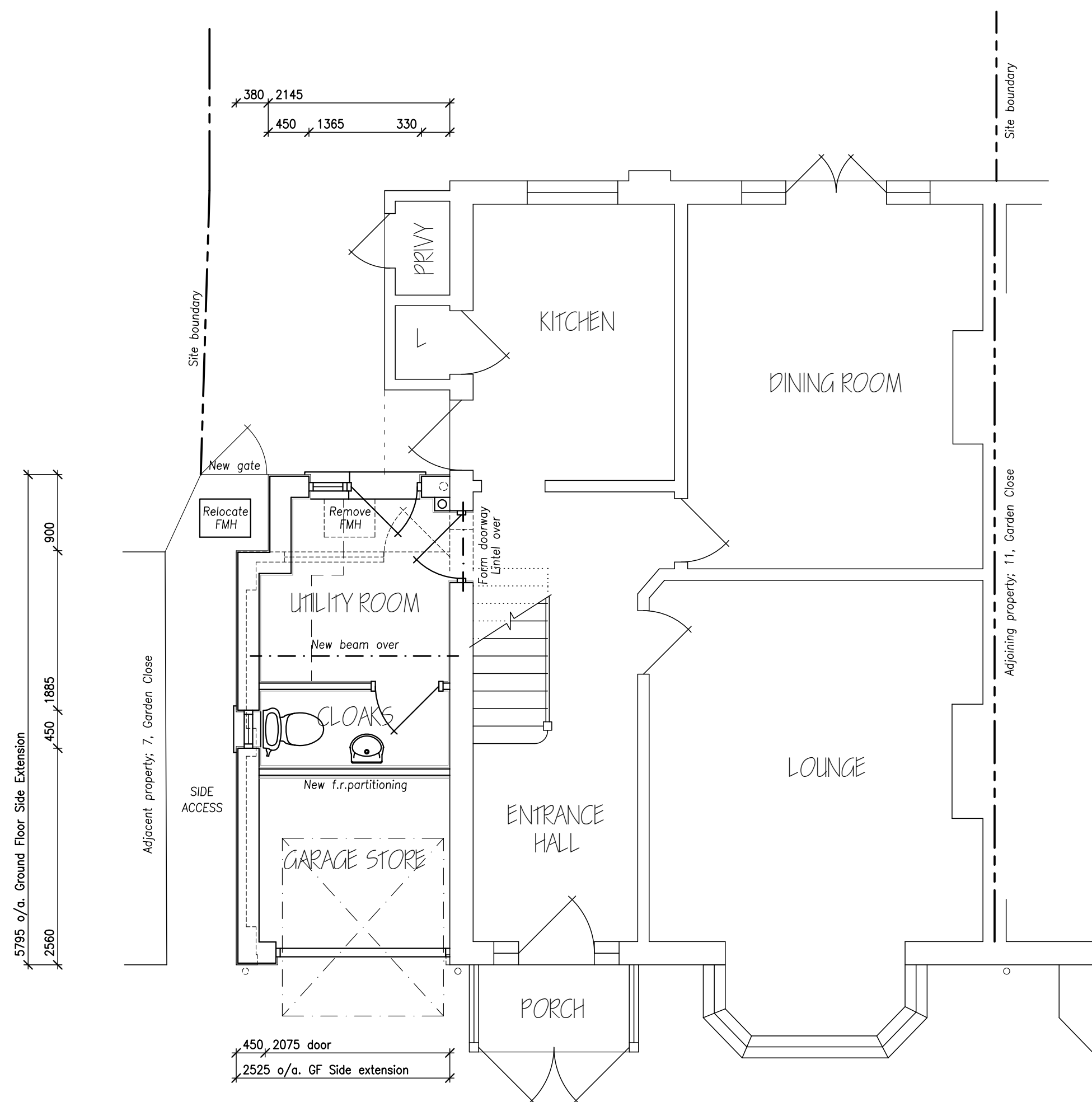


**W.A.D. Ltd.**  
**ASSOCIATES**  
SBC House, Restmor Way, Wallington, Surrey, SM6 7AH.  
T: 020. 8715. 9300 / 07831.244491 E: info@Williamson-Design.co.uk

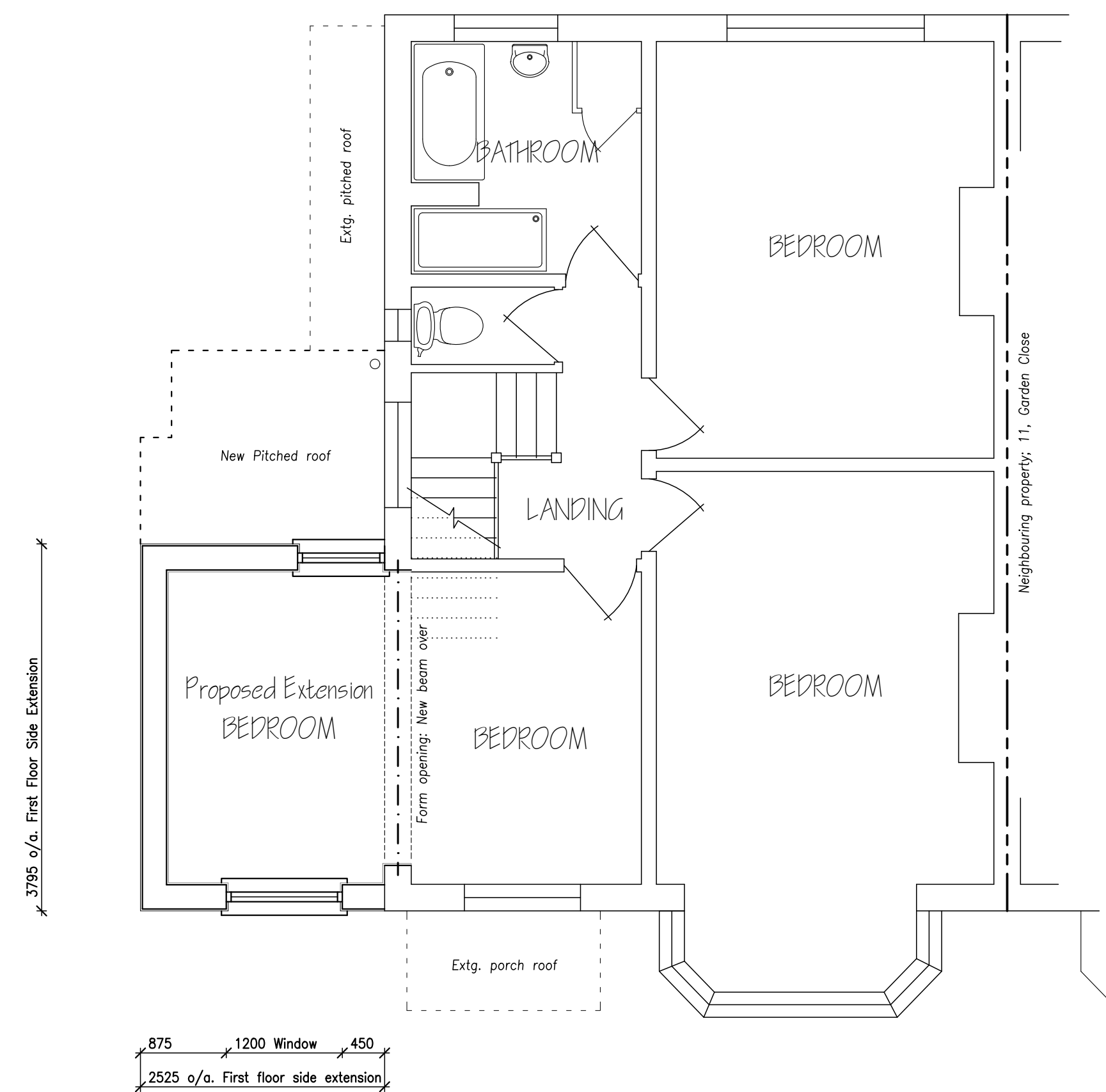
**Title** TWO STOREY SIDE EXTENSION  
**Address**  
9, GARDEN CLOSE,  
BANSTEAD, SURREY,  
SM7 2QB.  
**For** Mr. & Mrs. M. Trenaman.

Date NOV. 2020 Scale 1:50 (A1) Drawn EHW

Drawing Number  
E-1829-T-02



PROPOSED GROUND FLOOR PLAN




PROPOSED FIRST FLOOR PLAN

# Agenda Item 7

Planning Committee  
14<sup>th</sup> April 2021

Agenda Item: 7  
19/02559/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	14 <sup>th</sup> April 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Hollie Marshall
	<b>TELEPHONE:</b>	01737 276010
	<b>EMAIL:</b>	Hollie.marshall@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	7	<b>WARD:</b> Hooley Merstham and Netherne

<b>APPLICATION NUMBER:</b>	19/02559/F	<b>VALID:</b>	13 <sup>th</sup> January 2020
<b>APPLICANT:</b>	Turnbull Land	<b>AGENT:</b>	
<b>LOCATION:</b>	THE EPIPHANY HOUSE MANSFIELD DRIVE MERSTHAM REDHILL SURREY RH1 3JP		
<b>DESCRIPTION:</b>	The demolition of the existing properties and the erection of ten houses, comprising 2 x 2 bed and 4 x 3 bed terraced houses, 2 x 3 bed semi detached and 2 x 4 bed detached houses, with associated access and parking. As amended on 20/01/2020, 12/02/2020 and on 16/03/2020.		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the demolition of the existing buildings and the erection of ten houses, comprising 2 x 2 bed and 4 x 3 bed terraced houses, 2 x 3 bed semi detached and 2 x 4 bed detached houses, with associated access and 20 parking spaces. The application site is an allocated site for development for up to 10 residential homes with a list of criteria any future development will be subject to.

The application was determined at the Planning Committee meeting on 25<sup>th</sup> November 2020 and was approved with conditions. Since that time, an interested party, the UK Gospel Assembly Church (UKGAC), sought a judicial review on the basis that the red book valuation and supporting valuation documents submitted during the course of the application were not publicly available to view on the Council's website during the course of the application and thus failed to allow UKGAC the opportunity to comment on these documents. On 19<sup>th</sup> February 2021 the High Court sealed the order quashing the Council's original decision and the application now returns to the Planning Committee for a decision.

The site is allocated for residential development under policy RED4 of the 2019 Development Management Plan (DMP) which allocates the site for residential development of up to 10 homes, subject to the following requirements:



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- Development of a scale that reflects the character of the surrounding area and safeguards residential amenity – the proposal is considered acceptable in terms of the layout, scale and design of new dwellings and due to the separation distances to existing neighbouring dwellings, is not considered to result in a harmful impact upon neighbour amenity.
- Provide sufficient off-street parking in accordance with adopted local standards – the proposal includes 20 parking spaces, this accords with the minimum parking spaces numbers required by policy TAP1 and Annex 4 of the DMP.
- Measures to address and attenuate surface water flooding risk – no objection to the proposal is made by Surrey County Council as Lead Local Flood Authority subject to two conditions recommended that would be attached to a grant of planning permission.
- Appropriate improvements to the site access onto Mansfield Drive – no objection is raised by the County Highways Authority subject to a condition requiring each of the proposed vehicular accesses have been constructed in accordance with the approved plans.

The existing church constitutes a community facility to which the provisions of DMP policy INF2 and the Core Strategy (policy CS12) would normally apply.

To comply with the requirements of policy INF2 the application was submitted with supporting valuation and marketing information to demonstrate it was not viable for continued community use. During the course of the application Officers did seek to consider the marketing undertaken for completeness. A church group expressed an interest in the property and additional time was made for consideration of the application to allow time for viewing the site to be undertaken, which had previously not been, and to allow time for an offer to be made, which was subsequently submitted to the selling agent. However, this was below the market valuation. Following the quashing of the original decision, the UKGAC have submitted their own Red Book valuation which provides a valuation for the site of £765,000 and maintain their objections to the application. This valuation is below the Red Book Valuation submitted by the Applicant, which gave a valuation figure of £1,175,000. During the course of the marketing period, UKGAC did make an offer to purchase the site, however this was not accepted as it was below the valuation. The offer was 40.5% below the Applicant's Red Book valuation and 8.5% below the UKGAC Red Book valuation.

However, the requirement of policy INF2 is to satisfy either criteria a or b. It is considered the loss of the community facility would not result in a shortfall of local provision of this type and the requirements of policy INF2 are met, even if such were required by the housing allocation of policy RED4.

Policy RED4 though makes no requirements for replacement community use or the need to justify its loss and there is no in principle objection to residential development given the reasonably accessible, urban location and brownfield nature of the site.

Indeed, DMP considered the issue of the loss of the community use and the availability of other provision locally when allocating the site for residential. The long-term vacancy of the site, the proximity of other Anglican churches nearby, as well as



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19/02559/F

the new Merstham Hub and potential for community uses on the old library site all contributed to its allocation for residential without having to satisfy further tests relating to the loss of the community use and this situation is not considered to have significantly changed.

The DMP was adopted in September 2019 following consultation and examination and is therefore considered up to date with the proposal being in accordance with the plan.

## **RECOMMENDATION(S)**

Planning permission is **GRANTED** subject to conditions.

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14<sup>th</sup> April 2021

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## Consultations:

Planning Policy Team: *Further comments have been requested on the above application following the quashing of the original decision by the High Court.*

*The site is allocated for residential development under Policy RED4 of the Development Management Plan. The site was initially identified as part of the Housing Land Availability Assessment (M22) in 2018. The church had closed in 2015 and was being promoted for housing by the Diocese of Southwark.*

*The RED4 draft site allocation policy was subject to public scrutiny as part of the Regulation 18 consultation held between August and October 2016 and Regulation 19 consultation held between January and May 2018 in accordance with the Town and Country Planning Act 1990 and Planning and Compulsory Purchase Act 2004.*

*Representations requested more parking, being of a scale and character with the neighbouring residential area and more intensive development. There were no representations requesting the retention of the site for religious or any other community purposes.*

*The site allocation policy subsequently went through a robust examination between August 2018 and April 2019. The examination resulted in one Major Modification (MM25) requiring the consideration of surface water flooding and one minor modification to the wording of the criteria introduction. The Council subsequently adopted the Plan in July 2019. Following the post adoption legal challenge period, in the summer of 2019, under policy RED4, the site and its former use had been released. The INF2 infrastructure protection policy no longer applied.*

*Government has repeated the assertion that it supports a Plan led approach to new development. Policies within the Plan are there to provide certainty for communities and investors.*

*With the designation made and adopted by the Council, planning considerations are limited to the requirements identified in Policy RED4 namely:*

- Development of a scale that reflects the character of the surrounding area and safeguards residential amenity*
- Provide sufficient off-street parking in accordance with adopted local standards*
- Measures to address and attenuate surface water flooding risk*
- Appropriate improvements to the site access onto Mansfield Drive*

*These issues have each been addressed:*

- The proposal is considered acceptable in terms of the layout, scale and design of new dwellings and due to the separation distances to existing neighbouring dwellings, is not considered to result in a harmful impact upon neighbour amenity.*
- The proposal includes 20 parking spaces which accords with the minimum parking spaces numbers required by policy TAP1 and Annex 4 of the DMP.*

- *Surrey County Council as Lead Local Flood Authority made no objection subject to two conditions that would be attached to a grant of planning permission.*
- *The County Highways Authority raised no objection subject to a condition requiring each of the proposed vehicular accesses have been constructed in accordance with the approved plans.*

*Consequently, Planning Policy does not object to the proposal subject to it meeting the RET4 policy requirements.*

Highway Authority: The County Highway Authority has undertaken an assessment in terms of the likely net additional traffic generation, access arrangements and parking provision and are satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway. The County Highway Authority therefore has no highway requirements subject to conditions.

The information submitted following the highways response dated 23/03/2020 do not show any signs of a highway impact. No change to previous comments received.

Divisional Crime Prevention Design Advisor – recommends a planning condition or informative is included to require the development to achieve standards contained within the Secured by Design award scheme to be successfully granted the award.

Sustainable Drainage SCC - Thank you for consulting Surrey County Council as the Lead Local Flood Authority on the above Full Planning Application. We have reviewed the surface water drainage strategy for the proposed development and assessed it against the requirements of the NPPF, its accompanying PPG and the Non-Statutory Technical Standards for sustainable drainage systems.

The following documents submitted as part of the above application have been reviewed and should be referred to as part of any future submissions or discharge of planning conditions:

- Flood Risk Assessment & SuDS Report, Nimbus, Feb 2020, revision A, document reference: C2316-R1-REV-A;

We are satisfied that the proposed drainage scheme meets the requirements set out in the aforementioned documents and are content with the development proposed, subject to our advice below.

Our advice would be that, should planning permission be granted, suitably worded conditions are applied to ensure that the SuDS Scheme is properly implemented and maintained throughout the lifetime of the development

‘As there is no change to the drainage strategy or surface water drainage system we would have no further comments, please refer back to our letter dated 24/02/2020 reference LLFA-RE-20-0124’

Thames Water – Recommends a condition regarding waste water and informatives regarding ground water

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Neighbourhood Services – require a bin presentation point.

Minerals and Waste Planning SCC – ‘We do not have any specific comment to make on this application, but please keep us informed of any further consultations.’

Housing – no comments received

Environmental Health – no comments received

The Reigate Society – no comments received

Sutton and East Surrey Water Company – no comments received

UK Power Networks – no comments received

Crawley Hospital – no comments received

Infrastructure Agreements Manager SCC – no comments received

Policy and Community Initiatives – no comments received

## **Representations:**

Letters were sent to neighbouring properties on 16<sup>th</sup> and 21<sup>st</sup> January 2020 a site notice was posted 20<sup>th</sup> January 2020 and advertised in local press on 30<sup>th</sup> January 2020

8 responses and a petition of 444 signatures have been received raising the following issues:

<b>Issue</b>	<b>Response</b>
Loss of community facility	See paragraph 6.3 – 6.18
Loss of buildings	See paragraph 6.23 and condition 15
No need for the development	See paragraph 6.1
Out of character with surrounding area	See paragraph 6.19 – 6.24 and condition 4
Overlooking and loss of privacy	See paragraph 6.25 – 6.24
Overshadowing	See paragraph 6.25 – 6.24
Harm to wildlife habitat	See paragraph 6.41 – 6.43 and conditions 14 and 16
Loss of/harm to trees	See paragraph 6.35 – 6.40 and condition 5 and 6
Inconvenience during construction	See paragraph 6.27

Loss of a private view	This is not a material planning consideration
Noise and disturbance	See paragraph 6.27

Letters were sent to neighbouring properties on 28<sup>th</sup> January 2021 and site notices posted on 4<sup>th</sup> February 2021, following the quashing of the previous decision. 4 responses have been received to this consultation; 3 on behalf of the UK Gospel Assemble Church and 1 representation in support of the application from a resident raising the following issues:

<b>Issue</b>	<b>Response</b>
Loss of community facility	See paragraph 6.3 – 6.18
Valuation of site	See paragraph 6.3 – 6.18
Marketing exercise not compliant with policy INF2	See paragraph 6.3 – 6.18
Support – benefit to housing need	See paragraph 6.29
Support – Community/regeneration benefit	See paragraph 6.28
Support – visual amenity benefits	See paragraph 6.19 – 6.23

## **1.0 Site and Character Appraisal**

- 1.1 The site presently comprises the now vacant, 2 storey scale church building with single storey church hall and detached two storey vicarage set within a large plot. There are large open grassed areas fronting Mansfield Drive and to the rear, with established hedgerow along the front boundary to Mansfield Drive and substantial tree cover to the rear (eastern/south eastern) boundary.
- 1.2 The site is within a broadly residential locale, typified by terraces of post-war (1950s) two storey housing and three storey flatted blocks interspersed with areas of amenity space. To the south-west of the site is a large area of open land beyond which is Portland Drive which is itself the subject of redevelopment.

## **2.0 Added Value**

- 2.1 Improvements secured at the pre-application stage: Advice was given regarding the requirements of Policy RED4 and improvements to design and tree cover.
- 2.2 Further improvements could be secured: conditions regarding materials, levels, landscaping, trees, drainage and highways are recommended

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Planning Committee  
14<sup>th</sup> April 2021

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19/02559/F

## 3.0 Relevant Planning and Enforcement History

- |     |            |   |   |
|-----|------------|---|---|
| 3.1 | 04/02065/F | Installation of a decking area approx. 15.4m x 3.4m at the rear of the church hall.   | Approved with conditions<br>19 <sup>th</sup> November<br>2004 |
| 3.2 | 81P/0999/F | Former curates house adj Epiphany Church, Mansfield Drive, Merstham. Use as a communal home for ex-patients of netherne hospital. | Approved with conditions<br>5 <sup>th</sup> November 1981     |
| 3.3 | 78P/0014   | Use of church hall as a play group for up to 24 children between 9.30 am & 12.30 pm Tuesday to Friday during school terms.        | Approved with conditions<br>9 <sup>th</sup> March 1978        |

## 4.0 Proposal and Design Approach

- 4.1 This is a full application for the demolition of the existing properties and the erection of ten houses, comprising 2 x 2 bed and 4 x 3 bed terraced houses, 2 x 3 bed semi detached and 2 x 4 bed detached houses, with associated access and parking
- 4.2 The application proposes a pair of semi-detached houses and a terrace of three dwellings to fronting Mansfield Drive. These dwellings would follow the established building line in the street. Between these two buildings would be the access road into the rear of the development where two detached houses and another terrace of three houses would be sited.
- 4.3 The dwellings would be of a traditional design with gable roofs and the finishing materials proposed would have a contemporary appearance. The dwellings would have an eaves height of 5.3m and ridge height of 8.1m. There is variation in the wider area, that includes the more recent developments along Portland Drive.
- 4.4 A total of 20 parking spaces are proposed to serve the development, this includes 2 visitor parking bays sited parallel to the access road.
- 4.5 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;

Involvement;  
Evaluation; and  
Design.

4.6 Evidence of the applicant’s design approach is set out below:

Assessment	The character of the surrounding area is assessed as mainly characterised by 1950’s semi detached and terraced post-war properties set over 2 storey, including three storey flatted apartments. To the south west of the site, comprises a large area of open land beyond which is Portland Drive, currently being developed by Raven Housing. The site is ideally located for Merstham train station, local school and shops. In terms of materials, many of the local housing stock are finished in brick and plain tiles.
	No site features worthy of retention were identified.
Involvement	No community consultation took place.
Evaluation	The statement does not include any evidence of other development options being considered.
Design	The applicant’s reasons for choosing the proposal from the available options were to provide a positive contribution to the area, whilst increasing the Councils housing stock and would provide ten houses that are designed to be comparative to existing housing stock, both in terms of the materials and the colour palette being proposed.

4.7 Further details of the development are as follows:

Site area	3272sqm
Existing use	Church – Residential allocation
Proposed use	Residential
Existing parking spaces	0
Proposed parking spaces	20
Parking standard	20
Net increase in dwellings	10
Proposed site density	31 dwellings per hectare
Density of the surrounding area	36 dwellings per hectare (13 – 29 Mansfield Drive)

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## 5.0 Policy Context

### 5.1 Designation

Urban area  
Urban Site Allocation  
Medium Accessibility Parking Standards

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS2 (Valued Landscapes and Natural Environment),  
CS4 (Valued Townscapes and Historic Environment)  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),  
CS15 (Affordable Housing)

### 5.3 Development Management Plan 2019

DES1 (Design of New development)  
DES4 (Housing mix)  
DES5 (Delivering high quality homes)  
DES8 (Construction Management)  
TAP1 (Access, Parking and Servicing)  
CCF1 (Climate Change Mitigation)  
CCF2 (Flood risk)  
INF2 (Community facilities)  
INF3 (Electronic communication networks)  
RED4 (Church of Epiphany, Merstham)  
NHE3 (Protecting trees, woodland areas and natural habitat)

### 5.4 Other Material Considerations

National Planning Policy Framework  
National Planning Practice Guidance  
Supplementary Planning Guidance

Surrey Design  
Local Distinctiveness Design Guide  
A Parking Strategy for Surrey  
Parking Standards for Development  
Affordable Housing

Other

Human Rights Act 1998  
Community Infrastructure Levy  
Regulations 2010



## 6.0 Assessment

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development and where the principle of such residential development is acceptable in land use terms.

6.2 The main issues to consider are:

- Loss of community facility
- Design appraisal
- Neighbour amenity
- Housing mix
- Amenity for future occupants
- Access and parking
- Impact on trees
- Ecology
- Sustainable construction
- Drainage
- Infrastructure contributions
- Affordable Housing

### Loss of community facility

6.3 The site is allocated for residential development within the 2019 DMP. This is a recently adopted and up to date plan which has undergone extensive and robust consultation and examination. During the consultation and examination no representations were received regarding the need for its ongoing community use and rather, the site had been long vacant with sufficient alternative provision in the local area and the potential for further provision at the library site. For this reason the DMP allocates the site without it needing to meet the tests of Policy INF2 or other tests relating to the loss of the community use. This allocation alone should therefore be sufficient to establish the principle of residential use on the site. Nevertheless, as the existing church constitutes a community facility for the purposes of Development Management Plan policy INF2 and the provisions of the Core Strategy (policy CS12), consideration against these policies has also been given.

6.4 As per policy INF2 *'loss or change of use of existing community facilities will be resisted unless it can be demonstrated that the proposed use would not have an adverse impact on the vitality, viability, balance of services and/or evening economy of the surrounding community; and*

*a. Reasonable attempts have been made, without success, for at least six months to let or sell the premises for its existing community use or for another community facility that meets the needs of the community (see Annex 3 for details on what will be required to demonstrate this); or*

*b. The loss of the community facility would not result in a shortfall of local provision of this type, or equivalent or improved provision in terms of quantity and quality, or some wider community benefits, will be made in a suitable location.*

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- 6.5 In support of the application the following marketing information was submitted:
- Christopher St. James valuation letter dated 6<sup>th</sup> June 2019
  - SHW valuation letter dated 3<sup>rd</sup> June 2019
  - James Commercial valuation letter dated 11<sup>th</sup> June 2019
  - James Commercial marketing report dated 18<sup>th</sup> December 2019
- 6.6 The Council's Property Team reviewed the information submitted and were satisfied that the valuers have the necessary expertise and knowledge to assess the open market value of the property for the existing use. The values, as of June 2019, were £1.2m, £1.25m and £1.3m which is a normal range.
- 6.7 The property has subsequently been marketed by James Commercial at the lowest of the valuations both on a sale and rental basis. Based on the marketing report the Property Team were satisfied that the property has been fully exposed to the market for a reasonable period of time. It is clear from the Schedule of interest that there have been very few enquiries for church use and none for alternative uses other than residential. This lack of demand could be for a number of reasons including location, the condition of the property and other factors such as dwindling population of church going people.
- 6.8 During the course of the application representation was received from the UK Gospel Assembly Church (UKGAC) who stated they had expressed interest in the site however were not provided with opportunity to view the site or make an offer during the marketing period. This expression of interest was not reflected in the marketing report and therefore clarification on this was sought from the Applicant.
- 6.9 The Applicant then confirmed that a viewing was undertaken on 27<sup>th</sup> February 2020 and the UK Gospel Assembly Church made further representation in regard to their interest in the site and requested sufficient time to finalise their offer on the property. This representation was received on 5<sup>th</sup> March and subsequently, in further correspondence, confirmation an offer was made on 26<sup>th</sup> June 2020 was received.
- 6.10 Following this, in early July 2020, an updated marketing report was requested from the Applicant to include this and provide a response on this offer, and any other offers or interest in the site to date since the completion of the report originally submitted. The Applicant subsequently submitted the following additional marketing information:
- James Commercial marketing report dated 12 July 2020 – which refers to unspecified period of further marketing
  - James Commercial review dated 7 July 2020 of UK Gospel Assembly Church's offer, recommending rejection of the offer
  - Christopher St. James updated 'valuation' dated 14 July 2020
  - SHW letter dated 6 July 2020 providing updated valuation
- 6.11 The Council's Property Team were re-consulted and were of the view the marketing report does not meet the requirements of Annex 3 and were not

satisfied that the Agent has marketed the property at a price that properly reflects the market value in relation to condition, quality and location. The Applicant subsequently offered to provide a Red Book Valuation (a valuation undertaken in accordance with the relevant requirements of the RICS Red Book) in support of the price at which the property is being marketed for existing use value. This would inform whether a further marketing campaign should be undertaken at a revised price to demonstrate that the requirements of annex 3 and policy have been met.

- 6.12 A valuation report was submitted, dated 28<sup>th</sup> September 2020, which gave a valuation of approximately 2.1% lower than the £1.2million asking price for the site. The offer made by the UK Gospel Assembly Church was approximately 40.5% lower than the Red Book valuation price. On this basis, in the absence of an acceptable offer during the marketing period, it was considered that the requirements of policy INF2 had been met, and there was no in principle objection to residential development given the reasonably accessible, urban location and brownfield nature of the site and having regard to policy RED4 which allocates the site for residential development of up to 10 homes.
- 6.13 Since the conclusions of the previous assessment, as per the discussions above, the decision to approve the application has been quashed and the application must now be determined afresh. The interested party in the application site, the UKGAC, have had sight of the Red Book valuation and supporting documents and these are viewable on the Council's website. The UKGAC have submitted further objection to the application on the basis of the marketing exercise, the valuation of the site submitted by the Applicant and have also commissioned their own Red Book valuation of the site which gives a valuation of £765,000. This is lower than the Applicant's Red Book valuation which gave a valuation figure of £1,175,000. The UKGAC made an offer to purchase the site on 29<sup>th</sup> June 2020 'subject to contract' of £700,000. The offer was rejected by the Diocese of Southwark on the basis it was below the value. Based on the recent Red Book valuation submitted by the UKGAC, the offer remains below this value, at 8.5% lower (and 9.7% lower than the Crow Watkins valuation of £775,000 dated 8<sup>th</sup> June 2020 submitted by the UKGAC).
- 6.14 The requirement of Policy INF2 is to satisfy either criteria a or b. In this instance, the site is allocated for up to 10 homes by way of policy RED4. At the time of preparing the DMP, the Church of Epiphany was no longer being actively used as a church – it had closed in January 2015 due to safety concerns. There are two other Anglican churches within Merstham (All Saints and St Katharine's) which with St Andrews, Reigate form part of the Merstham and Gatton Ministry. Given that the other churches met this faith need in Merstham, the site was allocated for residential accommodation in the DMP.
- 6.15 However, at the time there were discussions between SCC, The Diocese of Southwark and Merstham Baptist Church regarding possible faith provision on Merstham Library site rather than housing. Therefore, the library site allocation policy was made flexible for Residential accommodation and/or Community facilities: replacement of nearby community use (on RED4) or other relevant community use.

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- 6.16 In the allocation of the site, there has been consideration had for local provision of community facilities. There is, nearby to the site, community facility provision in the newly built Merstham Hub where space is currently rented by UKGAC.
- 6.17 During the course of the application Officers did seek to consider the marketing undertaken for completeness albeit it is accepted that greater weight was attributed to this than was necessary. There have been differing valuations of the site submitted by the Applicant and the interested party, UKGAC. During the course of the marketing period a revised offer was not submitted by UKGAC following the decline of the offer made in June 2020, which remains below the UKGAC's previous valuation by Crow Watkins and more recent Red Book valuation by Fifield Glyn. The Applicants have since submitted a rebuttal letter from SHW, the author of their Red Book valuation, which raises concerns with the Fifield Glyn Red Book valuation report.
- 6.18 Policy INF2 requires either criteria a or b to be satisfied in order to comply. It is considered the loss of the community facility would not result in a shortfall of local provision of this type and the requirements of policy INF2 are met, even if such were required by the housing allocation of policy RED4.

## Design appraisal

- 6.19 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.20 The application proposes a residential development of ten, two storey houses made up of detached, semi-detached and terraced properties. The site adjoins existing residential dwellings that front Mansfield Drive. The proposed dwellings fronting this road would follow the existing building line and be of a traditional form with gable roofs, as per the existing houses. The new dwellings would have a more contemporary appearance achieved through the finishing materials and more modern fenestration, however this is not considered to result in a harmful impact upon the streetscene given the new development in the area to the south west in Portland Drive creating variety in the character of the area. The central dwelling of the two terraces would feature a front facing gable, lower in height than the main ridge; this would help break up the mass of the terrace and provide visual interest.
- 6.21 The dwellings include frontage parking and areas of front garden for soft landscaping. The access road into the site has opportunity for landscaping, particularly along the north western side and to the rear part of the site a further five dwellings are proposed, a pair of detached houses and a terrace of three.

- 6.22 Each dwelling would have a front and rear garden with plot sizes commensurate with those fronting Mansfield Drive and those to the rear of the application site in Huddleston Crescent. Gaps to the side boundaries are such that the proposal would not appear cramped and the heights of the proposed dwellings would be similar to the existing neighbouring houses resulting in a development that would integrate with the character of the area.
- 6.23 The existing buildings are not of particular architectural merit and their loss is not considered to warrant refusal of the application. However, the explanation to policy RED4 of the DMP states efforts should be made to salvage the sculpture on the building as an undesignated heritage asset. A condition is recommended to secure the safe recovery of the sculpture, with details of a proposal for the salvaging of the sculpture to be submitted.
- 6.24 It is considered therefore that the proposals comply with the provisions of DMP Policy DES1.

### Neighbour amenity

- 6.25 The proposed development has been assessed with regards to its impact on the amenity of neighbouring properties. To the rear of the site are properties fronting Huddleston Crescent. The rear of the new dwellings would have garden depths of between approximately 10 – 16m, and separation distance of between approximately 23 – 31m between rear elevations. These distances are considered acceptable to avoid a harmful impact upon neighbouring amenity by way of overbearing, domination, overlooking and loss of privacy.
- 6.26 The nearest plot to 13 Mansfield Drive would have a separation distance of 6m between the two flank walls, and the nearest plot to 11 Mansfield Drive would have a distance of 8m. Given the level of separation, the proposal is not considered to result in a harmful impact upon the amenities of these properties.
- 6.27 Objection was raised on the grounds of inconvenience during the construction period and noise and disturbance. The proposed use of the site would be residential and therefore is not considered to result in a harmful impact in terms of noise and disturbance. Whilst it is acknowledged there may be a degree of disruption during the construction phase, the proposal would not warrant refusal on this basis and statutory nuisance legislation exists to control any significant disturbance caused during the construction of the proposal.
- 6.28 Overall, it is considered the proposal would not have a significant adverse effect upon existing neighbouring properties and would accord with the provisions of DMP Policy DES1.

### Housing Mix

- 6.29 DMP Policy DES4 relates to Housing Mix and states that all new residential developments should provide homes of an appropriate type, size and tenure to meet the needs of the local community. The proposed housing mix must on

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sites of up to 20 homes, at least 20% of market housing should be provided as smaller (one and two bedroom) homes. In this case, the scheme would provide 2 x 2 bedroom dwellings and would accord with the terms of the policy.

## Amenity for future occupants

6.30 The NPPF provides that planning decisions should provide a high standard of amenity for future users. DMP Policy DES5 relates to the delivery of high quality homes and requires, *inter alia*, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units except where the Council accepts that an exception to this should be made in order to provide an innovative type of affordable housing that does not meet these standards. In addition, the policy also requires all new development to be arranged to ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.

6.31 All units would meet the minimum internal spaces standards.

Housing Type A x 4 – 3 bedroom, 5 person – 103sqm  
Housing Type B x 2 – 2 bedroom, 4 person – 83.5sqm  
Housing Type C x 2 – 4 bedroom, 6 person – 111sqm

6.32 Each dwelling would be orientated south west (front elevation)/north east (rear elevation) and therefore main habitable rooms would be afforded adequate levels of daylight and sunlight. The resultant plot sizes are considered to create an adequate level of amenity for future occupants with acceptable private outdoor amenity space for each dwelling and the proposal is therefore considered to comply with the requirements of policy DES5.

## Highway matters

6.33 The site is located in an area which is assessed as having a medium accessibility rating. In such areas, the Council's adopted parking standards require the provision of 1 space for each 2 bedroom house, 2 spaces for 3 and 4 bedroom houses and 2 visitor parking spaces. The application proposes a total of 20 parking spaces. This number includes the provision of 2 visitor parking spaces. The parking would therefore meet the minimum requirements of DMP policy TAP1 and Annex 4.

6.34 The proposed development has been reviewed by the County Highway Authority, with regard to highway safety, capacity and policy matters, who raise no objections subject to the imposition of a number of conditions. Accordingly, the application is considered acceptable in this regard.

## Impact on trees

6.35 The application has been supported by detailed arboricultural information compiled in accordance with the advice and guidelines contained within British Standard 5838:2012. Trees have been assessed for their suitability for

retention within the proposed development adopting the criteria and methodology from section 4 and table 1 of the above standard.

- 6.36 The arboricultural information comprises of arboricultural impact assessment (AIA) arboricultural method statement (AMS) and tree protection plan (TPP). The AIA identifies trees for removal that are lost directly to the development or as a result of indirect loss such as condition of tree or to provide room for replacement plantings. The trees lost either directly or indirectly are of lower categories and no 'B' Grade trees are lost as a result of the proposal. The tree lost as a result of this development are domestic fruit trees and would not be a sustainable reason for refusal.
- 6.37 The AMS and TPP sets out the tree protection measures and no proposed development or any part of the demolition process will occur within the calculated root protection areas The AMS also contain information on qualified supervision and monitoring by a retained consultant which require further consideration and would need to be upgraded for a site where high levels of construction activity and processes are expected through both phases of demolition and construction.
- 6.38 The supplied information does include any engineering details on the installation of services or drainage routes which are normally designed post decision. From experience contractors rarely adhered to NJUG guidelines and if left unmanaged significant and lasting harm to rooting environments will.
- 6.39 The Council will require a 'finalised' AMS and TPP to cover the areas of concern mentioned above, sufficient qualified arboricultural information has been supplied in order to reach an informed and balanced decision on the arboricultural and landscape matters.
- 6.40 The Tree Officer has recommended a tree protection and landscaping condition to be attached to a grant of planning permission and an informative be imposed in full on the decision notice as this clearly sets out the expectation and requirements from the imposed conditions expected by the Council.

## Ecology

- 6.41 The site is not subject to any designation to indicate a particular importance for nature conservation interests, but it does contain trees and shrubs, some of which would be lost as a result of the development, and two buildings to be demolished. Policy NHE2 of the DMP expects in such locations without a particular designation that development proposals be designed, wherever possible, to achieve a net gain in biodiversity. The proposal does not include analysis of present biodiversity interests, nor measures for biodiversity enhancement.
- 6.42 Given the overall size of the site and opportunities for planting within gardens and on landscape strips with provision for bat and nest boxes, identification of suitable measures for enhancement of biodiversity could reasonably dealt with by a planning condition. A condition is recommended to enable the

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expectations of Policy NHE2 to be met. Protected species legislation applies independently of planning permission.

- 6.43 With regard to boundary treatments, to be secured by condition, the developer is encouraged to incorporate measures to promote biodiversity and wildlife and to allow wildlife to move into and out of gardens, such as hedgehog friendly gravel boards, where appropriate. Details of the 'wildlife friendly' measures should be identified within the submission of the details for approval.

## Sustainable Construction

- 6.44 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. No evidence has been submitted to demonstrate that the proposed development can achieve either of the two requirements. However, in the event that planning permission is to be granted, a condition could be imposed to seek such information and its implementation prior to the first occupation of development. In this regard, there would be no conflict with DMP Policy CCF1.
- 6.45 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services.

## Drainage

- 6.46 Policy RED4 of the DMP states development of the site will be subject to measures to address and attenuate surface water flooding risk. The County Highways Authority were consulted upon the application as the Lead Local Authority and are satisfied that the proposed drainage scheme is acceptable subject to recommended conditions.

## Refuse collection

- 6.47 Neighbourhood Services have commented that the road surface is brick paved which is not appropriate for a 26 tonne rear steer refuse vehicle and that properties will need to present their bins adjacent to Mansfield Drive so a bin presentation point will need to be provided that will be able to house the number and type of recycling and refuse bins that are required to be supplied by the developer.
- 6.48 A condition is recommended to require waste management collection points within the site, with details to be submitted and plans where appropriate, and the surface of the road can be secured by way of this condition to ensure it is suitable for access for the refuse and recycling lorries.

## Community Infrastructure Levy (CIL)



- 6.49 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable, although the exact amount would be determined and collected after the grant of planning permission.

Affordable housing

- 6.50 DMP Policy DES6 relates to the provision of affordable housing. This states that on all sites which provide 11 or more homes, 30% of the homes on the site should be affordable housing. This proposal would not therefore qualify for the provision of affordable housing.

## CONDITIONS

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason:

To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Elevation Plan	6776 - PA03	A	08.01.2020
Floor Plan	UNNUMBERED		08.01.2020
Floor Plan	UNNUMBERED		08.01.2020
Floor Plan	6776-PA05		13.01.2020
Elevation Plan	6776-PA02		19.12.2019
Other Plan	UNNUMBERED		12.02.2020
Location Plan	UNNUMBERED		19.12.2019
Elevation Plan	6776-PA04		19.12.2019
Arb / Tree Protection Plan	19-862-TPP		19.12.2019
Site Layout Plan	6776 - PA01	F	16.03.2020

Reason:

To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

3. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason:

To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the

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visual amenities of the locality with regard Development Management Plan 2019 policy DES1.

4. No development shall take place until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason:

To ensure that a satisfactory external appearance is achieved of the development with regard to Development Management Plan 2019 policy DES1.

5. No development shall commence including demolition and or groundworks preparation until a detailed, scaled 'finalised' Tree Protection Plan (TPP) and the related finalised Arboricultural Method Statement (AMS) shall be compiled in conjunction with the construction method statement is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings and location of site offices. The AMS shall also include a pre commencement meeting, supervisory regime for their implementation & monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason:

To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

6. No development shall commence on site until a scheme for the landscaping and replacement tree planting of the site including the retention of existing landscape features has been submitted and approved in writing by the local planning authority. Landscaping schemes shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or use of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason:

To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Borough Local Plan 2019, British Standards including BS8545:2014 and British Standard 5837:2012

7. No part of the development shall be first occupied unless and until each of the proposed vehicular accesses have been constructed in accordance with the approved plans.

Reason:

In order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

8. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with the approved plans for vehicles to be parked. Thereafter the parking areas shall be retained and maintained for their designated purposes.

Reason:

In order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

9. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason:

The condition above is required in order to reduce carbon emissions and help tackle climate change to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

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10. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) measures to prevent the deposit of materials on the highway
  - (e) on-site turning for construction vehicles (including measures for traffic management)
  - (f) construction hours
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason:

In order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

11. The development hereby permitted shall not commence until details of the design of a surface water drainage scheme have been submitted to and approved in writing by the planning authority. The design must satisfy the SuDS Hierarchy and be compliant with the national Non-Statutory Technical Standards for SuDS, NPPF and Ministerial Statement on SuDS. The required drainage details shall include:

- a) The results of infiltration testing completed in accordance with BRE Digest: 365 and confirmation of groundwater levels.
- b) Evidence that the proposed final solution will effectively manage the 1 in 30 & 1 in 100 (+40% allowance for climate change) storm events and 10% allowance for urban creep, during all stages of the development. If infiltration is deemed unfeasible, associated discharge rates and storage volumes shall be provided using a maximum discharge rate of 2.4 l/s.
- c) Detailed drainage design drawings and calculations to include: a finalised drainage layout detailing the location of drainage elements, pipe diameters, levels, and long and cross sections of each element including details of any flow restrictions and maintenance/risk reducing features (silt traps, inspection chambers etc.).
- d) A plan showing exceedance flows (i.e. during rainfall greater than design events or during blockage) and how property on and off site will be protected.
- e) Details of drainage management responsibilities and maintenance regimes for the drainage system.
- f) Details of how the drainage system will be protected during construction and how runoff (including any pollutants) from the development site will be managed before the drainage system is operational.

Reason:

To ensure the design meets the national Non-Statutory Technical Standards for SuDS and the final drainage design does not increase flood risk on or off site.

12. Prior to the first occupation of the development, a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme (or detail any minor variations), provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/areas, flow restriction devices and outfalls).

Reason:

To ensure the Drainage System is constructed to the National Non-Statutory Technical Standards for SuDS.

13. No properties shall be occupied until confirmation has been provided that either:
1. Capacity exists off site to serve the development, or
  2. A housing and infrastructure phasing plan has been agreed with Thames Water. Where a housing and infrastructure phasing plan is agreed, no occupation shall take place other than in accordance with the agreed housing and infrastructure phasing plan, or
  3. All wastewater network upgrades required to accommodate the additional flows from the development have been completed

Reason:

Network reinforcement works may be required to accommodate the proposed development. Any reinforcement works identified will be necessary to avoid sewage flooding and/or potential pollution incidents

14. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected, including provisions for wildlife access, has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to Development Management Plan 2019 policy DES1 and NHE3.

15. No development shall commence until a scheme for the salvaging of the sculptures on the existing church building have has been submitted to and approved in writing by the Local Planning Authority. The sculptures shall thereafter be salvaged in accordance with the approved details.

Reason:

To ensure that the non-designated heritage asset is maintained with regard to Development Management Plan 2019 policy NHE9.

16. No development above ground level shall commence until a scheme to provide positive biodiversity benefits has been submitted to and approved in writing by the local planning authority (LPA). This should be designed alongside the soft landscaping proposals for the site. The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these

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details or as otherwise agreed in writing by the LPA, and before occupation of this development.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

17. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 1995, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

Reason:

To ensure that the development does not affect the amenity of the neighbouring property by overlooking and to protect the visual amenities of the area in accordance with Development Management Plan 2019 policy DES1.

18. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A B and C of Part 1 of the Second Schedule of the 2015 Order shall be constructed.

Reason:

To control any subsequent enlargements in the interests of the visual and residential amenities of the locality with regard to Development Management Plan 2019 policy DES1.

19. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
  - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

20. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband.

Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:

- a) A broadband connection accessed directly from the nearest exchange or cabinet
- b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

21. The development hereby approved shall not be first occupied unless and until full details (and plans where appropriate) of the waste management collection points within the site (and pulling distances where applicable), and road surface, throughout the development, have been submitted to and approved in writing by the Local Planning Authority.

The waste collection points should be of an adequate size to accommodate the bins and containers required for the dwellings which they are intended to serve in accordance with the Council's guidance contained within Making Space for Waste Management in New Development.

Each dwelling shall be provided with the above facilities in accordance with the approved details prior to occupation of the relevant dwellings.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage in accordance with Development Management Plan 2019 policy DES1.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.info](http://www.firesprinklers.info).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions.
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, a 140 litre wheeled bin conforming to British Standard BSEN840 and a 60 litre recycling box should be provided for the exclusive use of the occupants of that dwelling. Prior to the initial occupation of any communal dwellings or flats, wheeled refuse bins conforming to British Standard BSEN840, separate recycling bins for paper/card and mixed cans, and storage facilities for the bins should be installed by the developer prior to the initial occupation of any dwelling hereby permitted. Further details on the required number and specification of wheeled bins and recycling boxes is

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available from the Council's Neighbourhood Services on 01737 276501 or 01737 276097, or on the Council's website at [www.reigate-banstead.gov.uk](http://www.reigate-banstead.gov.uk). Bins and boxes meeting the specification may be purchased from any appropriate source, including the Council's Neighbourhood Services Unit on 01737 276775.

4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
- (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover to install dropped kerbs. Please refer to:



[www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs)

7. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
8. The applicant is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
9. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from un-cleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders (Highways Act 1980 Sections 131, 148, 149).
10. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
11. If proposed site works affect an Ordinary Watercourse, Surrey County Council as the Lead Local Flood Authority should be contacted to obtain prior written Consent. More details are available on our website.
12. If proposed works result in infiltration of surface water to ground within a Source Protection Zone the Environment Agency will require proof of surface water treatment to achieve water quality standards.
13. There are public sewers crossing or in close proximity to the development. The Applicant is advised to see <http://developers.thameswater.co.uk/Developing-a-large-site/Planning-your-development/Working-near-or-diverting-our-pipes>
14. A Groundwater Risk Management Permit from Thames Water will be required for discharging ground water into a public sewer. Any discharge made without a permit is deemed illegal and may result in prosecution under the provisions of the Water Industry Act 1991. Thames Water would expect the developer to demonstrate what measures they will undertake to minimise groundwater discharges into the public sewer. Permit enquiries should be directed to Thames Water's Risk Management Team by telephoning 020 3577 9483 or by emailing [wwriskmanagement@thameswater.co.uk](mailto:wwriskmanagement@thameswater.co.uk) Application forms should

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be completed online via [www.thameswater.co.uk](http://www.thameswater.co.uk). Please refer to the Wholesale; Business customers; Groundwater discharges section.

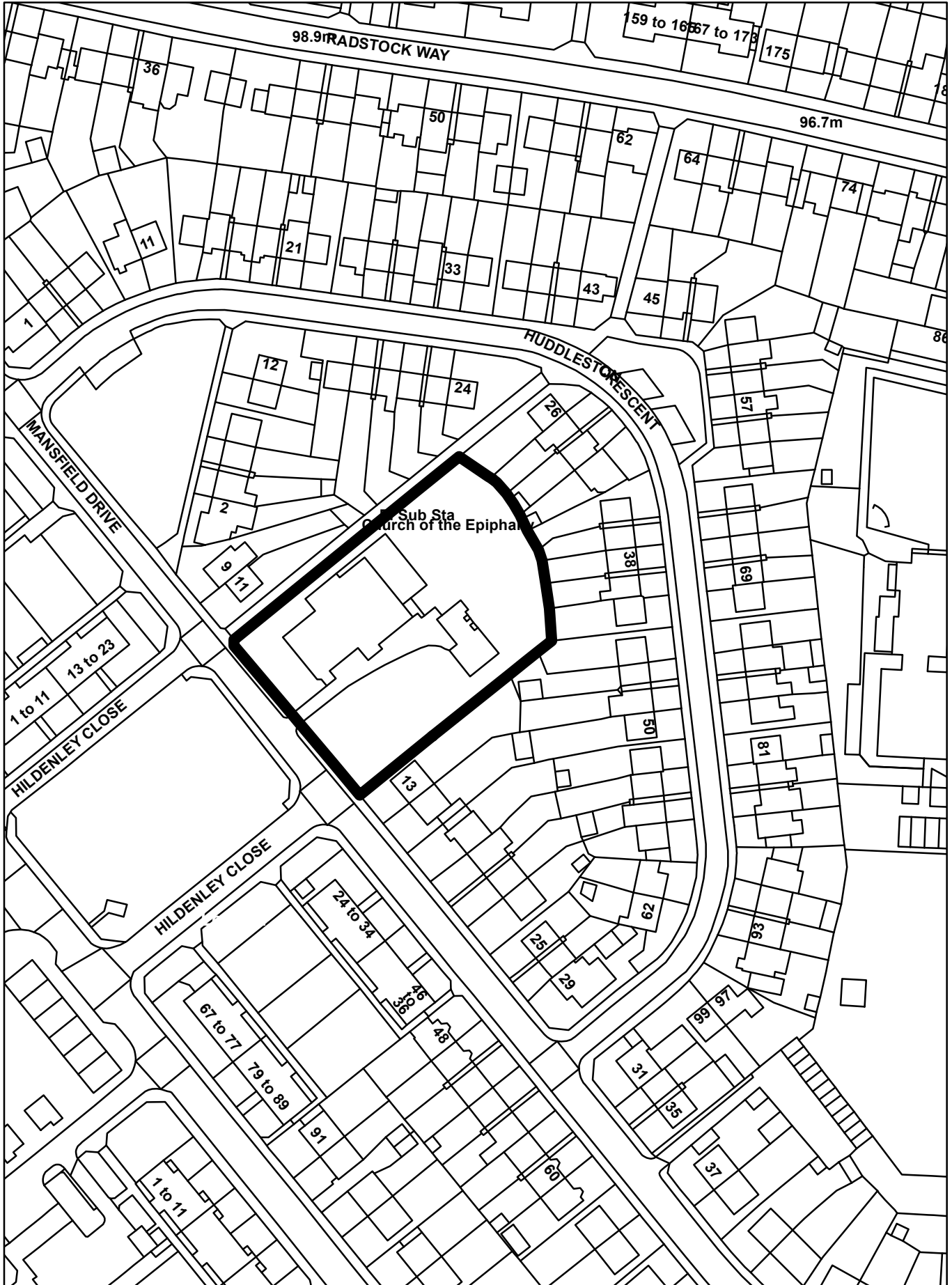
15. The development shall achieve standards contained within the Secured by Design aware scheme to be successfully granted to award.
16. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above relevant conditions. Replacement planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.

## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies DES1, DES4, DES5, DES8, TAP1, NHE3, INF3, CCF1, CCF2, RED4, INF2 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

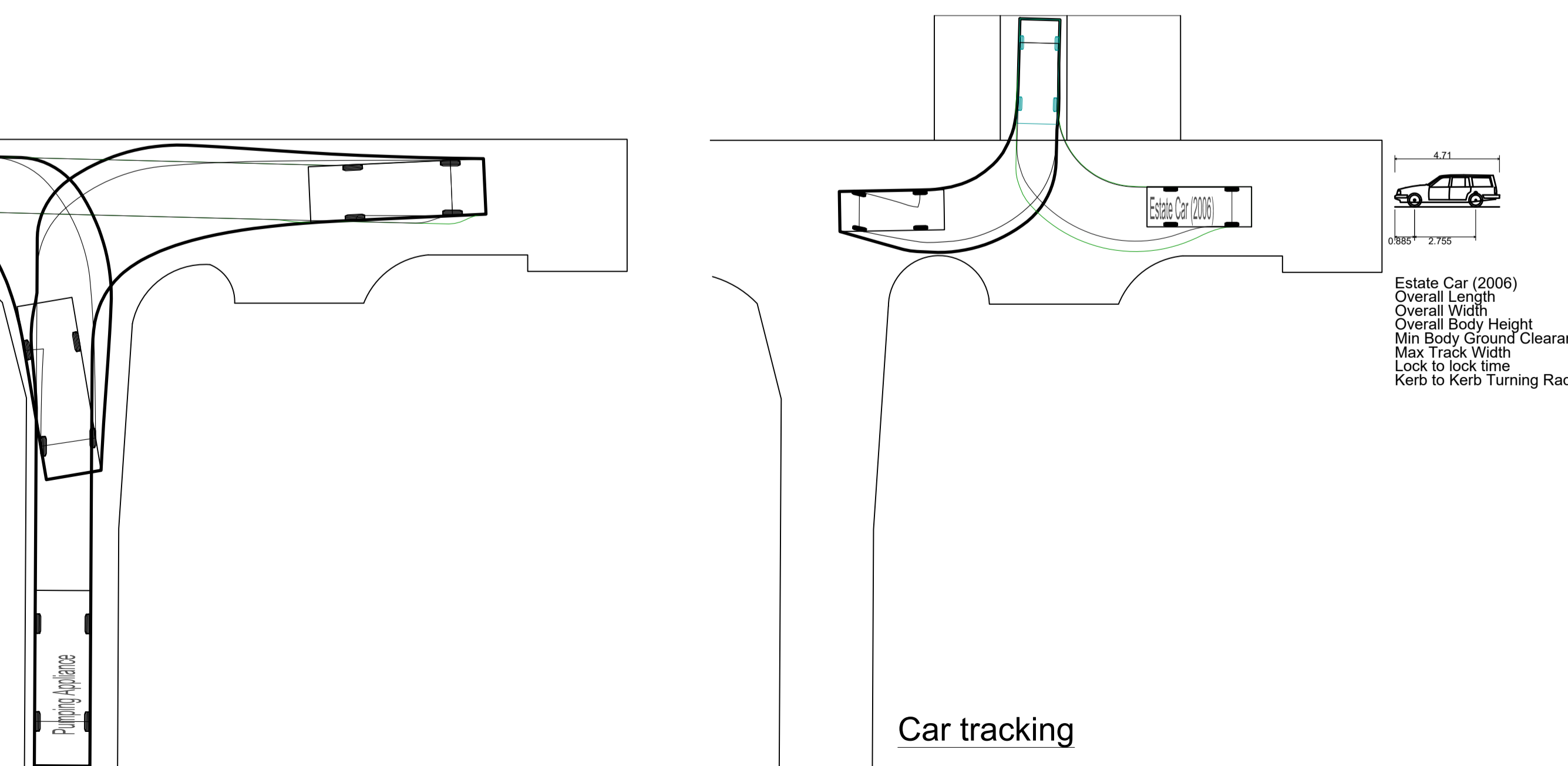
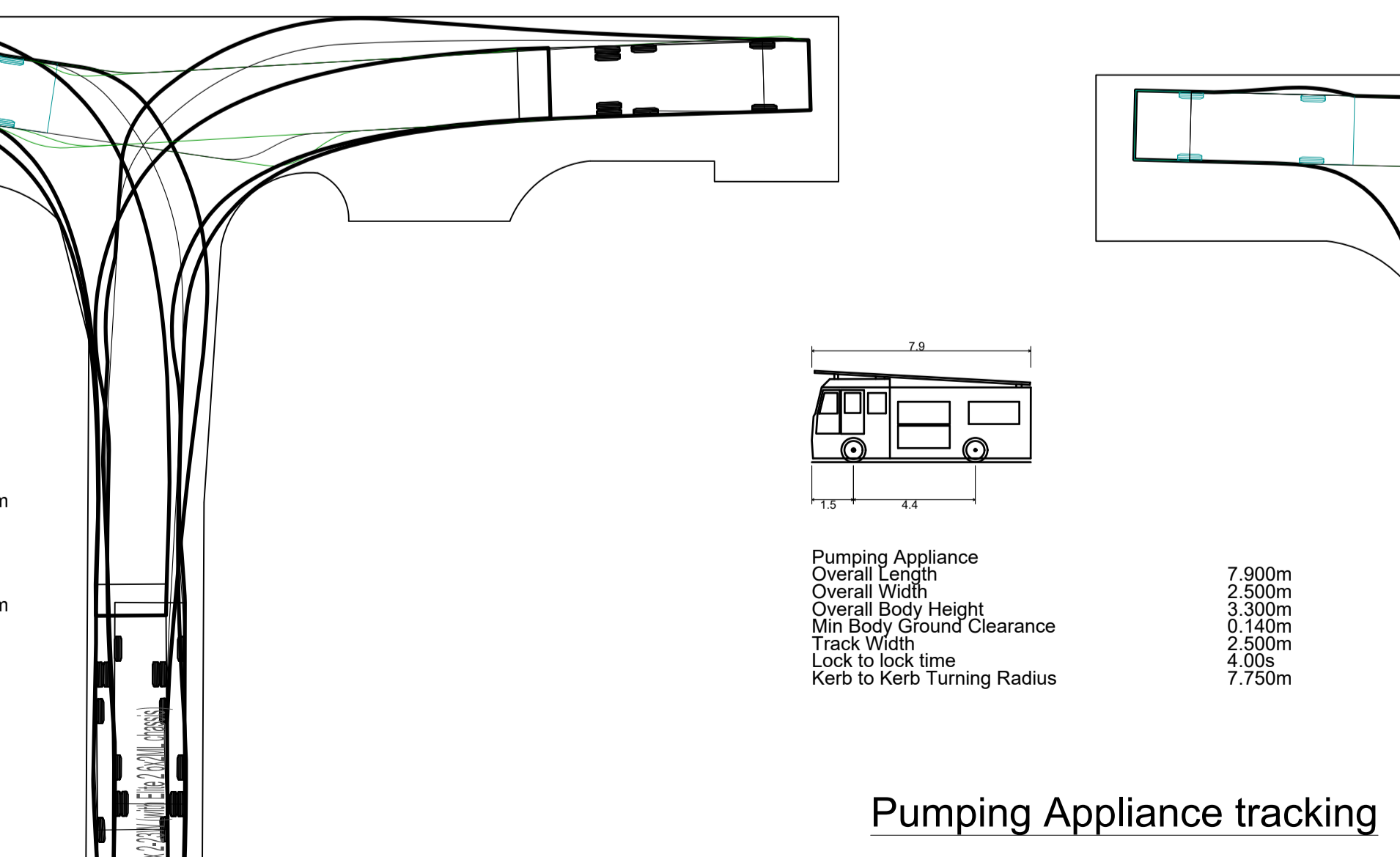
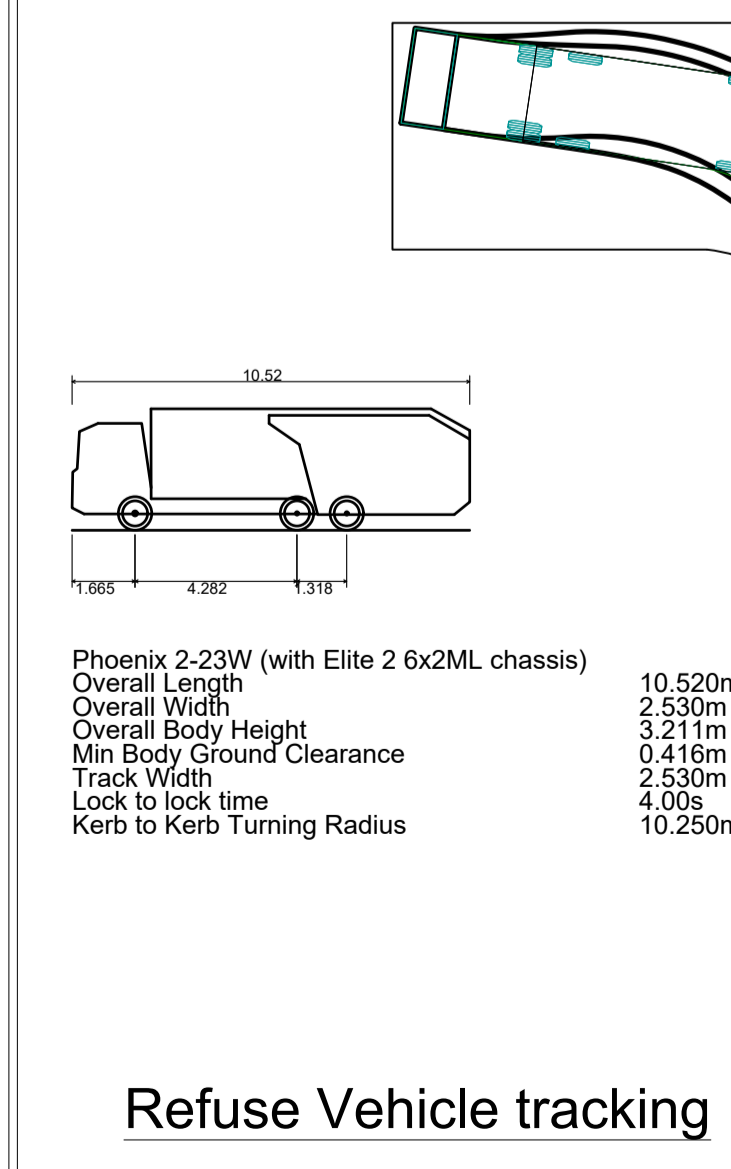
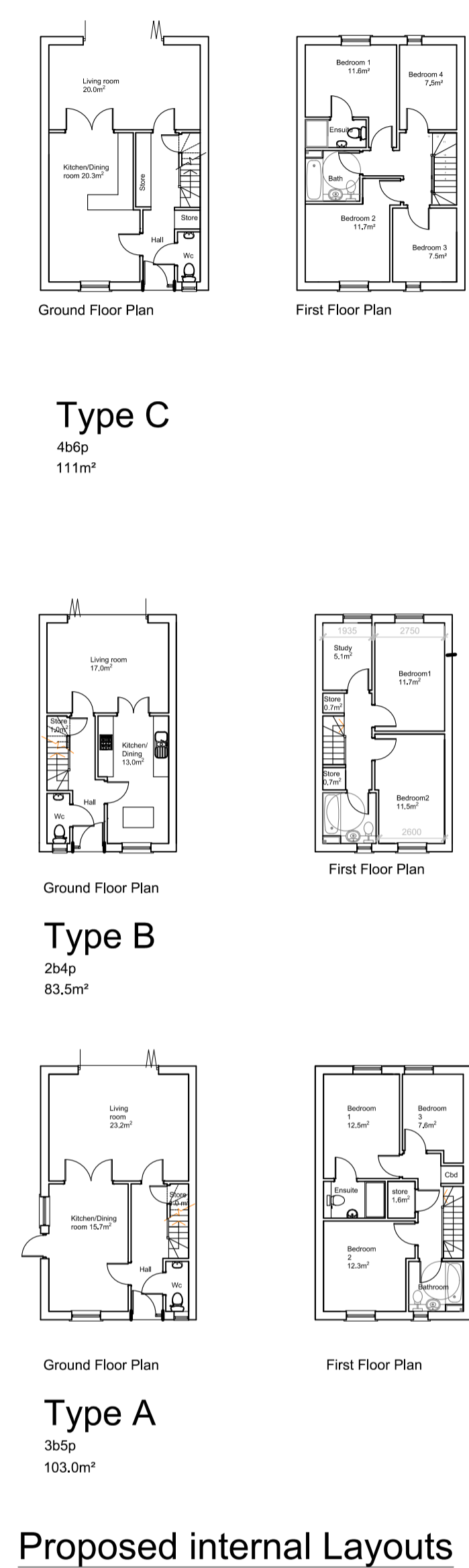
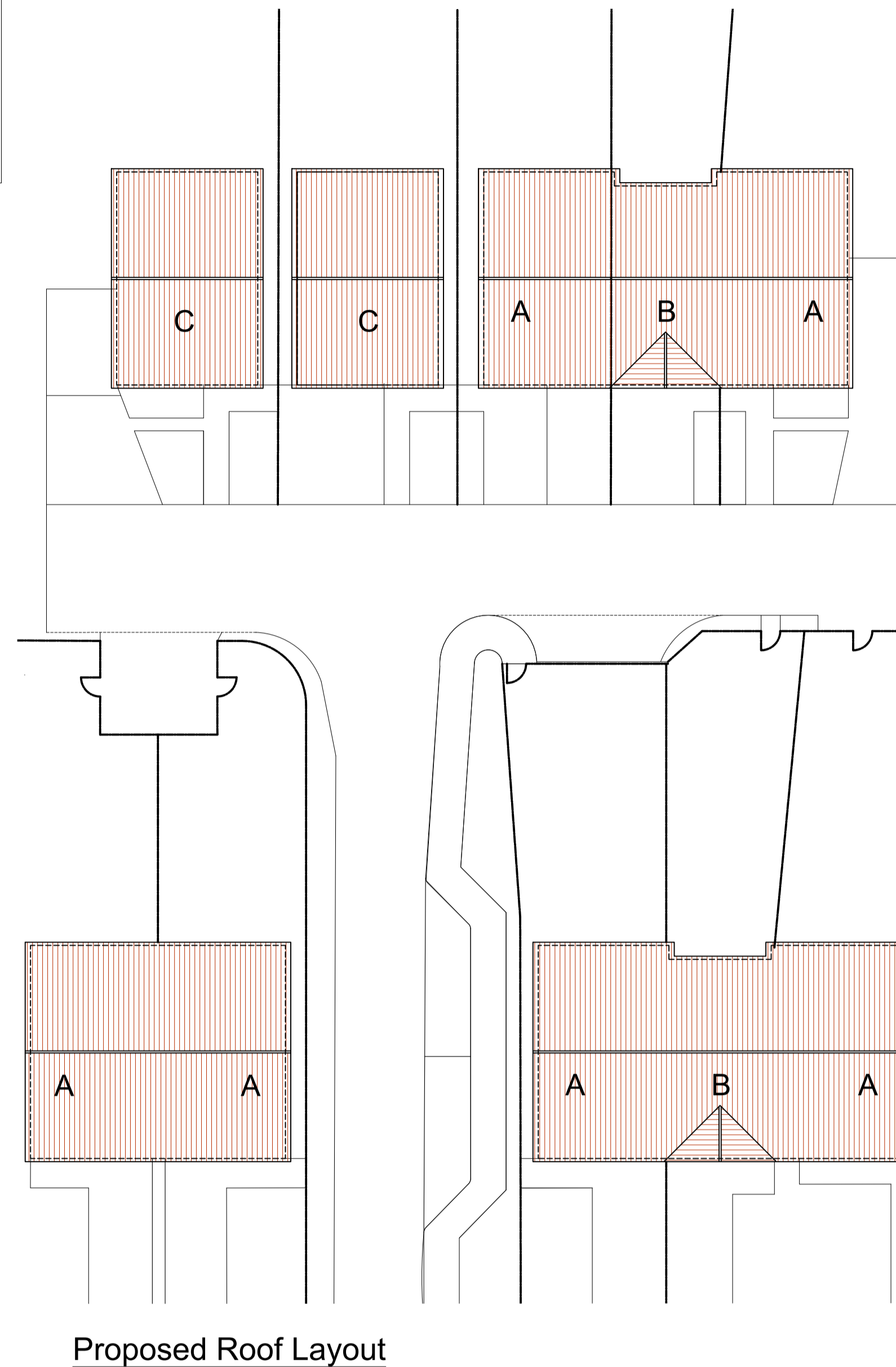
## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.





REVISIONS			DO NOT SCALE
REV	DATE	NOTES	INITIAL
E	07.01.2020	Roof layout added	MD
F	12.03.2020	Refuse Vehicle Amended	RT



**STATUS**

**Planning**

**Notes**

Joint Applicants are:  
Turnbull Land,  
The South London Church Fund and  
Southwark Diocesan Board of Finance.

**Howard Fairbairn:MHK**  
chartered architects

Howard Fairbairn Project Services Ltd  
Lambour House, Redlands  
Coulston  
Surrey  
CR5 2HT

Telephone  
020 8665 5252

Website  
hfmhk.com

Email  
croydon@hfmhk.com

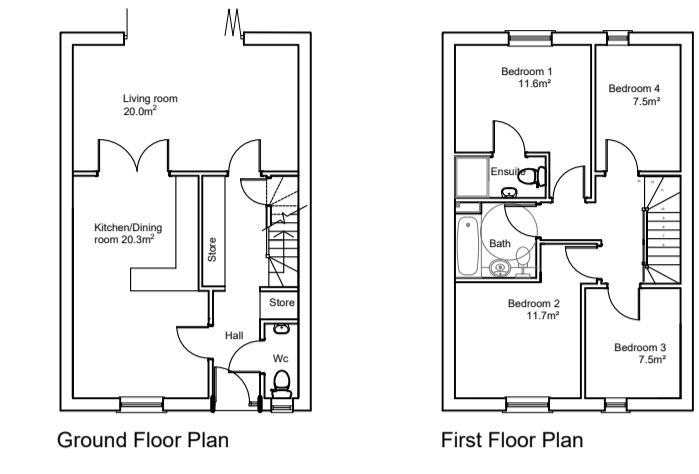
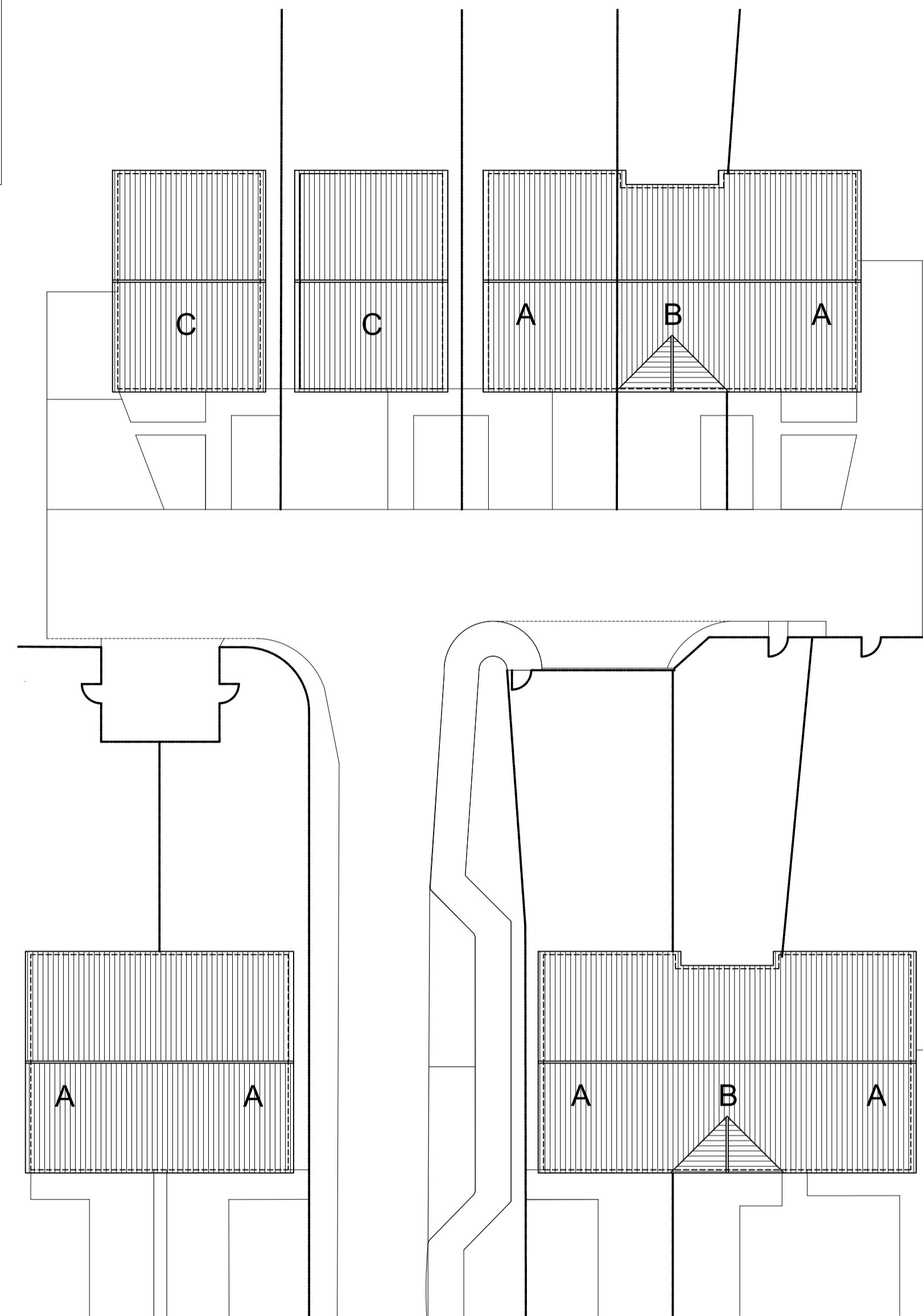
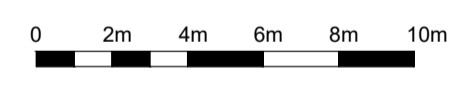
Client	Joint Applicants as above		
Project	Epiphany House Merstham RH1 3JP		
Title	Proposed Site Layout & Internal layouts		
Drawn by	MD	Checked by	
Scale	1:200 @ A1	Date	Dec 2019
CAD Ref:	6776		

Dwg N° 6776 - PA01 Rev F

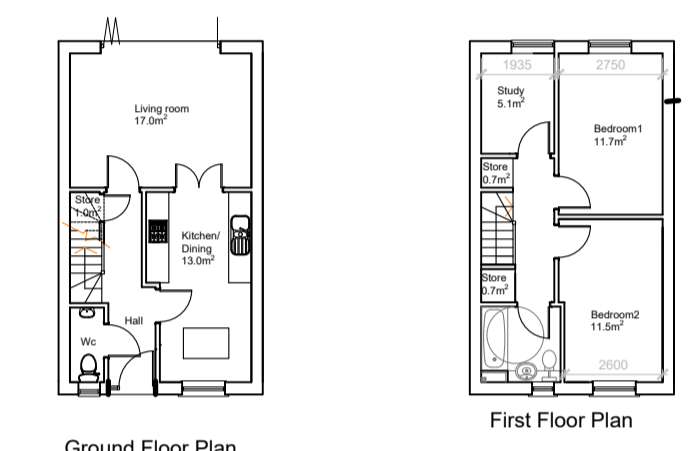




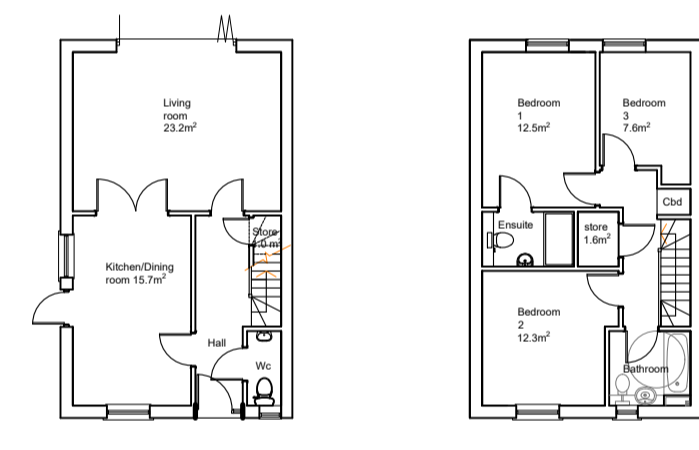
Proposed Site Layout



Type C  
4b6p  
111m<sup>2</sup>

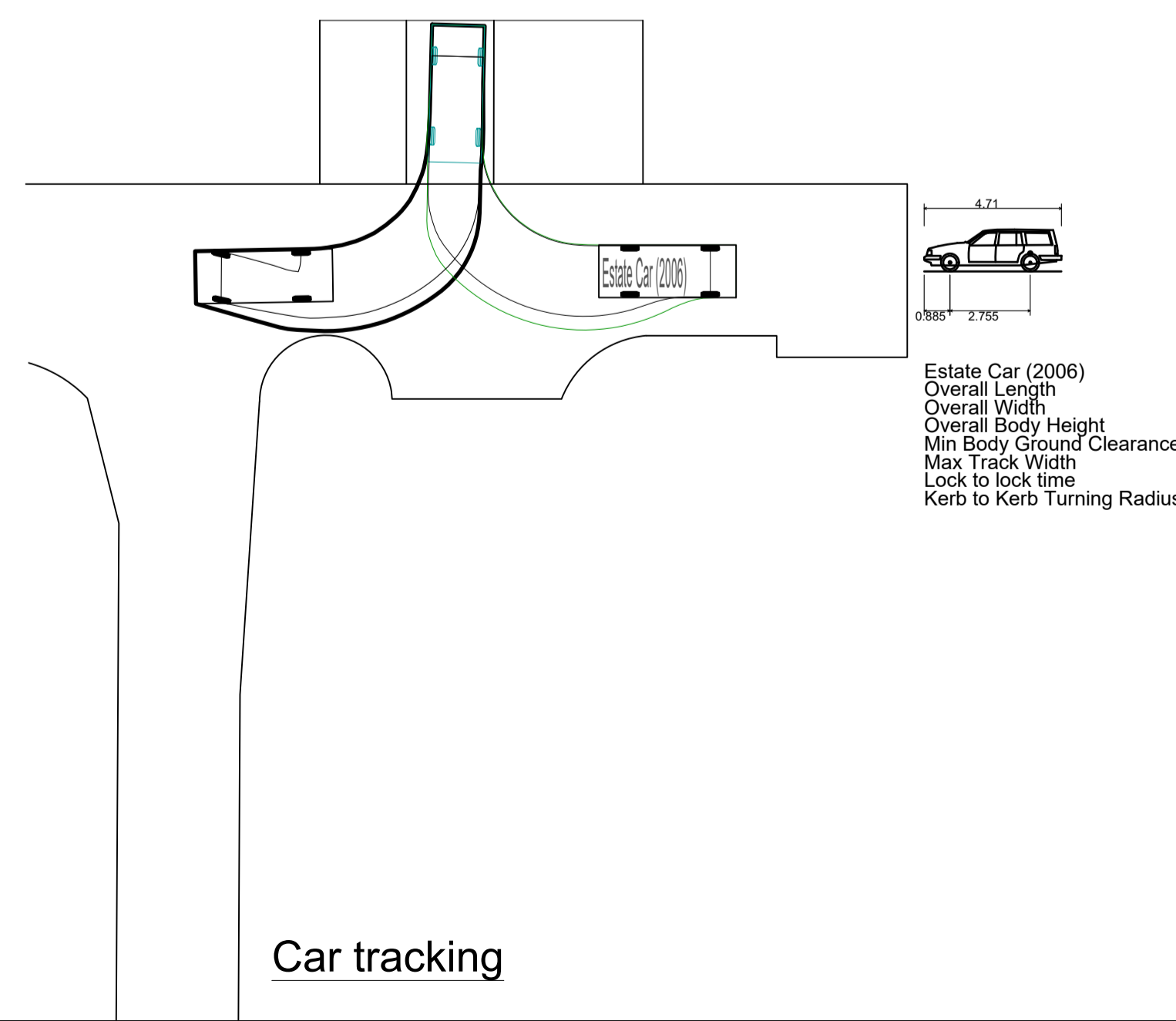
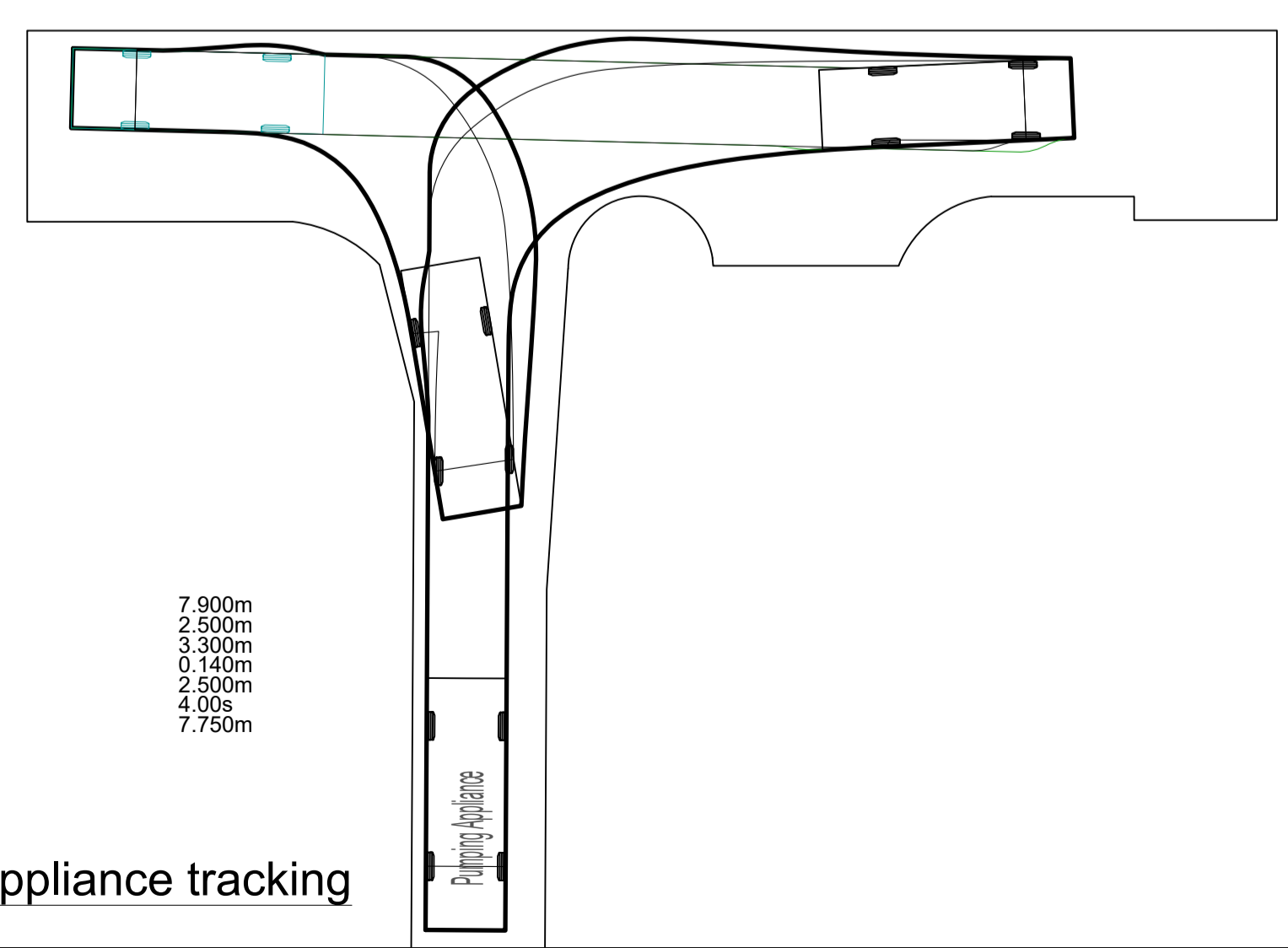
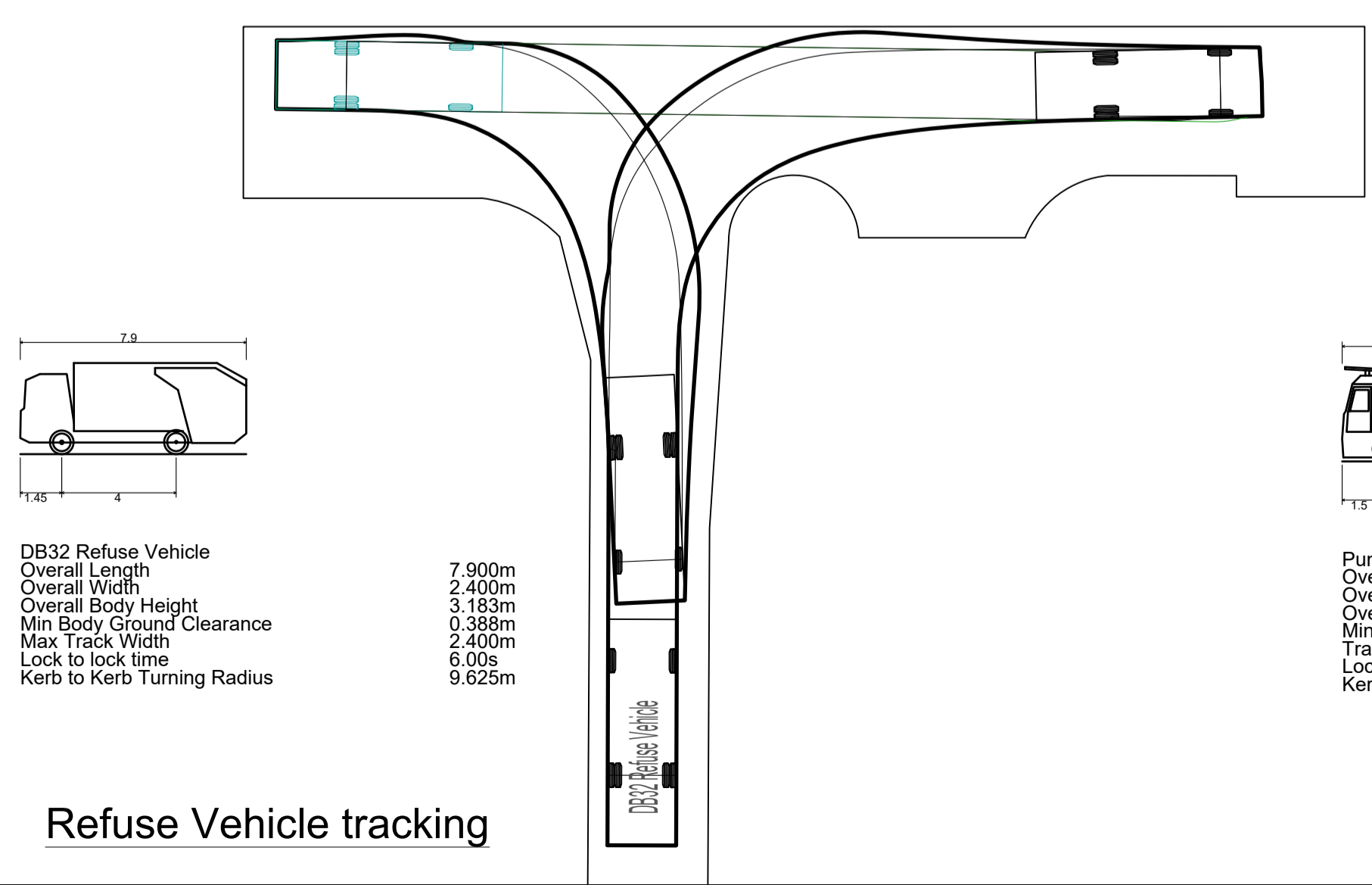


Type B  
2b4p  
83.5m<sup>2</sup>



Type A  
3b5p  
103.0m<sup>2</sup>

Proposed internal Layouts



REVISIONS			DO NOT SCALE
REV	DATE	NOTES	INITIAL
E	07.01.2020	Roof layout added	MD

STATUS

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HowardFairbairnMHK  
 chartered architects

Howard Fairbairn Project Services  
 Ltd  
 Lambourn House, Redlands  
 Coulsdon  
 Surrey  
 CR5 2HT  
 Telephone  
 020 8665 5252

Website  
 hfmk.com

Email  
 croydon@hfmk.com

Client	Joint Applicants as above		
Project	Epiphany House Merstham RH1 3JP		
Title	Proposed Site Layout & Internal layouts		
Drawn by	MD	Checked by	
Scale	1:200 @ A1	Date	Dec 2019
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Dwg N° 6776 - PA01 Rev E





1- South West Elevation (front elevation)



2- South West Elevation (rear block - front elevation)



3- North East Elevation (rear block - rear elevation)



4- North East Elevation (front block - rear elevation)

REVISIONS			DO NOT SCALE
REV	DATE	NOTES	INITIAL

STATUS  
**Planning**

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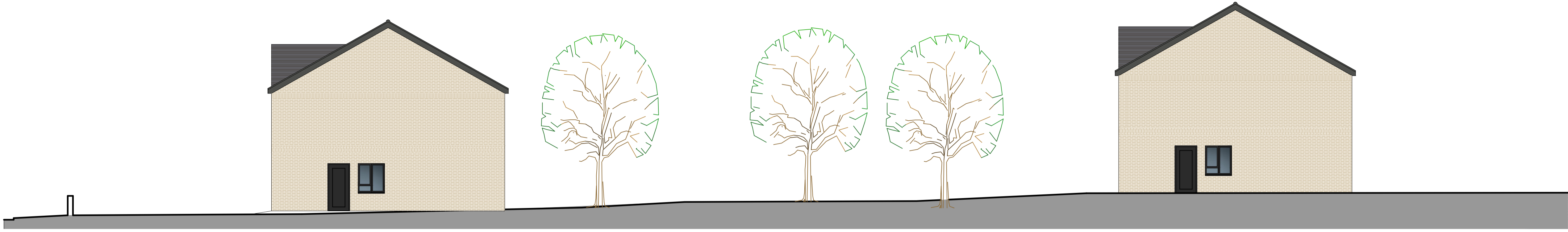
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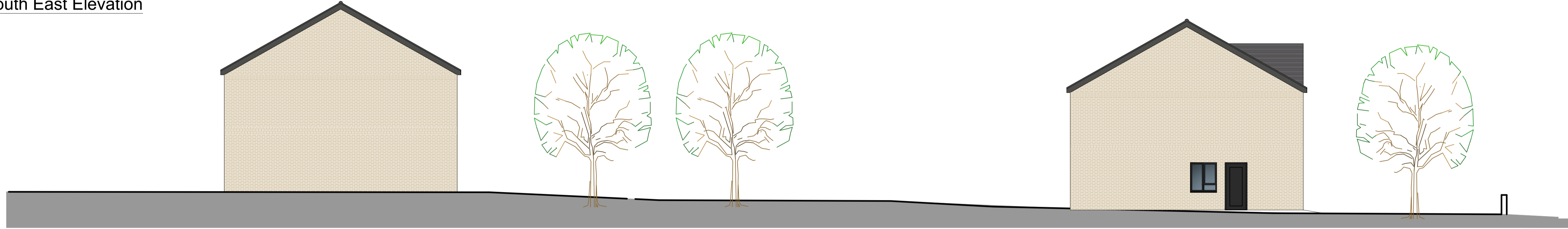
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Project	Epiphany House Merstham RH1 3JP		
Title	Proposed Elevations		
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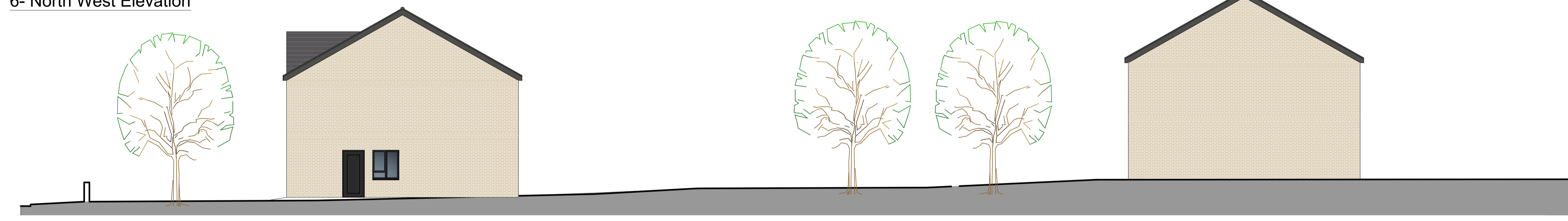




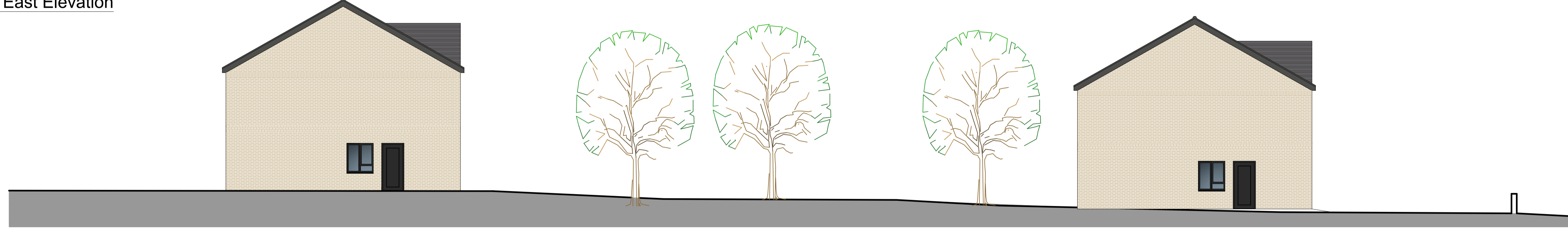
5- South East Elevation



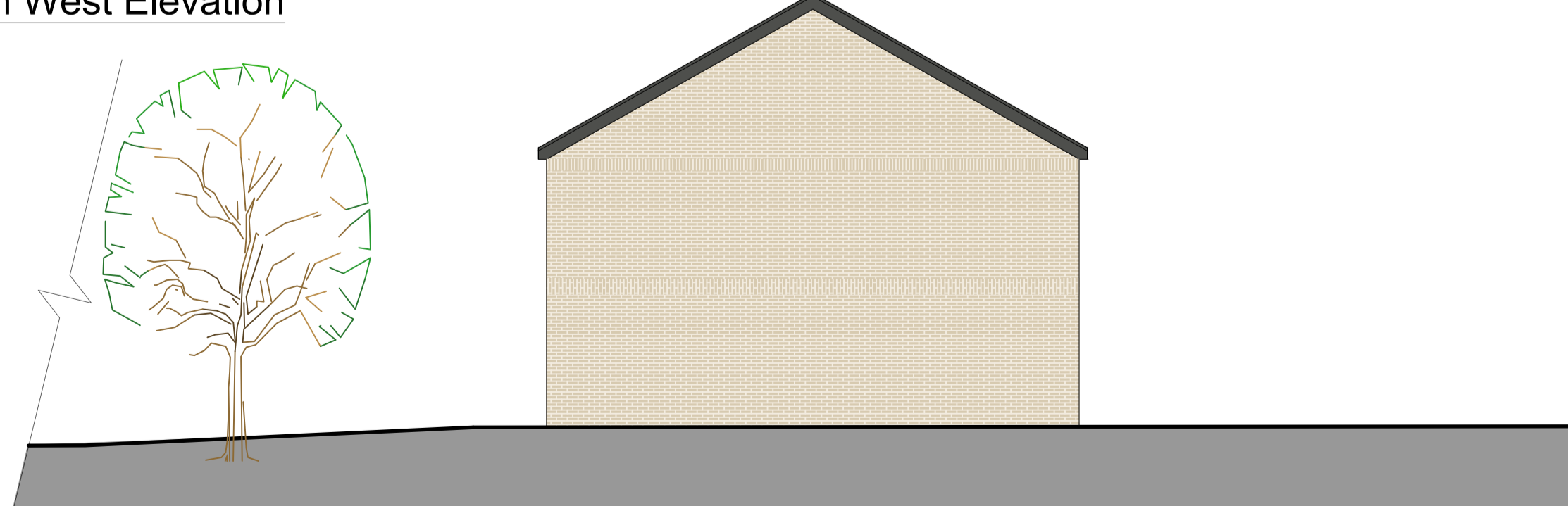
6- North West Elevation



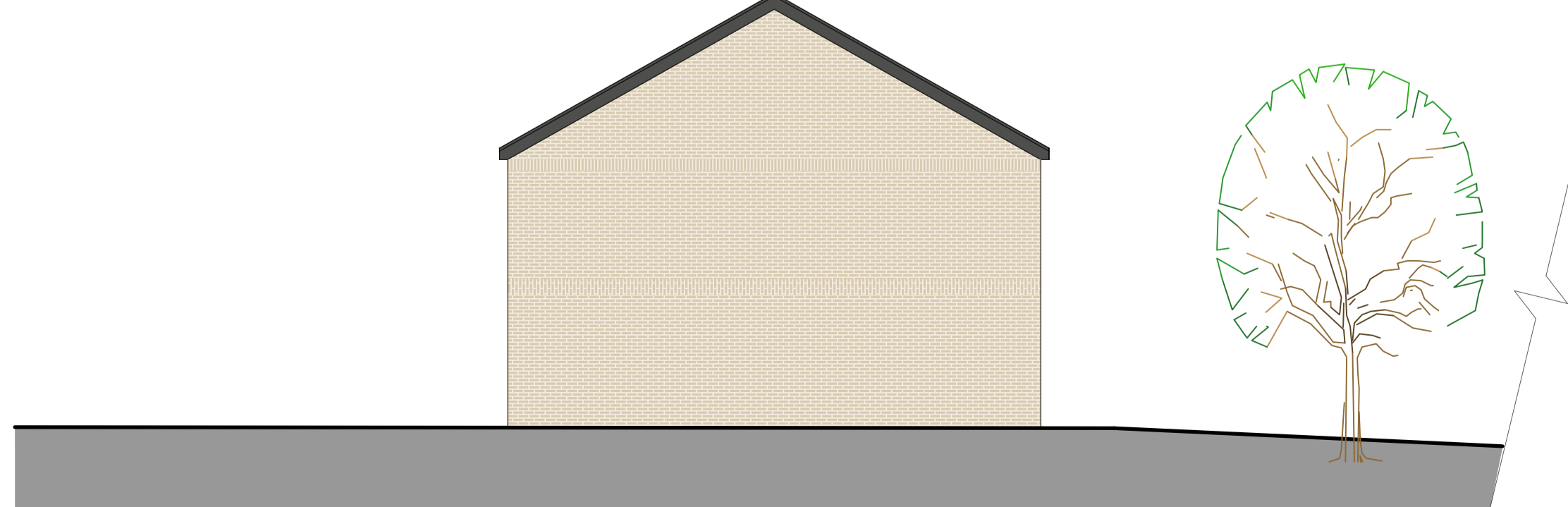
7- South East Elevation



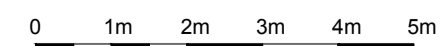
8- North West Elevation



9- South East Elevation



10- North West Elevation



REVISIONS			DO NOT SCALE
REV	DATE	NOTES	INITIAL

STATUS **Planning**

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Telephone  
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Drawn by	MD	Checked by	
Scale	1:100 @ A1	Date	Dec 2019
CAD Ref:	6776		

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Agenda Item 7


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# Agenda Item 8

Planning Committee  
14 April 2021

Agenda Item: 8  
Application 19/0986/F

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	14 April 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Michael Parker
	<b>TELEPHONE:</b>	01737 276339
	<b>EMAIL:</b>	Michael.parker@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	8	<b>WARD:</b> <i>Horley West and Sidlow</i>

<b>APPLICATION NUMBER:</b>	19/0986/F	<b>VALID:</b>	01/12/2020
<b>APPLICANT:</b>	Marden Homes Ltd.	<b>AGENT:</b>	Strutt & Parker
<b>LOCATION:</b>	<b>LAND AT THE CROFT MEATH GREEN LANE HORLEY SURREY RH6 8HZ</b>		
<b>DESCRIPTION:</b>	Erection of 10 dwellings with site access, private amenity space, garaging, parking and access to neighbouring development. As amended on 12/07/2019, 21/04/2020, 01/12/2020, 22/12/2020 and on 11/03/2021.		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for erection of 10 dwellings with associated car parking and landscaping. The existing dwelling at The Croft would be retained within a smaller plot. The proposed dwellings would comprise a mixture of two, four and five bedroom two storey houses arranged around a shared landscaped access road. Vehicular access to the site would be taken at the bend on Meath Green Lane with a separate pedestrian access indicated in the south west corner of the site.

The site is within the urban area and forms part of the allocated housing site for the wider Horley North West Sector (NWS). In this context, a residential redevelopment is acceptable in principle, subject to consideration of detailed matters.

Overall, it is considered that the proposed development would be of appropriate scale and design and would not be unduly detrimental to the character of the site and wider locality and would preserve the special character of the recently designated Meath Green Conservation Area and would provide an acceptable level of amenity for future occupants. It therefore complies with policies DES1, DES2, DES4 and DES5 and NHE9 in this respect.

The proposal would not have a significant adverse effect upon existing neighbouring properties. The proposal would provide parking in line with the DMP parking standards and would, subject to conditions, be acceptable with regard

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Planning Committee  
14 April 2021

Agenda Item: 8  
Application 19/0986/F

contamination, drainage and sustainable construction with regard to the impact on archaeology, trees and ecology.

The previously refused application 17/01385/F was refused on highway safety grounds in relation to the proposed access and the lack of existing or proposed pedestrian and cycle infrastructure for the site.

The applicant has now provided a Road Safety Audit document and incorporated the requirements in to the proposed access arrangement. The proposed development has been reviewed by the County Highway Authority (CHA), with regard to highway safety, capacity and policy matters, who raise no objections subject to the imposition of a number of conditions.

With regard to the proposed pedestrian route in the south-west corner of the site to provide a link to the rest of the North West Sector (NWS) development the context and proposal has now moved on significantly from the refused scheme. The amended scheme now includes a proposed path which links the site to the northern edge of the eastern access road, which is now under construction. The applicant has reached an agreement with the adjacent land owners which will allow them to construct this footpath over their land. The reserved matters application for the NWS land immediately adjoining the site has also been submitted to the Local Authority and is currently under consideration (ref. 04/2120/RM4B). The current site plan for this reserved matters shows the installation of a footpath all the way along the northern side of the eastern access road to the junction with Meath Green Lane to the west. Officers are therefore satisfied that there is now a reasonable likelihood that the proposed access path to link into the NWS can now be achieved and that the occupants will therefore be able to access the services within the NWS.

The proposed path does rely upon works on land not under the control of the applicant therefore it is considered reasonable and necessary to include a Grampian style pre-commencement condition which will require the path to be provided prior to commencement of this development. This will ensure that the infrastructure, which is essential to the acceptability of the application, is secured and in place well before any possible occupation of the site. The condition also requires an updated plan showing a wider path and detail of how it will connect with the reserved matters path as well as details of its future management.

## RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

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14 April 2021

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Application 19/0986/F

## Consultations:

Highway Authority: The Surrey County Council Highway Authority (CHA) has assessed the application on safety, capacity and policy grounds and has raised no objection subject to conditions.

Horley Town Council: No objections as the Council notes the realignment of the entrance to address sightline; however, the Council is concerned that the sightline may become degraded over time by overgrown soft landscaping.

Neighbourhood Services: Provided comments on their requirements for refuse collection. No objection raised to amended scheme.

Surrey County Council Archaeological Officer: no objection subject to condition to secure required archaeological work

Surrey County Council Lead Local Flood Authority: Satisfied that the proposed drainage scheme meets the national guidance and technical standards. Condition recommended to secure implementation of drainage strategy.

Surrey Police Designing Out Crime Officer: recommends a Secure by Design condition or informative.

Surrey Wildlife Trust (SWT): Advise that prior to determination further information relating to impact avoidance and mitigation measures for reptiles is provided. Information regarding the retention of bat and badger habitats should be provided. The Council should ensure that site enhancement measurements demonstrate a net gain in biodiversity [Officer note: an updated Ecological Assessment has been submitted to the Council to address SWTs comment.]

Tree Officer: No objection subject to conditions.

## Representations:

Letters were sent to neighbouring properties on 7/12/2020 and 16/3/2021 (with regard to updated highway and ecology information).

4 responses have been received raising the following comments:

<b>Issue</b>	<b>Response</b>
Flooding	See paragraph 6.48 to 6.49
Harm to wildlife habitat	See paragraph 6.37 to 6.43
Harm to Conservation Area	See paragraph 6.4-6.17
Hazard to highway safety	See paragraph 6.24 to 6.31
Inadequate parking	See paragraph 6.24 to 6.31

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Harm to Green Belt/countryside	The site is part of the North West Sector Allocation located within the urban area, outside of the designated metropolitan green belt
Inconvenience during construction	See paragraph 6.54
Increase in traffic and congestion	See paragraph 6.24 to 6.31
Loss of/harm to trees	See paragraph 6.32 to 6.36
Loss of private view	Not a material planning consideration
No need for the development	See paragraph 6.2
Noise and disturbance	See paragraph 6.18 to 6.23
Out of character with surrounding area	See paragraph 6.4-6.17
Overbearing relationship and loss of outlook	See paragraph 6.18 to 6.23
Overdevelopment	See paragraph 6.4-6.17
Overlooking and loss of privacy	See paragraph 6.18 to 6.23
Overshadowing	See paragraph 6.18 to 6.23
Increased demand to utilities and services with no infrastructure proposed	See paragraph 6.50 to 6.51
Work has already begun	Following a site visit no work has begun on this specific site. Work is underway on the North West Sector access road and a number of phases (4 and 5) to the south and east of the site.

## **1.0 Site and Character Appraisal**

- 1.1 The site comprises the detached dwelling of The Croft, its immediate gardens and an area of open paddocks, located on the bend on the southern side of Meath Green Lane. The Croft is an attractive, tile hung, mid-19th century cottage with later sympathetic Edwardian additions and remodelling sited prominently on the bend of Meath Green Lane. Whilst not listed, the building is identified in the Horley Design Guide SPD as being worthy of retention.

- 1.2 There is dense vegetation, including a number of mature and prominent trees on all boundaries of the site but particularly the south-eastern corner of the site and along the western boundary. As identified in the Horley Design Guide SPD, the trees and hedges along the western boundary of the site in particular mark historic field boundaries and the vegetation generally makes a significant positive contribution to the low weald landscape character of the area.
- 1.3 The site is located within the urban area. To the south and west the site adjoins the North West Sector Phase 4 land. Part of which is underway including the new eastern access road and parcels to the south of the access road. Reserved matters for the land to the immediately south and west is currently being considered under application 04/02120/RM4B. To the east is the property Meath Paddock which is a large detached dwelling set within a large plot. Beyond Meath Paddock is part of the phase 5 of the North West Sector which is also currently under construction.
- 1.4 Currently Meath Green Lane marks the transition between suburban development (including the North West Sector which is currently being built out) and the more semi-rural environs to the north (and open countryside beyond). Properties on northern side of Meath Green Lane are characterised by substantial detached properties set within large well landscaped plots and often with separate paddocks. However, the land immediately to the north of Meath Green Lane is now also designated urban area and is allocated for approximately 75 dwellings under allocation policy NWH1 of the Reigate and Banstead Development Management Plan 2019 (DMP).
- 1.5 Following the designation by the Planning Committee on 17 February 2021, part of the northern section of the site, including the existing dwelling, the proposed access area and part of the proposed Plot 1, are within the Meath Green Conservation Area. Whilst the consultation for this new Conservation Area is still to take place the designation carries full weight in decision making.

## **2.0 Added Value**

- 2.1 Pre-application stage: pre-application discussions were carried out in relation to a scheme for 14 dwellings. Concern was raised with regard to the quantum and scale and impact on the Meath Green Lane character. Concerns raised regarding the proposed access point and pedestrian access to the rest of the North West Sector.
- 2.2 Improvements secured during the course of the application: Officers requested and secured a reduction from 14 units to 10 units and change to the proposed mix. Pedestrian access now shown through to access road of North West Sector. Additional ecology, archaeology and tree information provided.

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- 2.3 Further improvements to be secured through conditions or legal agreement:  
The following conditions are recommended to be attached to the permission:
- Materials and design measures
  - Tree Protection
  - Landscaping
  - Ecology
  - Drainage
  - Archaeology

### 3.0 Relevant Planning and Enforcement History

- 3.1 17/01385/F Full detailed planning application: Refused  
10/11/2017  
erection of 13 no. dwellings,  
vehicular and pedestrian access,  
secure cycle storage and car  
parking, landscaping and servicing.  
As amended on 22/08/2017 and  
13/10/2017.

- 3.2 The application was refused for the following reasons:

1. It has not been satisfactorily demonstrated that adequate forward visibility of and for vehicles turning into the proposed vehicular access from Meath Green Lane is achievable. The proposed development could therefore lead to conditions prejudicial to highway safety, contrary to policy Mo5 of the Reigate and Banstead Borough Local Plan 2005, objective 3 of the Surrey Transport Plan 2011-2016 and the provisions of the National Planning Policy Framework.

2. The proposals, by virtue of the lack of existing adequate pedestrian or cycle infrastructure serving the site and the lack of certainty that appropriate provision can be achieved, would fail to ensure safe and suitable access to the development for all people and in doing so would fail to encourage travel to and from the site by sustainable modes. The proposals would therefore result in a car-reliant form of development and/or cause significant prejudice to the safety of highway users which would be contrary to policies Mo5 and Hr2B of the Reigate and Banstead Borough Local Plan 2005, policy CS17 of the Reigate and Banstead Core Strategy 2014, objective 3 of the Surrey Transport Plan 2011-2016 and the provisions of the National Planning Policy Framework.

3. The proposal fails to provide an agreed contribution to fund affordable housing provision within the Borough of Reigate & Banstead, and is therefore contrary to policy CS15 of the Reigate and Banstead Core Strategy 2014 and the Reigate and Banstead Affordable Housing SPD July 2014.

### 4.0 Proposal and Design Approach

- 4.1 This is a full application for erection of 10 dwellings with improved site access, private amenity space, garaging, parking and pedestrian connection to NW Sector A23 link road (bus route).

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- 4.2 A new access road would be created on to Meath Green Lane on the bend, the pedestrian access is shown at the south-western corner of the site and links in to the eastern access road which is currently under construction.
- 4.3 The proposal would provide 2 x 2 bed units, 6 x 4 bed units and 2 x 5 bed units. Each of the new homes would be 2 storey and have a private garden and at least two dedicated private parking spaces, including garages. Three additional visitor bays would be provided. There would be three shared landscaped areas, one to the north, one in the south-west corner and one to the south-east which would provide space for additional soft landscaping.
- 4.4 A design and access statement (D&A) should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.
- 4.5 Evidence of the applicant's design approach is set out below:

Assessment	Sections 2 to 6 of the submitted Design and Access Statement (DAS) clearly sets out the context and history of the site with a thorough analysis of the site, the surrounding area, accessibility and site constraints all being considered
Involvement	No evidence is provided that community consultation took place but as set out above pre-application discussions have taken place
Evaluation	Section 7 of the DAS shows the design development from site constraints, to the 2017 permission, to the 2018 pre-application to the initially submitted proposal under this application.
Design	Sections 8 to 16 of the DAS cover scale, appearance, accommodation, landscaping, parking, refuse, materials and access

- 4.6 Further details of the development are as follows:

Site area	0.79ha
Existing use	Residential (garden/paddock land to The Croft)
Proposed use	Residential (2 x 2bed, 6 x 4 bed and 2 x 5 bed)

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Existing parking spaces	0
Proposed parking spaces	30 (including garages and 3 visitor spaces)
Parking standard	23 (21 for dwellings and 2 visitor)
Number of affordable units	0
Net increase in dwellings	10
Proposed site density	13 dph
Density of the surrounding area	13 dph (resultant plot of The Croft) 2 dph (if you consider the three dwellings located immediately to the east and north of the site) 30 dph (approximate density of phase 5 of North West Sector located to the north east of Meath Paddock)

## 5.0 Policy Context

### 5.1 Designation

Urban area

Partly within Meath green Lane Conservation Area

Part of Allocated Housing Site - Land at Meath Green Lane, Horley (NW Sector - Policy Hr14 of the Reigate & Banstead Borough Local Plan 2005)

Flood Zone 1

Area of High Archaeological Potential - RB105 (Late Iron Age Settlement Site)

CIL Zone 4

Parking standard scores – 3 (low)

### 5.2 Reigate and Banstead Core Strategy

CS1 (Sustainable Development)

CS2 (Valued Landscapes and Natural Environment)

CS4 (Valued townscapes and the historic environment)

CS5 (Valued People/Economic Development),

CS10 (Sustainable Development),

CS11 (Sustainable Construction),

CS12 (Infrastructure Delivery),

CS13 (Housing Delivery),

CS14 (Housing Needs)

CS17 (Travel Options and accessibility)

### 5.3 Reigate and Banstead Development Management Plan 2019

DES1 (Design of New development)

DES2 (Residential garden land development)



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DES4 (Housing Mix)  
DES5 (Delivering High Quality Homes)  
DES7 (Specialist Accommodation)  
DES8 (Construction Management)  
DES9 (Pollution and contaminated land)  
TAP1 (Access, Parking and Servicing)  
CCF1 (Climate Change Mitigation)  
CCF2 (Flood Risk)  
NHE2 (Protecting and enhancing biodiversity)  
NHE3 (Protecting trees, woodland areas and natural habitats)  
NHE9 (Heritage Assets)  
INF1 (Infrastructure)  
INF2 (Community facilities)  
INF3 (Electronic communication networks)

## 5.4 Other Material Considerations

National Planning Policy Framework

National Planning Practice Guidance

Supplementary Planning Guidance

Surrey Design

Local Distinctiveness Design Guide

Vehicle and Cycle Parking

Guidance 2018

Householder Extension & alterations

Horley Design Guide 2006

Other

Human Rights Act 1990

Community Infrastructure

Regulations 2010

Conservation of Habitats and

Species Regulations 2010

## 6.0 **Assessment**

6.1 This is a full application for erection of 10 dwellings with associated car parking and landscaping. The existing dwelling at The Croft would be retained within a smaller plot. The proposed dwellings would comprise a mixture of two, four and five bedroom houses arranged around a shared landscaped access road. Vehicular access to the site would be taken at the bend on Meath Green Lane with a separate pedestrian access indicated in the south west corner of the site.

6.2 The site is within the urban area and forms part of the allocated housing site for the wider Horley North West Sector (NWS). In this context, a residential redevelopment is acceptable in principle, subject to consideration of detailed matters.

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6.3 The main issues to consider are:

- Design appraisal and impact on heritage asset
- Neighbour amenity
- Highway matters
- Impact on trees
- Impact on ecology
- Impact on archaeology
- Sustainable construction
- Drainage matters
- Community Infrastructure Levy
- Other matters

## Design appraisal and impact on heritage asset

- 6.4 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.5 DMP Policy DES2 which relates to development of residential garden land seeks to ensure that backland developments are of high-quality. The policy requires, amongst other things, that garden land developments should be designed to respect the scale, form and external materials of existing buildings in the locality and a height, mass and bulk to be in keeping with the existing street scene. DES2 requires that developments do not create an undue disruption in the street scene and that developments should ensure that a good standard of amenity is retained for existing and future occupants. The policy also encourages the retention of mature trees and hedges.
- 6.6 The application site is sited on the very edge of the urban area along Meath Green Lane. At this point, Meath Green Lane currently retains the character of a rural/semi-rural lane with buildings typically set within spacious plots and with the road frontage defined typically by hedgerow and vegetation. The exception is the existing dwelling of The Croft itself which has a more prominent and exposed frontage. As set out above part of the northern section of the site is now within the Meath Green Conservation Area. The character assessment for the Conservation Area designation states: "Handmade tile roofs, tile hanging and handmade brick, sometimes with burnt headers are characteristic of the area as traditional windows including cast iron frames. The lane itself has a picturesque hedge lined character of a meandering nature with grass verges. The Lane has been protected from development in the north west sector by generally being set back at 15

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metres and 20 metres or more where possible to retain the character of the lane as a country lane green corridor with hedgerows and tree lines strengthened.”

- 6.7 This application follows a similar approach to the refused 2017 scheme which was found to be acceptable in relation to design, scale and impact on the character of the area. It is also of note that the land adjacent to the site to north is now allocated for housing under the Council's Development Management Plan 2019.
- 6.8 The proposed development would have a relatively organic layout with the dwellings arranged informally around the access road which curves through the site. Whilst plots would be smaller than the very large plots which typify the existing dwellings on the opposite side of Meath Green Lane and Meath Paddock to the north-east, they would be generally larger than those on the adjoining North West Sector, allowing for the proposed dwellings to be well spaced and for the provision of front gardens. Larger areas of communal landscaping have also been incorporated into the development, including the retention of a 15 metre buffer zone to Meath Green Lane itself, where no housing is proposed but a landscaped area is proposed. It is considered that development would have a spacious character which would respect the rural nature of Meath Green Lane and the transition to the more open grain of development and countryside on the opposite side of Meath Green Lane.
- 6.9 Furthermore, the proposed layout would maintain the landscape dominated character of Meath Green Lane, with the majority of the plots having generous set back from the road allowing for meaningful soft landscaping to the front of the dwellings. As a result, these buildings would not appear as unduly prominent or conspicuous, particularly as they would be viewed from behind the landscaping within the 15 metre buffer. The layout would also ensure that the proposed dwellings would appear appropriately subservient to The Croft and as such, this attractive, historic building which would remain the prominent built feature along Meath Green Lane.
- 6.10 The variety in terms of unit types, building footprints and roof forms would help to reinforce the informality and "village" nature of the development and helps to avoid a regimented or uncharacteristically suburban feel. The appearance, materials and detailing would reflect local vernacular, advice in the Horley Design Guide SPD and elements of the retained building at The Croft, variously incorporating gabled projections, steep roof pitch, catslide and bay window features, plain roof tiles, brick and tile hanging, timber window and meaningful chimney stacks. Therefore, the design and appearance is considered to be acceptable.
- 6.11 Given the location of the site partly within the Meath Green Conservation Area the Council's Conservation Officer has considered the amended scheme for 10 dwellings. Given the changes to ensure an adequate buffer to Meath Green Lane, the submission of the further tree and landscaping details and the removal of render for materials the Conservation Officer has advised

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that he has no objection from a Conservation Area and Local Distinctiveness view point.

- 6.12 The Conservation Officer has recommended a number of conditions in relation to materials, hard landscaping, rooflights, fenestration, gutters, bargeboards, fencing and soft landscaping as well as removal of permitted development. It is considered that final details can be agreed via suitably worded conditions. Due to the location of the site partly within the Conservation Area, but with the majority of the dwellings within its setting it is considered reasonable and necessary to restrict permitted development in relation to extensions under Class A and B.
- 6.13 Overall, and subject to the conditions set out above, it is considered that the layout of the site and the design, scale and massing of the proposed dwellings would reflect the spacious, semi-rural character of Meath Green Lane and the Wealden vernacular that typifies Horley and would result in no material impact to the Meath Green Conservation Area and would comply with the provisions of DMP Policy DES1 and DES2 and NHE9.
- 6.14 DMP Policy DES4 relates to Housing Mix and states that all new residential developments should provide homes of an appropriate type, size and tenure to meet the needs of the local community. The proposed housing mix requires, on sites of up to 20 homes, at least 20% of market housing to be provided as smaller (one and two bedroom) homes. In this case, the proposal provides 20% smaller units and therefore complies.
- 6.15 DMP Policy DES5 relates to the delivery of high quality homes and requires, *inter alia*, that as a minimum, all new residential development (including conversions) must meet the relevant nationally described space standard for each individual units. In addition, the policy also requires all new development to be arranged to ensure primary habitable rooms have an acceptable outlook and where possible receive direct sunlight.
- 6.16 Each dwelling would have a floor area which is in excess of the relevant standard in the Nationally Described Space Standards with adequate built in storage. The proposed habitable rooms would all have outlook to the front or rear of their plots with the majority of the units benefitting from dual aspect living/dining/kitchen areas. As such the units would provide good levels of outlook and sunlight and daylight to the main habitable rooms. The dwellings would all benefit from sizeable rear gardens. The dwellings would be well separated from each other to ensure no impact on light and overbearing impact. The layout would also ensure that there is no unacceptable impact from the currently proposed dwellings to the south and west of the site as part of the reserved matters application 04/02120/RM4B due to the adequate separations distances, orientation of the dwellings and intervening boundary landscaping.
- 6.17 Overall, it is considered that the proposed development would be of appropriate scale and design and would not be unduly detrimental to the character of the site and wider locality and would preserve the special

character of the Meath Green Conservation Area and would provide an acceptable level of amenity for future occupants. It therefore complies with policies DES1, DES2, DES4 and DES5 and NHE9 in this respect.

### Neighbour amenity

- 6.18 The nearest neighbour is Meath Paddock which is adjacent to the site to the north-east. This property is itself set within a very large plot with significant landscaping and tree cover along the shared boundary.
- 6.19 The proposal would introduce 4 dwellings (Plots 7 to 10) along this boundary with Meath Paddock. However, Plots 7 to 10 would be oriented so that their rear or side boundaries adjoin the shared boundary and, as such, there would be a separation distance of approximately 30m from the proposed dwellings to the existing house on Meath Paddock and the closest dwelling would be a minimum of 13m away from the north-eastern boundary. At this distance, and given the intervening tree cover and landscaping, it is not considered that there would be any significant harm to the amenity of this neighbour in terms of overlooking, overshadowing or loss of outlook and noise and disturbance.
- 6.20 Whilst the existing dwelling at The Croft forms part of the application, consideration has been given to the resultant relationship that occupants of this property would experience. To accommodate the development, the curtilage of The Croft would be markedly reduced; however, it would still retain an adequate plot for its size. Two plots would adjoin the garden of The Croft; however, there would be separation of c.21m between rear of plot 1 and the main building at The Croft and c.8m between the proposed dwelling on Plot 1 and the new boundary of The Croft. Given these distances, and taking account of the respective orientation of the existing and proposed dwellings, it is not considered that occupants of The Croft would experience a harmful loss of amenity.
- 6.21 The development on this site would adjoin the on-going residential development of the North West Sector (NWS). Reserved matters for the phase of the NWS which adjoins this site have not yet been approved but the reserved matters have been submitted under ref. 04/02120/RM4B. It is not considered that the layout of this scheme would prejudice the achievement of acceptable levels of amenity for potential dwellings on the North West Sector. The dwellings proposed along the shared boundaries with this development would have gardens of a minimum of 14m deep along the western boundary and other than plot 7 the dwellings along the southern boundaries would have depths of over 10 metres. Plot 7 would have a separation distance of 7.4 metres at its closest point, however the reserved matters currently shows that the nearest units would have a rear to side relationship. It is therefore considered that the proposal would maintain adequate separation to any future dwellings on the NW Sector and would not result in an unacceptable impact in terms of overbearing impact, loss of light, loss of privacy and noise and disturbance.

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- 6.22 Neighbours on the opposite side of Meath Green Lane would be sufficiently distant from the development so as to not experience any material adverse change in amenity.
- 6.23 In conclusion, the proposal would not have an adverse impact upon existing neighbouring properties and would accord with the provisions of DMP Policy DES1.

## Highway matters

- 6.24 The previously refused scheme was refused on highway safety grounds in relation to the proposed access and the lack of existing or proposed pedestrian and cycle infrastructure for the site.
- 6.25 The applicant has now provided a Road Safety Audit document and incorporated the requirements in to the proposed access arrangement. The proposed development has been reviewed by the County Highway Authority (CHA), with regard to highway safety, capacity and policy matters, who raise no objections subject to the imposition of a number of conditions. The CHA notes that "The access into the site would include adequate sight lines and geometry to accommodate a refuse vehicle. The site would be able to accommodate the turning movements of a refuse vehicle." Conditions are recommended to secure the implementation of the access works and a Construction Transport Management Plan.
- 6.26 With regard to larger vehicles being able to manoeuvre within the site as above the CHA has raised no objection. Drawing DR2 by Journey Transport Planning shows an access road in excess of 5m wide and a swept path analysis which demonstrates that a full size refuse freighter of the length and width set out within the Council's Making Space for Waste Management document can enter and turn comfortably and safely within the site using that hammerhead and exit in forward gear. Within the DAS the applicant has set out bin storage and presentation locations which would allow minimal drag distances for residents and the refuse crews. However due to the change in the proposed layout the plan within the DAS is now out of date. A condition is therefore recommended to secure further details of the bin storage and presentation locations prior to occupation.
- 6.27 The site is located in an area which is assessed as having a low accessibility rating. In such areas, the Council's adopted parking standards require the provision of 2 spaces for each of the 2 and 4 bedroom dwellings and 2 .5 spaces each per 4+ bedroom dwelling, plus 2 visitor parking spaces. Thus, a total of 23 spaces would be required. In this case, a total of 30 spaces are proposed including the 8 garages. The garages have been designed to meet the size requirements in the DMP, so that they can be considered as part of the parking provision. It is therefore considered that the parking provision would be in excess of the minimum requirements and is acceptable. A condition is recommended to secure the retention of the garages on plots 3, 7 and 8 for parking only.

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- 6.28 Electric vehicle charging points could be secured by planning condition. The plots would be able to provide secure cycle storage for two bikes at each property which is in line with the requirements set out in the DMP. Again, this could be secured by condition.
- 6.29 With regard to the proposed pedestrian link to the rest of the North West Sector (NWS) development the context and proposal has now moved on significantly from the refused scheme. The amended scheme now includes a proposed path which links the site to the northern edge of the eastern access road, which is now under construction. The applicant has advised that they have reached an agreement with the adjacent land owners which will allow them to construct this footpath over their land. The reserved matters application for the NWS land immediately adjoining the site has also been submitted to the Local Authority and is currently under consideration (ref. 04/2120/RM4B). The current site plan for this reserved matters allows space for this proposed section of path and also shows the installation of a footpath all the way along the northern side of the eastern access road to the junction with Meath Green Lane to the west. Officers can therefore be satisfied that there is now a reasonable likelihood that the proposed access path to link in to the NWS can now be achieved and that the occupants will therefore be able to access the services within the NWS.
- 6.30 It is noted that Surrey County Council has asked for an updated plan to show pedestrian access all the way to the Webber Street junction with Meath Green on the northern side of the eastern access road. Given the progress of the reserved matters application under RM4B this is not considered necessary or reasonable. However, because the proposed path does rely upon works on land not under the control of the applicant it is considered reasonable and necessary to include a Grampian style pre-commencement condition which will require the path to be provided prior to commencement of this development. This will ensure that the infrastructure, which is essential to the acceptability of the application, is secured and in place well before any possible occupation of the site. The condition also requires an updated plan showing a wider path and detail of how it will connect with the reserved matters path.
- 6.31 Accordingly, it is considered that the proposals are acceptable from a highway point of view and accord with the provisions of DMP Policy DES1 and TAP1.

## Impact on Trees

- 6.32 Whilst the site does not have any Tree Preservations Orders the site includes a number of trees, particularly along the eastern and western boundaries. All trees along the eastern, southern and western boundaries will be retained and protected during construction. The majority of the trees within the site would be removed however with the exception of two B grade trees the other trees to be removed are low quality of either C or U grade.

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- 6.33 The Council's Tree Officer has not raised any concerns regarding the loss of the trees within the site. However initial concerns were raised regarding the post development pressure of the 14 dwellings scheme on the retained boundary trees due to shading and close proximity to the proposed dwellings.
- 6.34 Following the reduction to 10 dwellings an updated Arboricultural Report. The Tree Officer has made the following comments with regard to the revised arboricultural report:  
"The revised arboricultural report dated 26th November 2020 identifies the relevant tree protection measures that need to be installed and maintained during the course of the development.

The concerns regarding the spatial relationship between plots 7, 8 and trees 1-9 is addressed as the report identifies the extent of the pruning that will be necessary to increase the distance between the buildings and the nearest part of the crown, which is minimal and will not have an adverse impact on their appearance. The report contains a draft arboricultural method statement and therefore it will be necessary for a finalised tree protection plan condition to be attached to the decision notice.

Therefore, based on the information in the arboricultural report I support the application."

- 6.35 The Tree Officer has recommended three conditions, one requiring the finalised Tree Protection Plan and Arboricultural Method Statement, one requiring an agreed scheme of supervision and one in relation to the protection of retained trees and hedges within and adjacent to the site, during and post construction. It is considered that the final condition could be included within a landscaping condition.
- 6.36 Based on the above advice and subject to the recommended conditions it is considered that the proposal would be acceptable in relation to the impact on the existing trees.

## Impact on Ecology

- 6.37 A preliminary ecological appraisal (PEA) was submitted with the application dated 2019, which follows the ecology work undertaken in 2017 for the previously refused application. The report concludes that there would be no direct or indirect impact on any type of statutory or non-statutory conservation designation or impact upon a Priority Habitat Deciduous Woodland and Ancient Semi Natural Woodland to the east of the site and a Priority Habitat Traditional Orchard to the north-west.
- 6.38 In terms of protected species, the PEA found that:
- The reptile potential of the site since the 2017 survey effort had decrease - a repeated presence or absence survey is not required.
  - There is no badger sett of any type within the proposed development site. Badger access/foraging is still present along the southern boundary. Any



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- approved development must ensure badger access is restricted from the building zone.
- The sites boundary hedgerow/mature tree habitats will continue to provide foraging and dispersal routes for local bat populations. Any proposed mature tree removal as part of the potential development must be first assessed in relation to Potential Bat Roost Features – and if required a subsequent presence or absence survey.
  - The central acacia tree continues to have numerous bat roost features. A roost presence or absence survey in 2017 determined (at that time) a roost absence. For any development approval a repeated presence or absence survey would be required as near to the start date as possible to make it relevant. It is important that no illumination is directed towards or impacts upon the hedge/tree boundary habitats. Bat friendly illumination guidelines have been included in this report.
- 6.39 Surrey Wildlife Trust (SWT) has assessed the submitted PEA and advised that:
- the application does not clearly demonstrate that measurable net gains will be secured as a result of the proposed development; and
  - In terms of protected species further information is required in relation to the protection and retention of reptiles, bats and enhancing foraging and commuting routes and badger foraging.
- 6.40 An updated Ecological Assessment has subsequently been provided by the applicant dated 3/3/2021. This includes additional information in relation to bats, reptiles and badgers and net gain for biodiversity.
- 6.41 In summary following a further badger survey there was considered to be no field sign of recent badger activity within the site and no badger specific mitigation or licensing is required, and encouraging badger foraging within the site is not recommended due to its limited potential. Possible bat foraging/commuting along and through the boundary habitats will be protected and enhanced through the proposed landscaping which will retain and enhance these areas. Lighting specification has been recommended. In terms of reptiles following review it is not likely that a viable reptile population could establish within 2 years on this site – with the approved development of the adjacent possible reptile donor habitats since the 2019 survey efforts. A mitigation measure is recommended in the unlikely event that there is individual reptile presence prior to commencement.
- 6.42 In terms of net gain for biodiversity a number of measures have been recommended to achieve a measurable net gain including new planting, landscape management, bat and bird boxes, low impact bat lighting and boundary dead wood refugia for possible hedgehog, reptiles and stag beetles.
- 6.43 SWT has been consulted on the updated PEA but has not to date provided any further comments. However, based on the evidence provided by the applicant's qualified ecologists it is considered that the updated PEA has addressed all the areas raised by SWT and that there are no overriding

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species or habitat issues to prevent or delay development and the necessary mitigation and enhancement measures can be secured by suitably worded conditions.

## Impact on Archaeology

- 6.44 The site is within an Area of High Archaeological Potential (AHAP). Given this archaeological interest the applicant submitted a desk based archaeological assessment. The assessment reveals that the application site was included within the area that was investigated by means of an extensive trial trench evaluation in advance of the North West Sector development commencing in 2005. Significant archaeological remains were recorded, it is therefore clear that there are highly significant remains present at the site and further mitigation works will be required in advance of the development.
- 6.45 Therefore, the Surrey County Council Archaeological Officer has advised that the mitigation would be by means of strip, map and record area excavation. A pre-commencement condition is recommended to secure the agreement of an appropriate Written Scheme of Investigation and its implementation.

## Sustainable Construction

- 6.46 DMP Policy CCF1 relates to climate change mitigation and requires new development to meet the national water efficiency standard of 110litres/person/day and to achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations. Whilst the applicant has not acknowledged the updated requirements of the DMP, rather setting out the requirements of the North West Sector of 10% in the event that planning permission is to be granted, a condition could be imposed to seek such information and its implementation prior to the first occupation of development. In this regard, there would be no conflict with DMP Policy CCF1.
- 6.47 A condition is also recommended to ensure that each dwelling is fitted with access to fast broadband services in accordance with policy INF3 of the DMP.

## Drainage matters

- 6.48 The site is in Flood Zone 1 and is not in an area identified as being at any significant risk of surface water flooding. The application is however, given its size, required to incorporate sustainable drainage systems. In this respect, the application was supported by a drainage strategy which proposes a system of attenuation and controlled discharge.
- 6.49 This strategy has been considered by Surrey County Council as the Lead Local Flood Authority who has concluded that it meets the requirements of national technical standards. They therefore raise no objection subject to a condition securing implementation.

## Community Infrastructure Levy (CIL)

- 6.50 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable, although the exact amount would be determined and collected after the grant of planning permission.
- 6.51 In terms of other contributions and planning obligations, the Community Infrastructure Levy (CIL) Regulations which were introduced in April 2010 which states that it is unlawful to take a planning obligation into account unless its requirements are (i) relevant to planning; (ii) necessary to make the proposed development acceptable in planning terms; and (iii) directly related to the proposed development. As such only contributions, works or other obligations that are directly required as a consequence of development can be requested and such requests must be fully justified with evidence. In this case, no such contributions or requirements have been requested or identified. It should also be noted that as this site is part of the wider North West Sector Allocation there are a significant amount of new infrastructure being provided as part of the wider development including a school, shops and community facilities which this scheme would be able to access.

## Other Matters

- 6.52 The Surrey Police Designing Out Crime Officer has not raised an objection to the scheme but has raised some concern about the 'leaking' cul-de-sac approach as this allows additional escape routes. Whilst this is appreciated as set out above the inclusion of a pedestrian/cycle link to the North West Sector development is crucial to the acceptability of the scheme. It is noted that the officer has not raised an objection and also advises that security is incorporated within Building Regulations. The officer recommends a condition or informative to ensure that the scheme achieves standards within the Secured by Design (SBD) award. As this can be dealt with through building regulations for a scheme of this size an informative is considered appropriate.
- 6.53 Contamination - A Phase 1 Geoenvironmental Survey of the site was been undertaken by GEMCO. The submitted report concludes that the site is suitable for the proposed residential end use, subject to limited further geoenvironmental investigation to confirm the assumptions and observations of the initial site survey. Conditions are recommended to secure these further investigations and to deal with any unsuspected contamination.
- 6.54 Objection has been raised on the grounds of inconvenience during the construction period. Whilst it is acknowledged there may be a degree of disruption during the construction phase, the proposal would not warrant refusal on this basis. A condition is recommended in relation to a

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Construction Transport Management Plan and a condition to control matters such as working hours, noise and pollution is recommended to help mitigate any potential impact. Statutory nuisance legislation also exists to control any significant disturbance caused during the construction of the proposal.

- 6.55 Policy DES7 of the DMP requires that on sites of 5 or more homes at least 20% of homes should meet the Building Regulations requirements for 'accessible and adaptable dwellings'. The applicant has not referred to this requirement. Without any evidence to the contrary it is considered that such a requirement would be viable for the applicant and therefore a condition is recommended to secure adequate accessible housing in accordance with policy DES7.

## CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans:

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date</b>
Location Plan	884-LOC-01		01.12.2020
Proposed Plans	884-PL-02	B	01.12.2020
Proposed Plans	884-PL-03	A	01.12.2020
Proposed Plans	884-PL-04	A	01.12.2020
Proposed Plans	904-PL-05	A	01.12.2020
Proposed Plans	904-PL-06	A	01.12.2020
Proposed Plans	884-PL-07	A	01.12.2020
Other Plan	D-01	A	01.12.2020
Site Layout Plan	884-PL-01		11.03.2021
Site Layout Plan	884-PL-08		11.03.2021
Site Layout Plan	DR2		11.03.2021
Other Plan	DR1	B	11.03.2021
Survey Plan	1		15.05.2019

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

3. No development shall take place until the applicant has secured the implementation of a programme of archaeological excavation and post-

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excavation work in accordance with a Written Scheme of Investigation which has been submitted to and approved by the Local Planning Authority.

Reason: The site covers an area in which it is considered necessary to preserve for future reference any archaeological information before it is destroyed by the development with regard to policy NHE9 of the Reigate and Banstead Borough Development Management Plan 2019. This is necessary to be a pre-commencement condition because the suitable recording of archaeology goes to the heart of the planning permission.

4. Prior to commencement of development, in follow-up to the environmental desktop study by Gemoco dated March 2019, a contaminated land site investigation proposal, detailing the extent and methodologies of sampling, analyses and proposed assessment criteria required to enable the characterisation of the plausible pollutant linkages identified in the preliminary conceptual model, shall be submitted to the Local Planning Authority. This is subject to the written approval in writing of the Local Planning Authority, and any additional requirements that it may specify, prior to any site investigation being commenced on site. Following approval, the Local Planning Authority shall be given a minimum of two weeks written notice of the commencement of site investigation works. Please note this means a proposal is required to be submitted and approved prior to actually undertaking a Site Investigation.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

5. Prior to commencement of the development, a contaminated land site investigation and risk assessment, undertaken in accordance with the site investigation proposal as approved that determines the extent and nature of contamination on site and is reported in accordance with the standards of DEFRA's and the Environment Agency's Land Contamination: Risk Management Guidance (2020) and British Standard BS 10175, shall be submitted to the Local Planning Authority and is subject to the approval in writing of the Local Planning Authority and any additional requirements that it may specify. If applicable, ground gas risk assessments should be completed inline with CIRIA C665 guidance.

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

6. A. Prior to commencement of the development a detailed remediation method statement should be produced that details the extent and method(s) by which the site is to be remediated, to ensure that unacceptable risks are not posed

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to identified receptors at the site and details of the information to be included in a validation report, has been submitted to and approved in writing by the Local Planning Authority, and any additional requirements that it may specify, prior to the remediation being commenced on site. The Local Planning Authority shall then be given a minimum of two weeks written notice of the commencement of remediation works.

B. Prior to occupation, a remediation validation report for the site shall be submitted to the Local Planning Authority in writing. The report shall detail evidence of the remediation, the effectiveness of the remediation carried out and the results of post remediation works, in accordance with the approved remediation method statement and any addenda thereto, so as to enable future interested parties, including regulators, to have a single record of the remediation undertaken at the site. Should specific ground gas mitigation measures be required to be incorporated into a development the testing and verification of such systems should have regard to CIRIA C735 guidance document entitled 'Good practice on the testing and verification of protection systems for buildings against hazardous ground gases' and British Standard BS 8285 Code of practice for the design of protective measures for methane and carbon dioxide ground gases for new buildings

Reason: To demonstrate remedial works are appropriate and demonstrate the effectiveness of remediation works so that the proposed development will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

7. Unexpected ground contamination: Contamination not previously identified by the site investigation, but subsequently found to be present at the site shall be reported to the Local Planning Authority as soon as is practicable. If deemed necessary development shall cease on site until an addendum to the remediation method statement, detailing how the unsuspected contamination is to be dealt with, has been submitted in writing to the Local Planning Authority. The remediation method statement is subject to the written approval of the Local Planning Authority and any additional requirements that it may specify.

Note: Should no further contamination be identified then a brief comment to this effect shall be required to discharge this condition

Reason: To ensure that the proposed development and any site investigations and remediation will not cause harm to human health or pollution of controlled waters with regard to the Reigate and Banstead Local Plan Development Management Plan 2019 (Policy DES9 Pollution and contaminated Land) and the NPPF.

8. No development shall take place until the developer obtains the Local Planning Authority's written approval of details of the existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

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**Reason:** To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

9. No development shall commence until a Construction Management Statement, to include details of:
- Prediction of potential impacts with regard to water, waste, noise and vibration, dust, emissions and odours, wildlife. Where potential impacts are identified, mitigation measures should be identified to address these impacts.
  - Information about the measures that will be used to protect privacy and the amenity of surrounding sensitive uses; including provision of appropriate boundary protection.
  - Means of communication and liaison with neighbouring residents and businesses.
  - Hours of work.

Has been submitted to and improved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

**Reason:** The condition above is required in order that the development is managed in a safe and considerate manner to help mitigate potential impact on the amenity and safety of neighbours and to accord with policy DES8 of the Reigate and Banstead Development Management Plan 2019.

10. No development shall commence including demolition and or groundworks preparation until a detailed, scaled finalised Tree Protection Plan (TPP) and the related finalised Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas of trees (RPA) shown to scale on the TPP, including the installation of service routings, type of surfacing for the entrance drive and location of site offices. The AMS shall also include a scheme of supervision which shall include:
- Pre commencement meeting between the retained arboricultural consultant, local planning authority Tree Officer and individuals and personnel responsible for the implementation of the approved development
  - Timings, frequency of the supervision and monitoring regime and an agreed reporting process to the local planning authority.
  - The supervision monitoring and reporting process shall be undertaken by a qualified arboriculturist.

All works shall be carried out in strict accordance with the agreed details when approved.

**Reason:** To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and

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Construction – Recommendations’ and reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies NHE3 and DES1 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction.

11. No development shall commence on site until a scheme for the soft and hard landscaping (including hard surfacing and any street furniture), including details of existing landscape features to be retained or pruned, has been submitted and approved in writing by the local planning authority. The landscaping scheme shall include details of hard landscaping, planting plans, written specifications (including cultivation and other operations associated with tree, shrub, and hedge or grass establishment), schedules of plants, noting species, plant sizes and proposed numbers/densities and an implementation programme.

All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to first occupation of the approved development or in accordance with a programme agreed in writing with the local planning authority

All new tree planting shall be positioned in accordance with guidelines and advice contained in the current British Standard 5837. Trees in relation to construction.

Any trees shrubs or plants planted or any existing plants/hedging retained in accordance with this condition which are removed, die or become damaged or become diseased within five years of planting shall be replaced within the next planting season by trees, and shrubs of the same size and species.

Reason: To ensure good arboricultural and landscape practice in the interests of the maintenance of the character and appearance of the area and Meath Green Conservation Area, and to comply with policies NHE3, NHE9 and DES1 of the Reigate and Banstead Borough Development Management Plan 2019, British Standards including BS8545:2014 and British Standard 5837:2012.

12. Before the development is commenced the proposed belmouth vehicular access to Meath Green Lane and the first 15 metres of the new access road shall be constructed in accordance with the approved plan numbered DR1 rev B.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access , and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.



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13. Notwithstanding the submitted plan numbered 2884 PL 01 B no part of the development hereby approved shall be first commenced unless and until a safe and accessible pedestrian route between the Application Site and the eastern access road (main spine route) has been fully provided in accordance with a revised scheme to be submitted to and approved in writing by the Local Planning Authority. The revised scheme shall include details of the management of the access path and how it will be maintained and kept open for unimpeded access for the lifetime of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing TAP2 Airport Car Parking of the Reigate and Banstead Local Plan Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

14. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) provision of boundary hoarding behind any visibility zones
  - (g) vehicle routing
  - (h) measures to prevent the deposit of materials on the highway
  - (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
  - (k) on-site turning for construction vehicles
- has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing and DES8 Construction Management of the Reigate and Banstead Development Management Plan September 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

15. No development above slab level shall take place until details setting out how the applicant will ensure that at least 20%, unless otherwise agreed in writing, of the homes meet the Building Regulations requirements for 'accessible and adaptable dwellings' have been submitted to and agreed in writing by the Local Planning Authority. The development shall be implemented in accordance with the agreed details.

Reason: In order that the scheme provides accessible housing in accordance with policy DES7 of the Reigate and Banstead Borough Council Development Management Plan 2019.

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16. No development above slab level shall take place until written details of; the materials to be used in the construction of the external surfaces, including bricks, roof and hanging tiles and any other external materials; and final details of gutters, bargeboards, and brick arches or heads, have been submitted to and approved in writing by the Local Planning Authority. It is expected that the details should be in accordance with the following:
- For plots 1,2, 9 and 10 all brickwork shall be in handmade Flemish bond brick including stringcourses. For plots 1,2, 9 and 10 all tiles shall be of handmade sandfaced plain clay tiles, the tile hanging lighter than the roof tile. Winchester cut is not generally permitted.
  - Soldier brick arches, headers or stringcourses are not permitted. Brick arches or heads shall be of gauged brick or header on edge.
  - All bargeboards shall be straight edged with no boxed ends.
  - Any gutters fascias shall be no greater than 150mm height, or without gutter fascia.
  - Any weatherboarding shall be black finished featheredge boarding

The development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development with regard to Development Management Plan 2019 policy DES1.

17. No development above slab level shall take place until written details of the proposed fenestration including rooflights, and details of the doors, vents and windows (including how they open), has been submitted to and approved in writing by the Local Planning Authority. It is expected that the details should be in accordance with the following:
- All rooflights shall be black painted metal conservation rooflights with a single vertical glazing bar.
  - All sash windows shall be vertically sliding and set back behind the reveal at one brick depth. All casements shall have a casement in each opening to ensure equal sightlines.
  - All glazing bars shall be external and of traditional profile.

The development shall be carried out in accordance with the approved details.

Reason: To ensure that a satisfactory external appearance is achieved of the development which preserves the Meath Green Conservation Area with regard to Development Management Plan 2019 policy DES1 and NHE9.

18. No development above slab level shall take place until a plan indicating the positions, design, materials and type of boundary treatment, fencing or other means of enclosure to be erected has been submitted to and approved in writing by the Local Planning Authority. For any solid fencing, it is expected that this should be of vertically boarded close boarded timber with timber

posts and gravel boards. The boundary treatment shall be completed before the first occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area, to preserve the Meath Green Conservation Area, and protect neighbouring residential amenities with regard to the policy DES1 and NHE9 of the Reigate and Banstead Development Management Plan 2019.

19. No development above ground level shall commence until a scheme to provide positive biodiversity benefits, informed by the submitted Eco-Planning UK ecology report 'Revised Ecological Assessment Biodiversity Protection/Provision' (dated 3/3/2021), has been submitted to and approved in writing by the local planning authority (LPA). This should be designed alongside the soft landscaping proposals for the site. The biodiversity enhancement measures approved shall be carried out and maintained in strict accordance with these details or as otherwise agreed in writing by the LPA, and before occupation of this development.

Reason: To provide enhancements to the biodiversity of the site in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

20. Prior to the first occupation of the development a verification report carried out by a qualified drainage engineer must be submitted to and approved by the Local Planning Authority. This must demonstrate that the drainage system has been constructed as per the agreed scheme (Suds Report, Edward Parsley Associated Ltd, December 2020, Ref 11835 and drawing D-01A dated 11/11/20), or detail any minor variations, it must provide the details of any management company and state the national grid reference of any key drainage elements (surface water attenuation devices/area, flow restriction devices and outfalls).

The drainage system shall therefore be retained and maintained in accordance with the agreed details.

Reason: To ensure the drainage system is constructed to the national Non-Statutory Technical Standards for SuDs in order to mitigate against the risk of surface water flooding with regard to policy INF1 and CCF2 of the Reigate and Banstead Development Management Plan 2019.

21. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with approved plan numbered 884-PL-01-B and 884-PL-08 for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking spaces (all surface parking spaces and the garages for Plots 3, 7 and 8) and turning areas shall be retained and maintained for their designated purposes.

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Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Access, Parking, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

22. The development hereby approved shall not be first occupied unless and until space has been laid out within the site, in accordance with details and plans to be submitted to and approved in writing, for cycles to be parked in a covered and secure location for each dwelling. Thereafter the cycle parking area shall be retained and maintained for its designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17.

23. Prior to the first occupation of the development full details (and plans where appropriate) of the waste management storage and collection points, (and pulling distances where applicable), throughout the development shall be submitted to and approved in writing by the Local Planning Authority.

All waste storage and collection points should be of an adequate size to accommodate the bins and containers required for the dwelling(s) which they are intended to serve in accordance with the Council's guidance contained within Making Space for Waste Management in New Development.

Each dwelling shall be provided with the above facilities in accordance with the approved details prior to occupation of the relevant dwellings.

Reason: To provide adequate waste facilities in the interests of the amenities of the area and to encourage recycling in accordance with the Development Management Plan 2019 policy DES1.

24. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirement: 7kw Mode 3 with Type 2 connector - 230 v AC 32 amp single phase dedicated supply) in accordance with a scheme to be submitted to and approved in writing by the Local Planning Authority and thereafter retained and maintained for their designated purpose.

Reason: In order that the development promotes more sustainable forms of transport, and to preserve the character of the Conservation Area, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 and policy TAP1 and NHE9 of the Development Management Plan.

25. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:

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- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
- b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

26. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
  - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

27. The development shall be carried out in accordance with the mitigation measures set out within the Eco-Planning UK ecology reports – Revised Ecological Assessment Biodiversity Protection/Provision (dated 3/3/2021) and the Preliminary Ecological Assessment (dated 25/01/2019).

Reason: To ensure that any potential impact to protected species is adequately mitigated in accordance with the provisions of the National Planning Policy Framework and policy NHE2 of the Development Management Plan 2019.

28. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions or roof enlargements permitted by classes A or B of Part 1 of the Second Schedule of the 2015 Order (as amended) shall be constructed without the prior approval Local Planning Authority.

Reason: To control any subsequent enlargements in the interests of the visual and residential amenities of the locality and to preserve the Meath Green Conservation Area with regard to Reigate and Banstead Development Management Plan 2019 policy DES1 and NHE9.

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## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.org.uk](http://www.firesprinklers.org.uk).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. The Council's Neighbourhood Services team can be contacted on 01737 276292 or via the Council's website at [http://www.reigate-banstead.gov.uk/info/20085/planning\\_applications/147/recycling and waste developers guidance](http://www.reigate-banstead.gov.uk/info/20085/planning_applications/147/recycling_and_waste_developers_guidance)
4. You are advised that the Council will expect the following measures to be included within the Construction Management Statement require by Condition 9 to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

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In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigatebanstead.gov.uk/info/20277/street naming and numbering](http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering)
6. The permission hereby granted shall not be construed as authority to carry out any works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath, carriageway, verge or other land forming part of the highway. All works (including Stats connections/diversions required by the development itself or the associated highway works) on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see: <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>. The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see: [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/floodingadvice).
7. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
8. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).

# Agenda Item 8

Planning Committee  
14 April 2021

Agenda Item: 8  
Application 19/0986/F

9. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
10. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
11. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
12. The Highway Authority has no objection to the proposed development, subject to the above conditions but, if it is the applicant's intention to offer any of the roadworks included in the application for adoption as maintainable highways, permission under the Town and Country Planning Act should not be construed as approval to the highway engineering details necessary for inclusion in an Agreement under Section 38 of the Highways Act 1980. Further details about the post-planning adoption of roads may be obtained from the Transportation Development Planning Division of Surrey County Council.
13. The use of a suitably qualified arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837.
14. The use of landscape/arboricultural consultant is considered essential to provide acceptable submissions in respect of the above landscaping condition. The planting of trees and native hedging shall be in keeping with the character and appearance of the locality. There is an opportunity to incorporate structural landscape trees into the scheme to provide for future amenity and long term continued structural tree cover in this area. It is expected that the replacement structural landscape trees will be of Advanced Nursery Stock sizes with initial planting heights of not less than 4.5m with girth measurements at 1m above ground level in excess of 16/18cm.
15. Biodiversity enhancements – with regard to condition 19 the Council expects the applicant to provide an appropriately detailed document to demonstrate that a measurable net gain (not just compensation), secure for the life time of the development, is achievable. The applicant may wish to use an appropriate metric such as the DEFRA Biodiversity Metric 2.0 to demonstrate how the site



Planning Committee  
14 April 2021

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will provide biodiversity net gain. If net gain cannot be met this must be fully justified.

16. The applicant is advised that the development should seek to achieve standards contained within the Secured by Design award scheme to ensure that it results in a safe development.
17. Environmental Health would like to draw the applicant attention to the specifics of the contaminated land conditional wording such as 'prior to commencement', 'prior to occupation' and 'provide a minimum of two weeks notice'.

The submission of information not in accordance with the specifics of the planning conditional wording can lead to delays in discharging conditions, potentially result in conditions being unable to be discharged or even enforcement action should the required level of evidence/information be unable to be supplied. All relevant information should be formally submitted to the Local Planning Authority and not direct to Environmental Health.

## **REASON FOR PERMISSION**

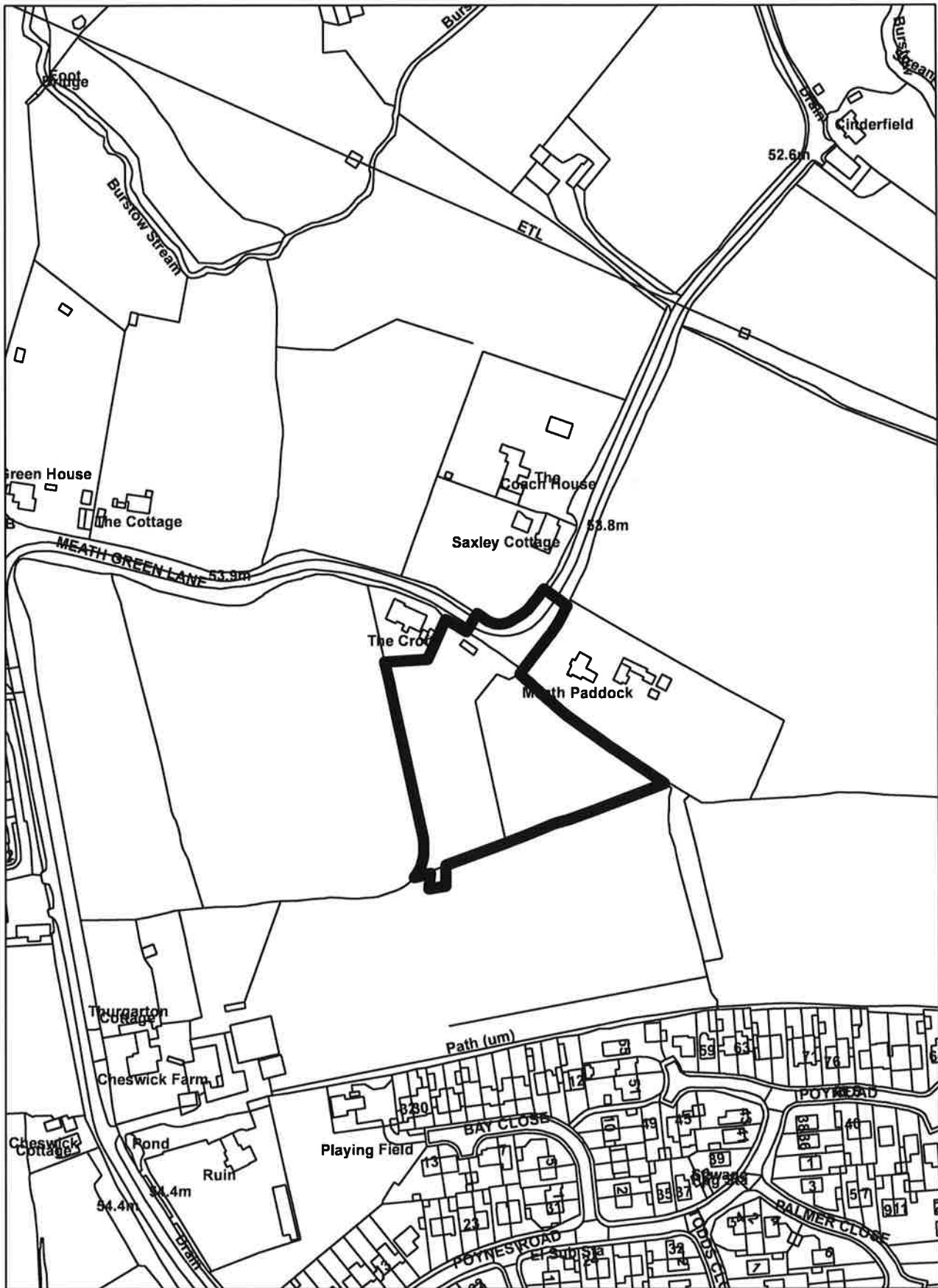
The development hereby permitted has been assessed against development plan policies CS1, CS2, CS4, CS5, CS10, CS11, CS12, CS13, CS14, CS17 of the Core Strategy and policies, DES1, DES2, DES4, DES5, DES7, DES8, DES9, TAP1, CCF1, CCF2, NHE2, NHE3, NHE9, INF1, INF2, INF3 of the Development Management Plan 2019 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

# Agenda Item 8

## 19/00986/F - Land At The Croft, Meath Green Lane, Horley



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Scale 1:2,500



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This design is for informational purposes only. All dimensions are to be checked and verified on site. All areas not designated are to be provided at the discretion of the contractor. All materials, methods, and practices to be used are to be as specified in the contract documents.

# Agenda Item 8

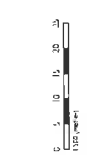
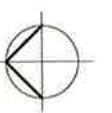
**ACCOMMODATION SCHEDULE**

Plot	Rooms
Plot 1	4 Bed - 1928sq
Plot 2	4 Bed - 1731sq
Plot 3	3 Bed - 712sq
Plot 4	4 Bed - 2238sq
Plot 5	4 Bed - 2038sq
Plot 6	3 Bed - 714sq
Plot 7	3 Bed - 952sq
Plot 8	3 Bed - 952sq
Plot 9	3 Bed - 952sq
Plot 10	3 Bed - 952sq

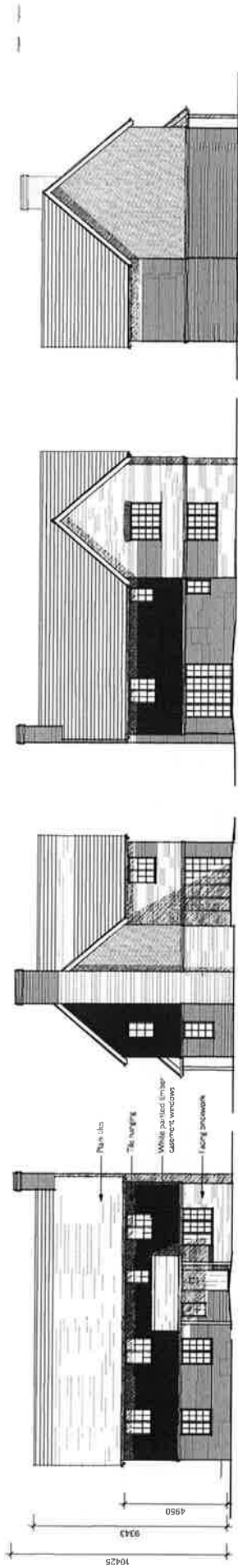


**ARC HILL CILIS**
  
 884-PL-01 B

Prepared by: **Marcin Tomaszewski**
  
 Date: 10/10/2023



This plan is a preliminary plan and is not intended to be used for construction. It is subject to the approval of the relevant authorities. The plan is not intended to be used for construction. It is subject to the approval of the relevant authorities.



FRONT ELEVATION

SIDE ELEVATION

REAR ELEVATION

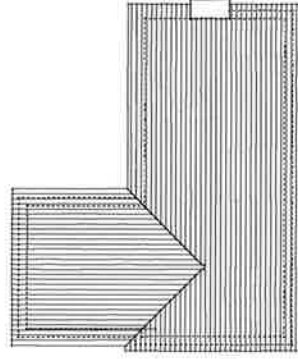
SIDE ELEVATION



GROUND FLOOR PLAN PLOTS 1 & B



FIRST FLOOR PLAN



ROOF PLAN

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PLOTS 1 & B  
AREA  
GIA: 1950sqft

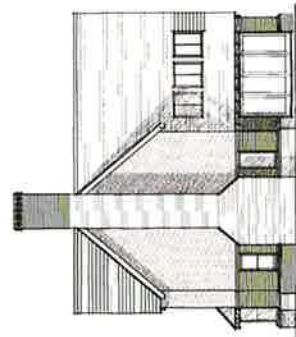
Robert Hutton Architects Ltd  
110, 112, 114, 116, 118, 120, 122, 124, 126, 128, 130, 132, 134, 136, 138, 140, 142, 144, 146, 148, 150, 152, 154, 156, 158, 160, 162, 164, 166, 168, 170, 172, 174, 176, 178, 180, 182, 184, 186, 188, 190, 192, 194, 196, 198, 200, 202, 204, 206, 208, 210, 212, 214, 216, 218, 220, 222, 224, 226, 228, 230, 232, 234, 236, 238, 240, 242, 244, 246, 248, 250, 252, 254, 256, 258, 260, 262, 264, 266, 268, 270, 272, 274, 276, 278, 280, 282, 284, 286, 288, 290, 292, 294, 296, 298, 300, 302, 304, 306, 308, 310, 312, 314, 316, 318, 320, 322, 324, 326, 328, 330, 332, 334, 336, 338, 340, 342, 344, 346, 348, 350, 352, 354, 356, 358, 360, 362, 364, 366, 368, 370, 372, 374, 376, 378, 380, 382, 384, 386, 388, 390, 392, 394, 396, 398, 400, 402, 404, 406, 408, 410, 412, 414, 416, 418, 420, 422, 424, 426, 428, 430, 432, 434, 436, 438, 440, 442, 444, 446, 448, 450, 452, 454, 456, 458, 460, 462, 464, 466, 468, 470, 472, 474, 476, 478, 480, 482, 484, 486, 488, 490, 492, 494, 496, 498, 500, 502, 504, 506, 508, 510, 512, 514, 516, 518, 520, 522, 524, 526, 528, 530, 532, 534, 536, 538, 540, 542, 544, 546, 548, 550, 552, 554, 556, 558, 560, 562, 564, 566, 568, 570, 572, 574, 576, 578, 580, 582, 584, 586, 588, 590, 592, 594, 596, 598, 600, 602, 604, 606, 608, 610, 612, 614, 616, 618, 620, 622, 624, 626, 628, 630, 632, 634, 636, 638, 640, 642, 644, 646, 648, 650, 652, 654, 656, 658, 660, 662, 664, 666, 668, 670, 672, 674, 676, 678, 680, 682, 684, 686, 688, 690, 692, 694, 696, 698, 700, 702, 704, 706, 708, 710, 712, 714, 716, 718, 720, 722, 724, 726, 728, 730, 732, 734, 736, 738, 740, 742, 744, 746, 748, 750, 752, 754, 756, 758, 760, 762, 764, 766, 768, 770, 772, 774, 776, 778, 780, 782, 784, 786, 788, 790, 792, 794, 796, 798, 800, 802, 804, 806, 808, 810, 812, 814, 816, 818, 820, 822, 824, 826, 828, 830, 832, 834, 836, 838, 840, 842, 844, 846, 848, 850, 852, 854, 856, 858, 860, 862, 864, 866, 868, 870, 872, 874, 876, 878, 880, 882, 884, 886, 888, 890, 892, 894, 896, 898, 900, 902, 904, 906, 908, 910, 912, 914, 916, 918, 920, 922, 924, 926, 928, 930, 932, 934, 936, 938, 940, 942, 944, 946, 948, 950, 952, 954, 956, 958, 960, 962, 964, 966, 968, 970, 972, 974, 976, 978, 980, 982, 984, 986, 988, 990, 992, 994, 996, 998, 1000.

884-PL-02 B  
ARCHITECTS

# Agenda Item 8



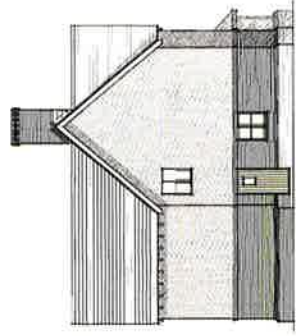
Front Elevation



Side Elevation



Rear Elevation



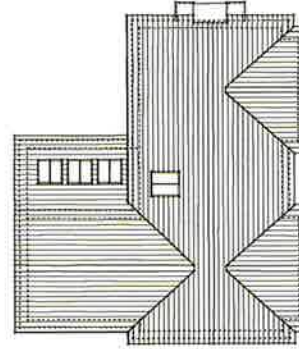
Side Elevation



Ground Floor Plan  
Plots 3 & 7



First Floor Plan



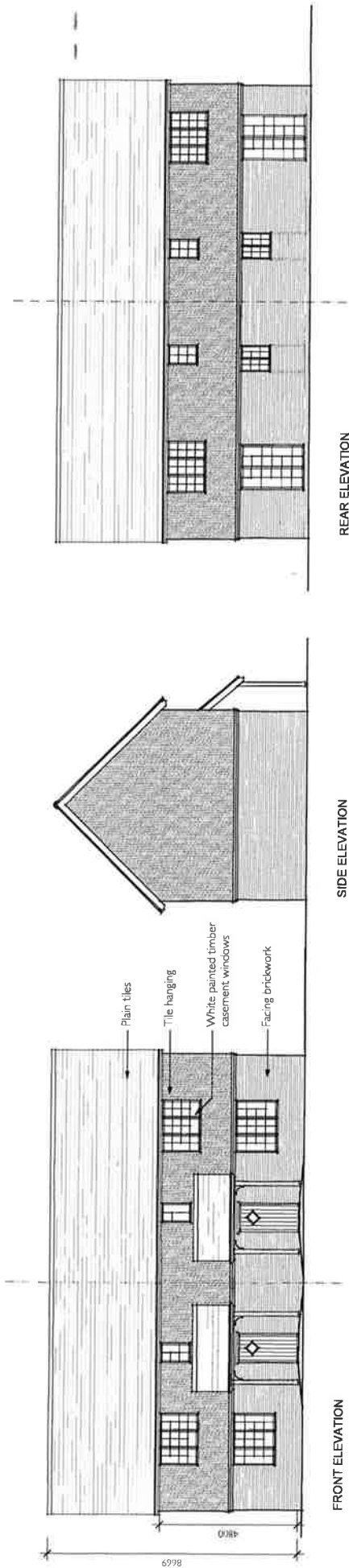
Roof Plan

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Plots 3 & 7  
AREA  
GIA: 2142sqft

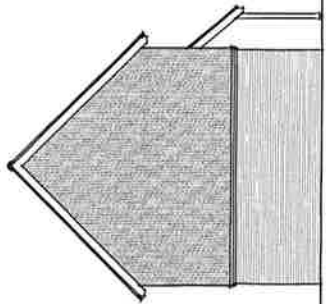
Robert Hudson Architects  
884-PL-04 A  
ARCHITECTS



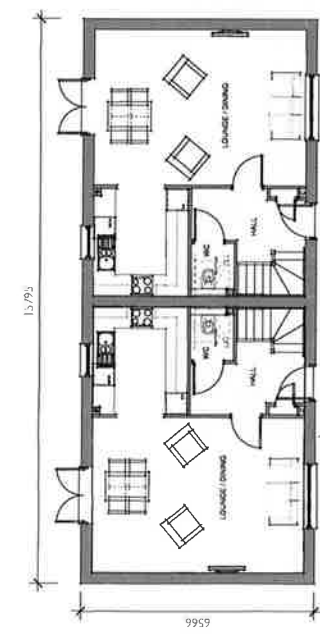


FRONT ELEVATION

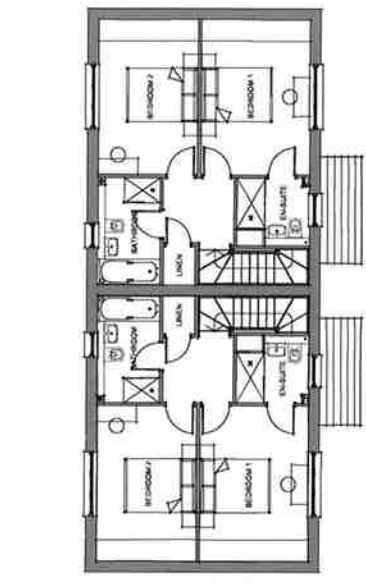
REAR ELEVATION



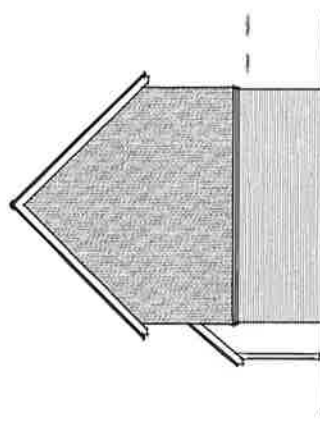
SIDE ELEVATION



GROUND FLOOR PLAN



FIRST FLOOR PLAN



SIDE ELEVATION

- Plain tiles
- Tile hanging
- White painted timber casement windows
- Facing brickwork

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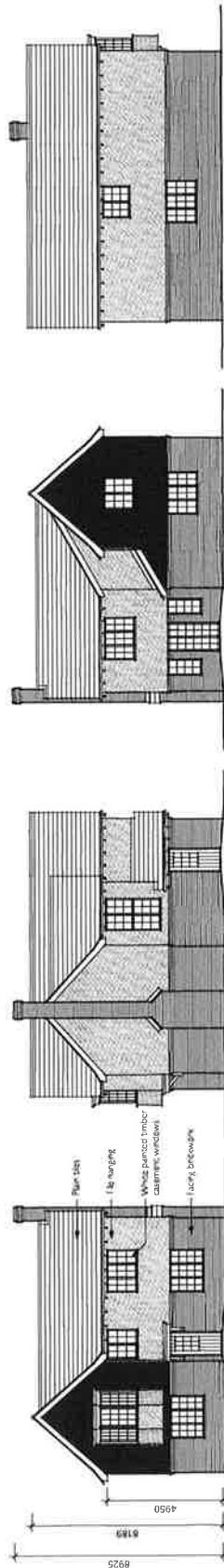
Position of structural elements for guidance only, exact location and sizes to be verified in field.

**PLOTS 9 & 10**  
**AREA**  
**GIA : 955sqft**



Marden Homes  
 The Craft  
 Math Green, Surrey  
 Plots 9 & 10 plans & elevation  
 1:100 A/E 10  
 1:100 A/E  
 904-PL-06 A  
**ROBERT HUDSON ARCHITECTS**  
 THE MILL - 145 THE EAST HANNINGFIELD - ESSEX - CM3 8AE Tel: 01245 800008 - max@robert-hudson-architects.co.uk

# Agenda Item 8



FRONT ELEVATION PLOTS 2 & 4

SIDE ELEVATION PLOTS 2 & 4

REAR ELEVATION PLOTS 2 & 4

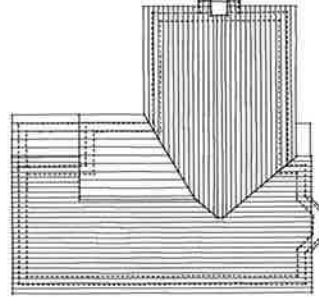
SIDE ELEVATION PLOTS 2 & 4



GROUND FLOOR PLAN PLOTS 2 & 4



FIRST FLOOR PLAN PLOTS 2 & 4



ROOF PLAN PLOTS 2 & 4

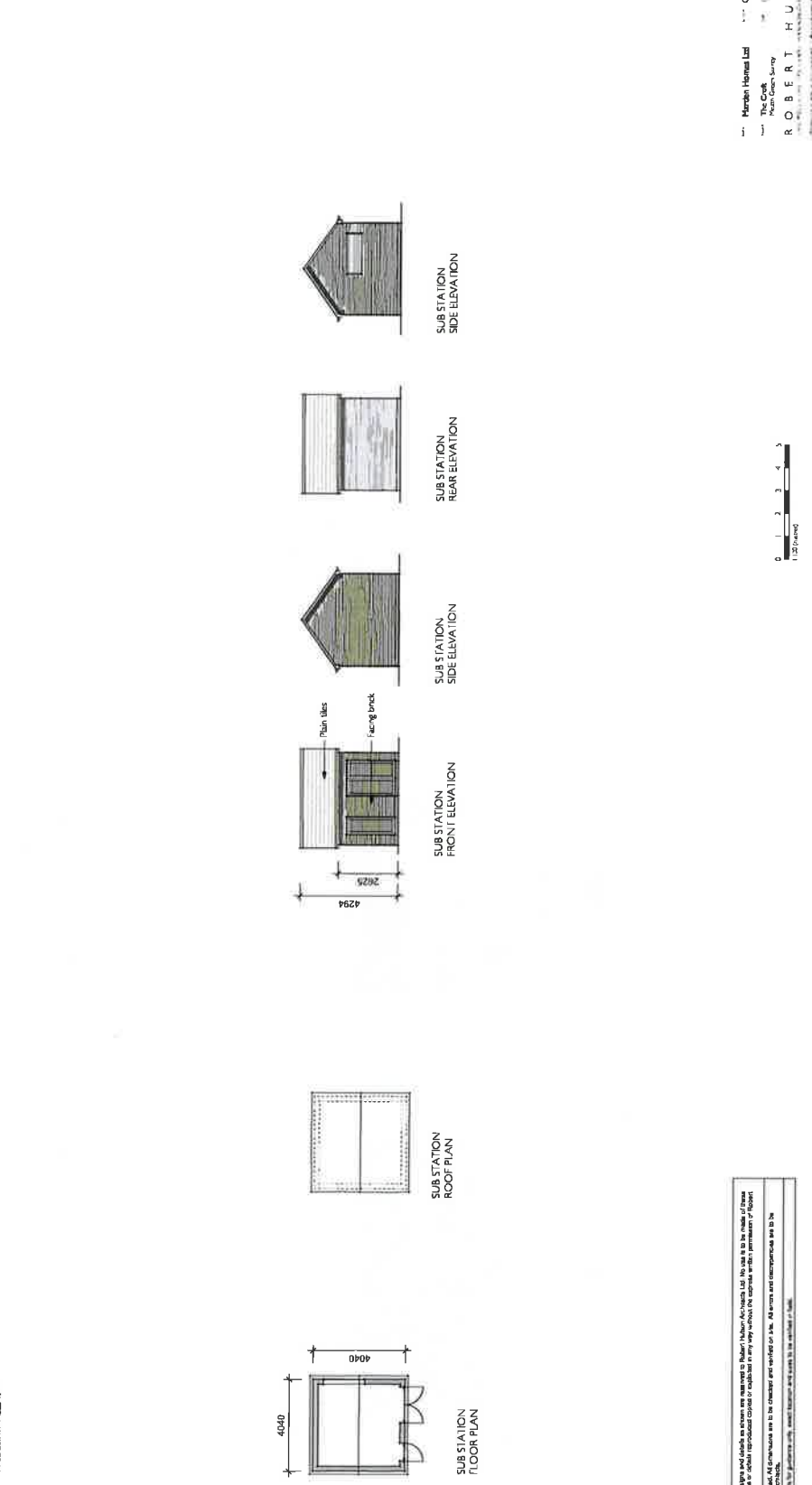
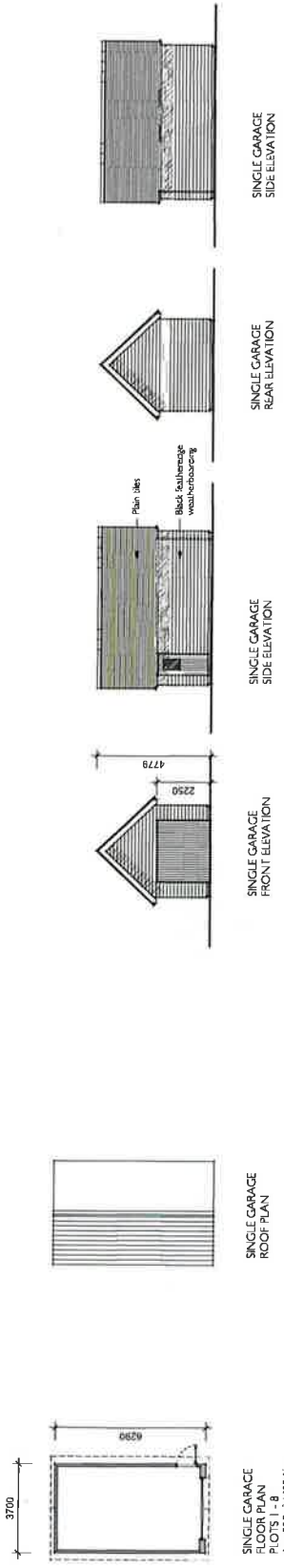
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PLOTS 2 & 4  
AREA  
GIA: 1731 sqft

**ROBERT HUTSON ARCHITECTS**
  
 The Craft Group
   
 884-PL-03 A
   
 ARCHITECTS





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Robert Hutson Architects Ltd, 115, The Quadrant, London, W2 2JF, United Kingdom and details to be written to: 115, The Quadrant, London, W2 2JF, United Kingdom

# Agenda Item 8



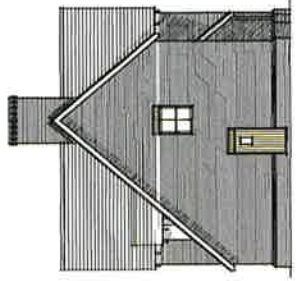
Front Elevation



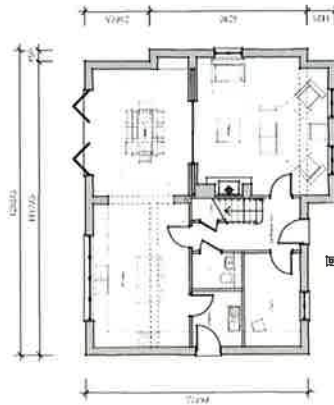
Side Elevation



Rear Elevation



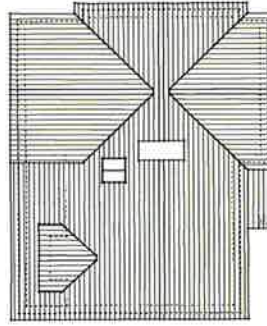
Side Elevation



Ground Floor Plan  
Plots 5 & 6



First Floor Plan




Roof Plan

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PLOTS 5 & 6 - 4b  
AREA  
2038 sqft



Meridian Homes  
The Craft  
Finish-Grade Living  
ROBERT HUTSON ARCHITECTS  
Plot 5 & 6 Area & Location  
904-PL-05 A  
ARCHITECTS

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	14 <sup>th</sup> April 2021
	<b>REPORT OF:</b>	HEAD OF PLACES & PLANNING
	<b>AUTHOR:</b>	James Amos
	<b>TELEPHONE:</b>	01737 276188
	<b>EMAIL:</b>	james.amos@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	9	<b>WARD:</b> <i>Tattenham Corner and Preston</i>

<b>APPLICATION NUMBER:</b>	20/02510/F	<b>VALID:</b>	02/12/2020
<b>APPLICANT:</b>	Devine Homes Plc	<b>AGENT:</b>	None
<b>LOCATION:</b>	<b>10 WEST DRIVE AND LAND TO THE REAR OF 9,11 AND 12 WEST DRIVE, BURGH HEATH, SURREY, KT20 5PA</b>		
<b>DESCRIPTION:</b>	<b>Demolition of dwelling at 10 West Drive, and erection of 7 residential dwellings on land to the rear of 9-12 West Drive, associated landscaping, parking, access onto West Drive, and associated ancillary work. As amended on 05/02/2021</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

## SUMMARY

This is a full application for the demolition of 10 West Drive and the erection of 7 dwellings on land to the rear, with associated landscaping and car parking. The dwellings would comprise 2 x 2 bedroom and 5 x 3 bedroom semi-detached and detached bungalows and chalet bungalows and a total of 16 parking spaces are proposed (2 spaces per unit and 2 visitor bays).

There is recent planning history (ref: 17/01261/F) at the site for a more substantial backland development, comprising of 9 two storey semi-detached and terraced dwellings in a similar layout to the current proposal.

An appeal was dismissed in March 2018 with the Inspector finding harm to the character of the area, by virtue of the urban form of development which would not amount to a subservient form of development in this location and would fail to promote or reinforce the distinctive characteristics of this established residential area. The appeal decision and previous layout are attached.

The Inspector also raised concerns about the scale and two storey form of the proposed dwellings, different to those typically found in the area and limited opportunity to soften the impact of development through a comprehensive landscaping scheme.

# Agenda Item 9

Planning Committee  
14<sup>th</sup> April 2021

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This application has made amendments to reduce the number of units from 9 to 7, to reduce the scale of the dwellings to a bungalow and chalet style bungalows and to increase separation distances to side boundaries. Cumulatively, the revisions to the layout, design, and scale of the dwellings is considered to have overcome the harm identified by the Inspector. It is considered that the proposal would respect the character of the existing area and the reductions in scale would allow for a more spacious form of development.

The proposed dwellings would be of a traditional design with brick and hanging elevations and pitched roofs finished with clay tiles, a form of design which reflects which would be in keeping with the character of the area. Each dwelling would be of the appropriate size in terms of the Nationally Described Space Standards and would be provided with private amenity space.

The impact on the amenities of neighbouring properties is considered acceptable due to good separation and the single storey nature of the proposed dwellings.

The proposal would meet the Council's parking standards as set out within the Development Management Plan. The County Highways Authority have raised no objection to the proposal subject to recommended conditions.

The Council's tree officer has reviewed the plans and considers that the gardens will not be heavily shaded by the trees and therefore his original concerns on this issue have been addressed. He also notes that future applications to undertake tree work will be assessed on a case by case basis and this will ensure trees of value will be retained protecting the local canopy cover. Conditions are recommended to ensure that the tree protection measures are implemented and maintained during the construction period and for details of landscaping to be submitted.

The Council's Neighbourhood Services team have confirmed that the provision of a refuse bin presentation point along the access road into the development would be acceptable, whilst the provision of a turning head at the site entrance would allow the refuse collection vehicle to better access existing properties in West Drive, where at present, due to the narrow width of the road, turning within the highway is not possible.

The proposals would make efficient use of this previously developed site for new housing without harming the amenities of neighbouring properties and are considered acceptable.

## **RECOMMENDATION**

Planning permission is **GRANTED** subject to conditions.

## Consultations:

Highway Authority: No objection subject to conditions. The County Highway Authority has assessed the application on safety, capacity and policy grounds and is satisfied that the application would not have a material impact on the safety and operation of the adjoining public highway with respect of access, net additional traffic generation and parking. The County Highway Authority therefore has no highway requirements subject to conditions which would require the visibility splays and tactile paving at the entrance to the site from West Drive, for the provision of the parking spaces and turning areas prior to occupation, the provision of a construction transport management plan, the provision of electric vehicle charging sockets, the provision of an extended pavement along the proposed access road and the provision of a refuse collection point alongside the access road.

Neighbourhood Services – Have confirmed that the use of a private collection service would not be acceptable option as waste collection is a statutory duty that the Council has a duty to provide.

It is acknowledged that West Drive is already a difficult road to access, and it is considered that a properly constructed turning head access into the site would be advantageous. In order to support this, state that they would require a tracking diagram which shows how a 26 tonne RCV can access a refuse collection point no more than 9m from the existing highway along the proposed access drive. All new developments should be planned to comply with the Health & Safety requirements. Reversing manoeuvres are usually not acceptable and must be kept to a minimum in line with Health & Safety Executive directives.

In support of the application, a tracking drawing has been submitted which shows that a refuse collection vehicle could access the site, utilise the site entrance as a turning head and collect the refuse from a collection point alongside the site access.

The Neighbourhood Services team have confirmed that these arrangements would be acceptable and have also asked that the edges of the bellmouth access be strengthened to give added protection should a refuse collection vehicle run over them.

Surrey CC Countryside Access – Provide advice and guidance on the need to ensure that the adjoining public right of way is not affected by the proposed development.

Reigate Ramblers: the application site abuts a public right of way on its west boundary, ie the footpath designated UF/1014/25/10. This footpath provides access from Great Tattenhams to the Lonesome Ponds area on Burgh Heath. As such the footpath provides access for recreation, a short cut to the Asda superstore, bus routes and the shops on the A217 on Burgh Heath. The footpath is narrow (just over 1.0m wide) unlit and is bordered for most of its length by 1.8m high close boarded fencing interspersed with a few patches of similar height chain link fencing/brick wall.

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Concerns are raised with the proposed development and the site location plan which shows a narrowing of the footpath that is not evident when walking the path. Their requirement would be that the proposed development does not cause any reduction in width to the existing footpath. For safety of footpath users, it is recommended that the fencing alongside the footpath should remain as straight as possible and thus not provide any hidden sight lines that could be potential hiding spots for muggers or other criminals.

Given the residential location of the footpath, and its closed fenced-in nature, it is not considered that there will be objections from footpath users over the proposed development itself.

## Representations:

Letters were sent to neighbouring properties on 8<sup>th</sup> December 2020. Neighbours were re-notified on the revised plans for a 21 day period commencing 16<sup>th</sup> February 2021.

29 responses have been received raising the following issues:

<b>Issue</b>	<b>Response</b>
Inadequate parking	See paragraph 6.31
No need for the development	See paragraph 6.2
Noise & disturbance	See paragraph 6.23 – 6.28
Inconvenience during construction	See paragraph 6.28
Out of character with surrounding area	See paragraph 6.4 – 6.17
Increase in traffic and congestion	See paragraph 6.30 – 6.32
Overdevelopment	See paragraph 6.4 – 6.17
Hazard to highway safety	See paragraph 6.30 – 6.32
Overlooking and loss of privacy	See paragraph 6.25
Overbearing relationship	See paragraph 6.25
Crime fears	See paragraph 6.46
Health fears	See paragraph 6.25
Poor design	See paragraph 6.6 – 6.17
Loss of/harm to trees	See paragraph 6.37 – 6.42
Harm to wildlife habitat	See paragraph 6.47

Drainage/sewage capacity	See paragraph 6.46
Flooding	See paragraph 6.46
Harm to Conservation Area	The site is not located within a conservation area
Harm to Green Belt/countryside –	The site is not located within the green belt / countryside
Loss of buildings	The buildings are not designated and there is no in-principle objection to their loss
Property devaluation	This is not a material planning consideration
Covenant conflict	This is not a material planning consideration
Loss of private view	This is not a material planning consideration

## **1.0 Site and Character Appraisal**

- 1.1 The application site is located on the northern side of West Drive and comprises no 10 West Drive and the rear gardens of 9, 11 and 12 West Drive located on the northern side of the road. West Drive is a relatively narrow residential cul-de-sac, which leads off the west side of Reigate Road (A240). The area is characterised by detached bungalows, a number extended to the rear, set in good sized plots with extensive rear gardens. The bungalows are set back from the road on a uniform building line, with a green verge with street trees and set back pavement, all giving the street a verdant and spacious feel. Land levels fall away from West Drive towards dwellings in Great Tattenhams to the north. The majority of the application site sits at a lower level than the dwellings on West Drive.
- 1.2 Gardens to the rear are mature with many large amenity trees and significant tree cover to the rear of the site. The trees at the back of the site are protected by an area tree protection order RE701. The low form of development, visual separation between the bungalows and predominance of mature trees seen behind the bungalows and between dwellings, coupled with the space and predominance of landscaping to the front all contribute to the sylvan character of the area.
- 1.3 The surrounding area is characterised by single storey detached bungalows with some 1.5 storey development where loft space has been converted. This form of development is typical of the surrounding roads including development on Great Tattenhams to the north, the adjoining A240 (west side) and Waterer Gardens to the east. Development in Chapel Way where it abuts West Drive to the west is considered of a different character to the application site where the form of development is much denser with significantly smaller (narrower and shorter) plot sizes and some two storey forms. Burgh Heath adjoins development in West Drive to the south and forms the boundary to the green belt.

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## 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: Pre-application consultation was undertaken on a scheme for 7 units with a mixture of single storey and two storey development. It was noted that the number of units has been reduced following the dismissed appeal together with the scale of development. However, concerns were raised with regards to the provision of two storey development on the site. Given the prevailing character of single storey bungalows on West Drive, with only limited roof space conversions, it is considered that single storey or 1.5 storey development (chalet style development) would only be permitted.
- 2.2 Improvements secured during the course of the application: Revised plans have been submitted which have reduced the scale and height of the proposed dwellings so that the proposed dwellings would be either single storey or chalet style bungalows with rooms in the roof space. The revised plans also show the provision of car barns in place of detached garages.
- 2.3 Further improvements could be secured through the use of conditions.

## 3.0 Relevant Planning and Enforcement History

- |      |            |  |   |
|------|------------|--|---|
| 3.12 | 94/03910/F | 6 -14 West Drive- Erection of 9 dwellings with associated external works   | Refused<br>25.05.1995                                   |
| 3.1  | 17/01261/F | Demolition of dwelling at '10 West Drive', and erection of 9 residential dwellings on rear land afforded to properties 9-12 West Drive, associated landscaping, parking, access onto West Drive, and associated ancillary work | Refused<br>03.08.2017<br>Appeal dismissed<br>20.03.2018 |
- 3.3 Application ref: 17/01261/F for the demolition of dwelling at '10 West Drive', and erection of 9 residential dwellings was refused for the following reason:
- The proposal, by virtue of the scale, height and dense building forms and the poorly considered layout dominated by excessive areas of hardstanding and car parking, would result in a cramped and visually dominant overdevelopment of the site which would be out of keeping with, and seriously harmful to, the character and appearance of the locality. It would therefore be contrary to policies Ho9, Ho13, Ho14 and Ho16 of the Reigate and Banstead Local Plan 2005, policies CS1 and CS4 of the Reigate and Banstead Core Strategy 2014, the Reigate and Banstead Local Distinctiveness Guide SPD and the provisions of the Framework in relation to "Good Design".*



3.4 As noted above, the subsequent appeal was dismissed. A copy of the appeal decision is attached as an Appendix to this report. The comments made by the Inspector are discussed below.

## 4.0 Proposal and Design Approach

4.1 This is a full application for the demolition of the existing bungalow at 10 West Drive and the erection of 7 single and 1.5 storey detached and semi-detached dwellings on land to the rear of 9-12 West Drive, the provision of a new access onto West Drive, associated landscaping, parking car ports and garages, and associated ancillary work.

4.2 A design and access statement should illustrate the process that has led to the development proposal, and justify the proposal in a structured way, by demonstrating the steps taken to appraise the context of the proposed development. It expects applicants to follow a four-stage design process comprising:  
Assessment;  
Involvement;  
Evaluation; and  
Design.

4.3 Evidence of the applicant’s design approach is set out below:

Assessment	The character of the immediate surrounding area is assessed as being characterised by single storey detached dwellings, some of which have been extended into the roof space. Further afield it is noted that there are many examples of infill and backland forms of development where rear garden areas have been developed.
	Site features meriting retention are listed as trees at the rear of the site, some of which are protected by TPO
Involvement	No community consultation took place.
Evaluation	The other development options considered included a more intense form of development for 9 units which was refused in 2017 and dismissed on appeal.
Design	The applicant’s reasons for choosing the proposal from the available options were influenced by the previous appeal decision and following pre-application advice from the Council.

4.6 Further details of the development are as follows:

Site area	0.47 ha
Existing parking spaces	2

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Proposed parking spaces	16
Parking standard	16
Number of affordable units	0
Net increase in dwellings	6
Existing site density	2 dpha
Proposed site density	15 dpha
Density of the surrounding area	10 dpha

## 5.0 Policy Context

### 5.1 Designation

Urban area  
TPO RE701

### 5.2 Reigate and Banstead Core Strategy

CS1(Sustainable Development)  
CS10 (Sustainable Development),  
CS11 (Sustainable Construction),  
CS14 (Housing Needs)  
CS17 (Travel Options and accessibility)

### 5.3 Reigate and Banstead Development Management Plan 2019

DES1 (Design of New development)  
DES2 (Residential garden land development)  
DES4: Housing Mix  
DES5: Delivering high quality homes  
DES8: Construction management  
TAP1: Access and Parking  
NHE3: Protecting trees, woodland areas and natural habitats  
CCF1: Climate Change  
INF3: Electronic communication networks

### 5.4 Other Material Considerations

National Planning Policy Framework  
National Planning Practice Guidance  
Supplementary Planning Guidance      Surrey Design  
Local Distinctiveness Design Guide  
Vehicle and Cycle Parking  
Guidance 2018  
Other      Human Rights Act 1998

## Community Infrastructure Levy Regulations 2010

### 6.0 Assessment

- 6.1 The application site is within the urban area where there is a presumption in favour of sustainable development and where the principle of residential development is acceptable.
- 6.2 There is no objection in principle to a potential redevelopment of the site and such a redevelopment would help the Council meet some of the Borough's identified housing need and furthermore would be welcomed as a contribution to housing supply. However, the principle of acceptability in this case rests upon considering the impact of the proposal and resultant harm and the need to provide additional housing, and its resultant benefit. The following report sets out the key considerations.
- 6.3 The main issues to consider are:
- Design appraisal
  - Housing Mix and Standard of Accommodation
  - Neighbour amenity
  - Highway matters
  - Trees and Landscaping
  - Energy, Sustainability and Broadband
  - Community Infrastructure Levy

#### Design appraisal

- 6.4 DMP Policy DES1 relates to the Design of New Development and requires new development to be of a high quality design that makes a positive contribution to the character and appearance of its surroundings. New development should promote and reinforce local distinctiveness and should respect the character of the surrounding area. The policy states that new development will be expected to use high quality materials, landscaping and building detailing and have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site.
- 6.5 At the previous appeal, the Inspector referred to Policy CS4 of the Core Strategy and paragraph 60 (NPPF 2012) which state that it is proper to promote or reinforce local distinctiveness. He considered that the previous proposal failed to do this by proposing an urban form of development which would be similar in appearance to a small estate rather than a discreet individual development that would not amount to a subservient form of development in this location and would fail to promote or reinforce the distinctive characteristics of this established residential area.

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- 6.6 The Inspector noted that whilst the design of the proposed dwellings would be acceptable, the scale and two storey form of the proposed dwellings would nevertheless be substantial in this location and the overall plot size of the dwellings would be very different to those typically found in the area. He found that there would be limited opportunity to soften the impact of development through a comprehensive landscaping scheme. He stated that although they would be set back and stepped down, the proposed dwellings, by virtue of their scale and massing would appear visually cramped, compromise the sense of space and openness in the area and interrupt the established pattern of development in the area.
- 6.7 In order to address these concerns, the current proposal is for 7 units rather than 9, and the scale of the proposed dwellings has been reduced, both in terms of the footprint and floor area of the proposed units and in terms of their maximum height. In the previous application and dismissed appeal, the proposed dwellings varied in height from 8.7m (plots 7 to 9) to 8.9m (plots 1 to 6).
- 6.8 In the current application Plot 1, proposed as a chalet style bungalow with rooms in the roof would have a maximum height of 6.6m to the top of the front gable and a height to the main ridge height of 5.8m. Plots 2 and 3 which are proposed as a pair of semi-detached chalet style bungalows would have a maximum height of 5.8m. Plot 7 would be a traditional detached bungalow with a maximum height 6.5m. The existing dwelling at 10 West Drive has a maximum height of 5.4m.
- 6.9 As a result of the reduction in the number of units proposed and, in the scale and height of the proposed dwellings, the revised proposals appear more sympathetic to the character of the area and would be reflective of the scale and design of existing dwellings in West Drive. The location of the proposed dwellings on the lower site levels to the rear also reduces their impact when viewed from West Drive. The additional space between the proposed dwellings would provide more space whilst the revised layout also reduces the overall amount of hard surfacing on the site providing further opportunities for landscaping.
- 6.10 Since the appeal decision, the Council has adopted the Development Management Plan. Policy DES2 relates to Residential Garden Land development. It states as follows:

*Development of residential garden land, including infilling schemes and development on back garden land, will be required to comply with the following criteria: Proposals must:*

*a. be designed to respect the scale, form and external materials of existing buildings  
in the locality to reinforce local distinctiveness*

*b. be of a height, bulk, mass, and siting to ensure the development is in keeping with the existing street scene*

- 6.11 The current application has proposed a number of changes, as noted above, in order to address the concerns raised by the Inspector in the previous appeal. The number of units has been reduced together with the scale of development. The application proposes single storey or 1.5 storey development (chalet style development) which would be compatible with the form and scale of development in West Drive, which is characterised by bungalows, some of which have been extended into their roof spaces.

*c. for infilling, incorporate plot widths, front garden depths, building orientation and spacing between buildings in keeping with the prevailing layout in the locality*

- 6.12 The proposed development, although not an infill proposal, would nevertheless incorporate plot widths similar to others in the area and would maintain similar spacing to frontage development in West Drive. Given that the dwellings would be sited in a back garden location, the orientation of dwellings would be considered acceptable with good separation between proposed and existing properties.

*d. provide well-designed access roads, with space for suitable landscaping and maintain separation to neighbouring properties*

- 6.13 The proposed access road has been designed as such to allow for a suitable landscape mitigation scheme to soften the impact of the proposed development and allow suitable separation to adjoining neighbours. This aspect of the proposed development was not objected to by the previous Inspector and is considered acceptable.

*e. retain mature trees and hedges, and other significant existing landscape features, and include grass verges and street planting that supports wildlife and maintains green corridors*

- 6.14 The impact of the proposals on protected trees on the site is discussed in more detail below. With regards to landscaping, the space alongside the proposed access road and in front of the proposed dwellings would allow for a suitable scheme of landscaping to be implemented and which would soften the impact of the proposed development and provide a landscaped setting.

*f. demonstrate they have been carefully designed to ensure a good standard of amenity for all existing and future occupants; and*

- 6.15 The revised layout shows good separation to existing dwellings fronting onto West Drive. The previous application proposals were considered to have an acceptable impact on the amenities of neighbouring properties. Given the reduced scale and quantum of development in the revised scheme, it is not considered that the current proposals would cause harm to the amenities of neighbouring properties.

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*g. does not create an undue disruption to the character and appearance of an existing street frontage, particularly where the form and rhythm of development within the existing street frontage is uniform.*

- 6.16 The proposals would involve the demolition of a single property in order to facilitate the access into the site. The proposed dwellings would be located at the rear of nos. 9-12 and there would only be limited visibility from West Drive of the proposed development. As a result, it is not considered that this form of development would lead to undue disruption to the character and appearance of the area. It is also acknowledged that there are other similar backland type developments in the area.
- 6.17 In light of these comments, it is considered that the current proposals address the concerns raised by the Inspector who dealt with the previous appeal and accord with Development Management Plan policies DES1 and DES2.

## Housing Mix and Standard of Accommodation

- 6.18 Policy DES4 requires that on sites of up to 20 homes, at least 20% of the housing should be provided as smaller (1 and 2) bed homes, unless it can be demonstrated that it is not financially viable or technically feasible to do so, that there would be no need or market demand for a particular size of homes (as may be the case for certain types of specialist accommodation), or that doing so would have an adverse impact on the character of the surrounding area.
- 6.19 The application proposes a total of 7 dwellings, two of which (plots 2 and 3) would be provided with two bedrooms at first floor level and a third room at ground floor level which is labelled as a study/bedroom 3. The other 5 dwellings would provide 3 bedrooms at first floor level in each property. The area round the site is characterised by a mixture of 3 and 4 bedroom detached dwellings and it is considered that the mix proposed would be acceptable and would accord with the requirements of the policy.
- 6.20 Policy DES5 requires that all new residential development must provide high quality adaptable accommodation and provide good living conditions for future occupants. New accommodation must meet the relevant nationally prescribed internal space standard for each individual unit unless the council considers that an exception should be made. Sufficient space must be included for storage, clothes drying and the provision of waste and recycling bins in the home. Adequate outdoor amenity space including balconies and terraces and /or communal outdoor space should be provided.
- 6.21 Each dwelling would have a floor area which accords with the relevant standard in the Nationally Described Space Standards and each dwelling would also be provided with appropriate private amenity areas. The orientation of the proposed dwellings would also allow the main habitable rooms to be penetrated by adequate levels of sunlight and daylight.

- 6.22 In light of these comments it is considered that the proposed development would provide an appropriate mix of housing and provide a high standard of accommodation in accordance with DMP policies DES4 and DES5.

### Neighbour amenity

- 6.23 In addition to the comments noted above DMP Policy DES1 also requires new development to provide an appropriate environment for future occupants whilst not adversely impacting upon the amenity of occupants of existing nearby buildings, including by way of overbearing, obtrusiveness, overshadowing, overlooking and loss of privacy.
- 6.24 The proposed development has been assessed with regards to its impact on the amenity of neighbouring properties. Local concerns have been expressed about the impact of the proposal on local residential amenities, as regards overdevelopment, overbearing effect and impacts with respect to overshadowing, overlooking and loss of privacy.
- 6.25 Given the separation distances between the new houses and existing development on West Drive (over 20m), it is not considered that the proposed buildings would cause such a level of harm to neighbour amenity in terms of overbearing, overlooking, loss of light or loss of outlook so as to warrant refusal.
- 6.26 Due to the design, siting and aspect between the proposed dwellings and neighbouring properties, no direct loss of privacy, overshadowing or loss of light would occur.
- 6.27 The separation and privacy within the site between dwellings whilst tight in some respects, is not considered to form a reason for refusal in this case. The development proposes a new access road and the additional movements generated are not considered likely to cause undue noise and disturbance or unreasonable harm with respect to car headlights.
- 6.28 Objection has been raised on the grounds of inconvenience during the construction period. Whilst it is acknowledged there may be a degree of disruption during the construction phase, the proposal would not warrant refusal on this basis and statutory nuisance legislation exists to control any significant disturbance caused during the construction of the proposal. A construction method statement would be secured by planning condition.
- 6.29 While giving rise to a degree of change in the relationship between buildings, the proposed scheme would not unacceptably affect the amenity of neighbouring properties and would comply with policy DES1.

### Highway matters

- 6.30 Policy TAP1 of the Development Management Plan 2019 requires new development to demonstrate that it would not adversely affect highways safety or the free flow of traffic, that it would provide sufficient off-street

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parking in accordance with published standards and that it would constitute development in a sustainable location.

- 6.31 The developer has provided turning overlays that show drivers of transit and grocery delivery vans can enter and leave the site in forward gear without having to drive to the other side of the road where cars could potentially be parked. In the event that cars are parked on the site side of West Drive, this may affect the ability of transit and grocery delivery plans from safely entering and leaving the site as shown in the submitted plans numbered 201340/TR/04 and 201340/TR/05 from Lanmor Consulting. As such parking restrictions would be required to prevent this problem from occurring and the County Highway Authority recommend a condition that the access and associated parking restrictions and the first 20 metres of the new access road are provided before commencement of development. This will help in preventing dirt getting to the highway and causing a highway safety problem and parked vehicles preventing vehicles from entering and leaving the site access.
- 6.32 The application site is located in an area with low accessibility to public transport and other services. In such areas, dwellings with two or three bedrooms should be provided with 2 parking spaces and a further 2 visitor spaces should be provided. The submitted drawings show that each dwelling would be provided with 2 parking spaces comprising garages, car ports and open spaces. The proposed garages would be of an appropriate width to qualify as a parking space. This complies with Reigate and Banstead Minimum parking standards for the location of the proposed development.
- 6.33 In terms of refuse collection, the developer has provided a plan numbered 17008 A PL 064 Rev A that shows a refuse vehicle could enter and leave the site in a forward gear but that it would take up the entire width of the carriageway in order to do this. This is likely to cause a highway safety problem in the event of parked cars. The County Highway Authority have therefore recommended a condition that a refuse collection point is provided within 25 metres of the highway. The refuse collection point shown in the plan 7008 A PL 064 Rev A is located within 25 metres of the highway. Serving the site from the highway would be no different to existing arrangements and would therefore be acceptable for the proposed development too.
- 6.34 The Council's Neighbourhood Services team have been consulted on this part of the proposals and consider that West Drive is already a difficult road to access, and it is considered that a properly constructed turning head access into the site would be advantageous. In order to support this, the applicants have submitted a tracking drawing which shows how a 26 tonne RCV can access a refuse collection point no more than 9m from the existing highway along the proposed access drive. Neighbourhood Services also state that all new developments should be planned to comply with the Health & Safety requirements and that reversing manoeuvres are usually not acceptable and must be kept to a minimum in line with Health & Safety Executive directives.



- 6.35 The tracking drawing demonstrates that a refuse collection vehicle could access the site, utilise the site entrance as a turning head and collect the refuse from a collection point alongside the site access.
- 6.36 The Neighbourhood Services team have confirmed that these arrangements would be acceptable and have also asked that the edges of the bellmouth access be strengthened to give added protection should a refuse collection vehicle run over them. A condition is recommended to require further details of the refuse collection point be submitted.

### Trees and Landscaping

- 6.37 Policy NHE3 advises that unprotected but important trees, woodland and hedgerows with ecological or amenity value should be retained as an integral part of the development.
- 6.38 The tree officer was consulted on the proposal in order to assess the proposed development against impact upon existing trees and vegetation.
- 6.39 A detailed arboricultural report has been submitted in support of the application. The tree officer notes that the additional arboricultural report by DAA, dated February 2021, demonstrates the gardens will not be heavily shaded by the trees and therefore his original concerns on this issue have been addressed. He also notes that future applications to undertake tree work will be assessed on a case by case basis and this will ensure trees of value will be retained protecting the local canopy cover.
- 6.40 The arboricultural information provided to date does not identify underground services or whether there will changes in levels, therefore it will be necessary for a finalised tree protection condition to be attached to the decision notice.
- 6.41 With regards to the proposed landscape scheme, this shows that a mixture of trees and shrubs will be planted, which overtime will enhance the development.
- 6.42 Therefore, based on the information submitted the tree officer is able to support this application.

### Energy, Sustainability and Broadband

- 6.43 In accordance with adopted policy, conditions are imposed to seek the installation of carbon reduction measures within the dwellings hereby permitted to secure energy savings through the use of renewable technologies where appropriate and the provision of fast broadband services for future residents to ensure that the dwellings are future proofed.

### Withdrawal of Permitted Development Rights

- 6.44 Given the relatively small plot sizes for the proposed dwellings it is also recommended that permitted development rights be withdrawn for the

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proposed dwellings so that the Council is able to retain control of the size and design of further extensions at ground floor level and within the roof space.

## Community Infrastructure Levy (CIL)

- 6.45 The Community Infrastructure Levy (CIL) is a fixed charge which the Council will be collecting from some new developments from 1 April 2016. It will raise money to help pay for a wide range of infrastructure including schools, road, public transport and community facilities which are needed to support new development. This development would be CIL liable and the exact amount would be determined and collected after the grant of planning permission.

## Other Issues

- 6.46 Objections have been received due to the loss of property value, loss of private views and conflict with covenants, but these are not material planning considerations. Concern has been raised from neighbouring properties regarding fear of crime, flooding and drainage/sewage. The proposal would result in the redevelopment of a rear garden area, new boundary treatment is proposed, and the development is not considered to cause crime issues. The site is not located within a flood zone and sewage capacity would be assessed at building control stage. The proposal is considered to have a satisfactory impact with regards flooding and drainage/sewerage capacity. It is noted a condition could be applied to a grant of permission to ensure that sustainable drainage is present on the site and an appropriate surface water drainage scheme implemented.
- 6.47 Concern has been raised regarding the potential for harm to wildlife by neighbouring residents. Whilst the proposal would result in the redevelopment of rear gardens, it is not considered likely to result in significant impact on existing wildlife habitats for protected species and may provide opportunities to incorporate features into the design which are beneficial. Measures to enhance biodiversity within the site could be designed in to the development in accordance with para 118 of the NPPF and secured by a planning condition. No issues were previously identified in this regard by the previous appeal inspector and bats and their roosts are protected by law and the protected species legislation applies independently of planning permission.

## **CONDITIONS**

1. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004.

2. The development hereby permitted shall be carried out in accordance with the following approved plans:

Plan Type	Reference	Version	Date
Site Location Plan	17008-A-PL-050	A	15/02/2021
Proposed site layout	17008-A-PL-051	E	23/03/2021
Proposed site layout	17008-A-PL-052	E	23/03/2021
Proposed block plan	17008-A-PL-053	E	23/03/2021
Combined Plan	17008-A-PL-054	D	15/03/2021
Combined Plan	17008-A-PL-055	D	15/03/2021
Combined Plan	17008-A-PL-056	B	15/02/2021
Combined Plan	17008-A-PL-057	C	15/03/2021
Combined Plan	17008-A-PL-058	A	15/02/2021
Combined Plan	17008-A-PL-059	A	15/02/2021
Proposed sections	17008-A-PL-060	B	15/02/2021
Proposed sections	17008-A-PL-061	E	23/03/2021
Proposed sections	17008-A-PL-062	B	15/02/2021
Proposed sections	17008-A-PL-063	C	11/03/2021
Unilateral Undertaking plan	17008-A-PL-065	E	23/03/2021
Proposed site layout	17008-A-PL-066	D	23/03/2021
Existing combined plan	17008-A-PL-067	A	15/02/2021
Tracking plan	17008-A-PL-068	A	15/02/2021
Proposed site layout	17008-A-PL-069	E	23/03/2021
Tracking plan	17008-A-PL-071	B	23/03/2021
Arboricultural Plan	TCP01		15/02/2021

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

- No development shall take place until the developer obtains the Local Planning Authority's written approval of details of both existing and proposed ground levels and the proposed finished ground floor levels of the buildings. The development shall be carried out in accordance with the approved levels.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

- No development shall take place above slab level until written details of the materials to be used in the construction of the external surfaces, including fenestration and roof, have been submitted to and approved in writing by the Local Planning Authority, and on development shall be carried out in accordance with the approved details.

Reason: To ensure the Local Planning Authority are satisfied with the details of the proposal and its relationship with adjoining development and to

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safeguard the visual amenities of the locality with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

5. No development shall commence including demolition and or groundworks preparation until a detailed, scaled Tree Protection Plan (TPP) and the related Arboricultural Method Statement (AMS) is submitted to and approved in writing by the Local Planning Authority (LPA). These shall include details of the specification and location of exclusion fencing, ground protection and any construction activity that may take place within the Root Protection Areas (RPA) of trees shown to scale on the TPP, including the installation of service routings type of surfacing for the entrance drive and location of site offices. The AMS shall also include a pre-commencement meeting, supervisory regime for their implementation and monitoring with an agreed reporting process to the LPA. All works shall be carried out in strict accordance with these details when approved.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies DES1 and NHE3 of the Reigate and Banstead Development Management Plan 2019.

6. No development, groundworks or demolition processes shall be undertaken until an agreed scheme of supervision for the arboricultural protection measures have been submitted to and approved in writing by the local planning authority. The pre commencement meeting, supervision and monitoring shall be undertaken in accordance with these approved details. The submitted details shall include.
  1. Pre commencement meeting between the retained arboricultural consultant, local planning authority Tree Officer and individuals and personnel responsible for the implementation of the approved development
  2. Timings, frequency of the supervision and monitoring regime and an agreed reporting process to the local planning authority.
  3. The supervision monitoring and reporting process shall be undertaken by a qualified arboriculturist.

Reason: To ensure good arboricultural practice in the interests of the maintenance of the character and appearance of the area and to comply with British Standard 5837:2012 'Trees in Relation to Design, demolition and Construction – Recommendations' and policies DES1 and NHE3 of the Reigate and Banstead Development Management Plan 2019.

7. All hard and soft landscaping work shall be completed in full accordance with the approved scheme, prior to occupation or within the first planting season following completion of the development hereby approved.

Any trees shrubs or plants planted in accordance the approved scheme which are removed, die or become damaged or diseased within five years of

planting shall be replaced within the one year by trees, shrubs of the same size and species in the same location.

Reason: To ensure good landscape practice in the interests of the maintenance of the character and appearance of the area and to comply with policies DES1 and NHE3 of the Reigate and Banstead Development Management Plan 2019 and the recommendations within British Standard 5837:2012 Trees in relation to design, demolition and construction

8. Notwithstanding the submitted plan titled Sight Lines Plan numbered 17008 A PL 066 Rev C the development shall not be commenced until the proposed bellmouth vehicular access to West Drive and the first 20 metres of the new access road have been provided with tactile paving at the pedestrian crossing points of the access and parking restrictions along the both sides of West Drive for a distance of 43 metres in both directions from the access in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

9. No development shall commence until a scheme for the disposal of foul and surface water drainage from the site has been submitted to and approved by the Local Planning Authority. The scheme shall be implemented as approved prior to the occupation of the dwelling hereby permitted.

Reason: To ensure that the site is satisfactorily drained with regard to Development Management Plan policy CCF2 and National Planning Policy Framework 2019.

10. The development hereby approved shall not be commenced unless and until a pedestrian inter-visibility splay measuring 2m by 2m has been provided on each side of the access to West Drive, the depth measured from the back of the footway and the widths outwards from the edges of the access in accordance with a scheme to be submitted to and approved in writing with the Local Planning Authority. No obstruction to visibility between 0.6m and 2m in height above ground level shall be erected within the area of such splays.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

11. The development hereby approved shall not be first occupied unless and until space has been laid out within the site in accordance with approved plan

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titled Proposed Site Layout and numbered 17008 A PL052 Rev C for vehicles to be parked and for vehicles to turn so that they may enter and leave the site in forward gear. Thereafter the parking and turning areas shall be retained and maintained for their designated purposes.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

12. No development shall commence until a Construction Transport Management Plan, to include details of:
- (a) parking for vehicles of site personnel, operatives and visitors
  - (b) loading and unloading of plant and materials
  - (c) storage of plant and materials
  - (d) programme of works (including measures for traffic management)
  - (e) provision of boundary hoarding behind any visibility zones
  - (g) vehicle routing
  - (h) measures to prevent the deposit of materials on the highway
  - (i) before and after construction condition surveys of the highway and a commitment to fund the repair of any damage caused
  - (k) on-site turning for construction vehicles
  - (i) hours of construction

has been submitted to and approved in writing by the Local Planning Authority. Only the approved details shall be implemented during the construction of the development.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and DES8 Construction Management of the Reigate and Banstead Local Plan Development Management Plan September 2019.

13. The development hereby approved shall not be occupied unless and until each of the proposed dwellings are provided with a fast charge socket (current minimum requirements - 7 kw Mode 3 with Type 2 connector - 230v AC 32 Amp single phase dedicated supply) in accordance with a scheme to be submitted and approved in writing by the Local Planning Authority and thereafter retained and maintained to the satisfaction of the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Reigate and Banstead Core Strategy 2014 Policy CS17 (Travel Options and Accessibility).

14. Notwithstanding the submitted plan titled Proposed Site Layout dated October 2020 and numbered 17008 A PL052 Rev C the development shall not be commenced until the north eastern access road footway has been extended to the south east boundary of plot 13 in accordance with a revised scheme to be submitted to and approved in writing with the Local Planning Authority.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

15. The development shall not be occupied until a refuse collection area has been provided in accordance with the approved submitted plan 17008 A PL 071 Rev A, all to be permanently retained and maintained.

Reason: The condition above is required in order that the development should not prejudice highway safety, nor cause inconvenience to other highway users, and to accord with the National Planning Policy Framework 2019 and Policy TAP1 Parking, access, and Servicing of the Reigate and Banstead Local Plan Development Management Plan September 2019.

16. The development hereby approved shall not be occupied until a plan for the management of the bin presentation point area has been submitted to and approved in writing by the Local Planning Authority. The plan be informed by the Council's Making Space for Waste in New Developments, including requirements for the bins to be positioned there only on bin collection day. Upon occupation the plan shall be implemented, and the bin presentation point maintained in accordance with the approved details.

Reason: To provide adequate waste facilities in the interests of the amenities of the area in accordance with Development Management Plan 2019 policy DES1.

17. The development shall not be occupied until a plan indicating the positions, design, materials and type of boundary treatment to be erected has been submitted to and approved in writing by the Local Planning Authority. The boundary treatment shall be completed before the occupation of the development hereby permitted.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the policy DES1 of the Reigate and Banstead Development Management Plan 2019.

18. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015, (or any Order revoking and re-enacting that Order with or without modification), no first floor windows, dormer windows or rooflights other than those expressly authorised by this permission shall be constructed.

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Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the policy DES1 of the Reigate and Banstead Development Management Plan 2019.

19. Notwithstanding the provisions of the Town and Country Planning (General Permitted Development) Order 2015 (or any order revoking and re-enacting that Order with or without modification), no extensions permitted by Classes A B and C of Part 1 of the Second Schedule of the 2015 Order shall be constructed.

Reason: To preserve the visual amenity of the area and protect neighbouring residential amenities with regard to the policy DES1 of the Reigate and Banstead Development Management Plan 2019.

20. The development hereby approved shall not be first occupied unless and until an Energy and Water Efficiency Statement has been submitted to and approved in writing by the Local Planning Authority. The Statement shall detail how the development will:
- a) Ensure that the potential water consumption by occupants of each new dwelling does not exceed 110 litres per person per day
  - b) Achieve not less than a 19% improvement in the Dwelling Emission Rate (DER) over the Target Emission Rate (TER) as defined in Part L1A of the 2013 Building Regulations

The development shall be carried out in accordance with the approved details and any measures specific to an individual dwelling(s) shall be implemented, installed and operational prior to its occupation.

Reason: To ensure that the development supports the efficient use of resources and minimises carbon emissions with regard to Policy CS10 of the Reigate & Banstead Core Strategy 2014 and Policy CCF1 of the Reigate & Banstead Development Management Plan 2019.

21. All dwellings within the development hereby approved shall be provided with the necessary infrastructure to facilitate connection to a high speed broadband. Unless otherwise agreed in writing with the Local Planning Authority, this shall include as a minimum:
- a) A broadband connection accessed directly from the nearest exchange or cabinet
  - b) Cabling and associated installations which enable easy access for future repair, replacement or upgrading.

Reason: To ensure that the development promotes access to, and the expansion of, a high quality electronic communications network in accordance with Policy INF3 of the Reigate & Banstead Development Management Plan 2019.

22. The development hereby permitted shall not be occupied until a scheme for the provision of measures to improve the bio-diversity interest of the site are



submitted to the Council and approved in writing. The approved measures shall be implemented in full and maintained thereafter.

Reason: To ensure that the development would not harm wildlife or protected species and deliver a biodiversity enhancement in accordance with Policy NHE2 of the Development Management Plan, Natural England standing advice and the provisions of the NPPF.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.org.uk](http://www.firesprinklers.org.uk).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).
3. The applicant is advised that prior to the initial occupation of any individual dwelling hereby permitted, to contact the Council's Neighbourhood Services team to confirm the number and specification of recycling and refuse bins that are required to be supplied by the developer. The Council's Neighbourhood Services team can be contacted on 01737 276292 or via the Council's website at [http://www.reigate-banstead.gov.uk/info/20085/planning\\_applications/147/recycling\\_and\\_waste\\_developers\\_guidance](http://www.reigate-banstead.gov.uk/info/20085/planning_applications/147/recycling_and_waste_developers_guidance)
4. You are advised that the Council will expect the following measures to be taken during any building operations to control noise, pollution and parking:
  - (a) Work that is audible beyond the site boundary should only be carried out between 08:00hrs to 18:00hrs Monday to Friday, 08:00hrs to 13:00hrs Saturday and not at all on Sundays or any Public and/or Bank Holidays;
  - (b) The quietest available items of plant and machinery should be used on site. Where permanently sited equipment such as generators are necessary, they should be enclosed to reduce noise levels;
  - (c) Deliveries should only be received within the hours detailed in (a) above;
  - (d) Adequate steps should be taken to prevent dust-causing nuisance beyond the site boundary. Such uses include the use of hoses to damp down stockpiles of materials, which are likely to generate airborne dust, to damp down during stone/slab cutting; and the use of bowsers and wheel washes;
  - (e) There should be no burning on site;
  - (f) Only minimal security lighting should be used outside the hours stated above; and
  - (g) Building materials and machinery should not be stored on the highway and contractors' vehicles should be parked with care so as not to cause an obstruction or block visibility on the highway.

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Further details of these noise and pollution measures can be obtained from the Council's Environmental Health Services Unit.

In order to meet these requirements and to promote good neighbourliness, the Council recommends that this site is registered with the Considerate Constructors Scheme - [www.ccscheme.org.uk/index.php/site-registration](http://www.ccscheme.org.uk/index.php/site-registration).

5. The applicant is advised that the essential requirements for an acceptable communication plan forming part of a Method of Construction Statement are viewed as: (i) how those likely to be affected by the site's activities are identified and how they will be informed about the project, site activities and programme; (ii) how neighbours will be notified prior to any noisy/disruptive work or of any significant changes to site activity that may affect them; (iii) the arrangements that will be in place to ensure a reasonable telephone response during working hours; (iv) the name and contact details of the site manager who will be able to deal with complaints; and (v) how those who are interested in or affected will be routinely advised regarding the progress of the work. Registration and operation of the site to the standards set by the Considerate Constructors Scheme (<http://www.ccscheme.org.uk/>) would help fulfil these requirements.
6. The applicant is advised that the Borough Council is the street naming and numbering authority and you will need to apply for addresses. This can be done by contacting the Address and Gazetteer Officer prior to construction commencing. You will need to complete the relevant application form and upload supporting documents such as site and floor layout plans in order that official street naming and numbering can be allocated as appropriate. If no application is received the Council has the authority to allocate an address. This also applies to replacement dwellings. If you are building a scheme of more than 5 units please also supply a CAD file (back saved to 2010) of the development based on OS Grid References. Full details of how to apply for addresses can be found [http://www.reigatebanstead.gov.uk/info/20277/street\\_naming\\_and\\_numbering](http://www.reigatebanstead.gov.uk/info/20277/street_naming_and_numbering)
7. Notwithstanding any permission granted under the Planning Acts, no signs, devices or other apparatus may be erected within the limits of the highway without the express approval of the Highway Authority. It is not the policy of the Highway Authority to approve the erection of signs or other devices of a non-statutory nature within the limits of the highway.
8. The permission hereby granted shall not be construed as authority to obstruct the public highway by the erection of scaffolding, hoarding or any other device or apparatus for which a licence must be sought from the Highway Authority Local Highways Service.
9. The permission hereby granted shall not be construed as authority to carry out any works on the highway or any works that may affect a drainage channel/culvert or water course. The applicant is advised that a permit and, potentially, a Section 278 agreement must be obtained from the Highway Authority before any works are carried out on any footway, footpath,

carriageway, verge or other land forming part of the highway. All works on the highway will require a permit and an application will need to be submitted to the County Council's Street Works Team up to 3 months in advance of the intended start date, depending on the scale of the works proposed and the classification of the road. Please see <http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/the-traffic-management-permit-scheme>.

The applicant is also advised that Consent may be required under Section 23 of the Land Drainage Act 1991. Please see [www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice](http://www.surreycc.gov.uk/people-and-community/emergency-planning-and-community-safety/flooding-advice).

10. The developer is reminded that it is an offence to allow materials to be carried from the site and deposited on or damage the highway from uncleaned wheels or badly loaded vehicles. The Highway Authority will seek, wherever possible, to recover any expenses incurred in clearing, cleaning or repairing highway surfaces and prosecutes persistent offenders. (Highways Act 1980 Sections 131, 148, 149).
11. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
12. The developer is advised that as part of the detailed design of the highway works required by the above condition(s), the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.
13. The Highway Authority has no objection to the proposed development, subject to the above conditions but, if it is the applicant's intention to offer any of the roadworks included in the application for adoption as maintainable highways, permission under the Town and Country Planning Act should not be construed as approval to the highway engineering details necessary for inclusion in an Agreement under Section 38 of the Highways Act 1980. Further details about the post-planning adoption of roads may be obtained from the Transportation Development Planning Division of Surrey County Council.
14. It is the responsibility of the developer to ensure that the electricity supply is sufficient to meet future demands and that any power balancing technology is in place if required. Please refer to: <http://www.beama.org.uk/resourceLibrary/beama-guide-to-electric-vehicle-infrastructure.html> for guidance and further information on charging modes and connector types.
15. The developer is advised that that the parking restrictions required by the above access condition would need to be approved and advertised through

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Surrey County Council and then provided by the developer. The aforementioned is all at the developer's own expense.

16. The use of a suitably qualified landscape/arboricultural consultant is essential to provide acceptable submissions in respect of the arboricultural tree condition above. All works shall comply with the recommendations and guidelines contained within British Standard 5837.

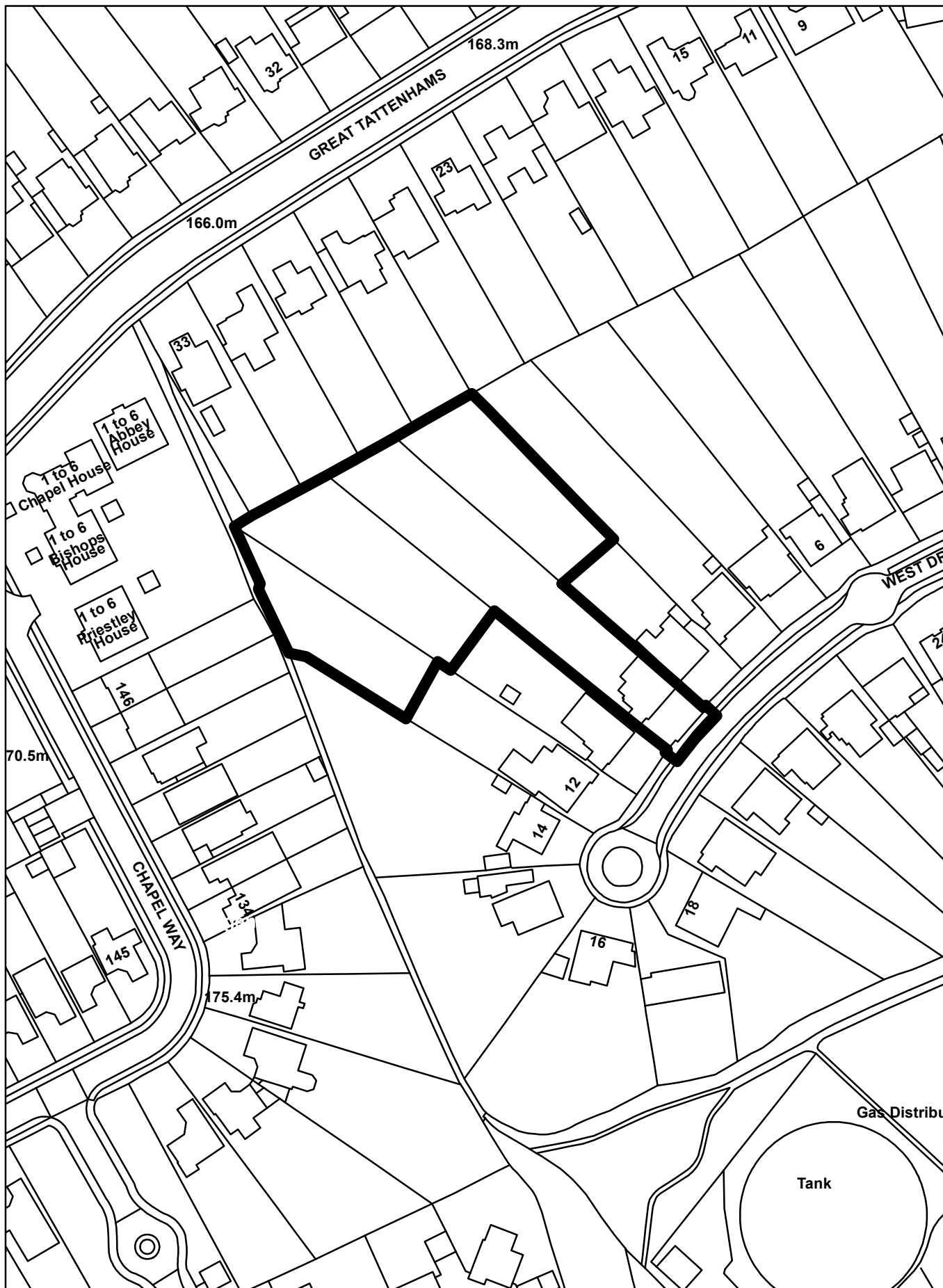
## **REASON FOR PERMISSION**

The development hereby permitted has been assessed against development plan policies CS1, CS10, CS11, CS14, CS17, DES1, DES2, DES4, DES5, DES8, TAP1, NHE3, CCF1, INF3 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

## **Proactive and Positive Statements**

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

20/02510/F - 10 West Drive And Land To The Rear Of  
9,11 And 12 West Drive, Burgh Heath



## Appeal Decision

Site visit made on 20 February 2018

**by David Troy BSc (Hons) MA MRTPI**

**an Inspector appointed by the Secretary of State**

**Decision date: 20 March 2018**

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### **Appeal Ref: APP/L3625/W/17/3184390**

#### **9-12 West Drive, Burgh Heath KT20 5PA**

- The appeal is made under section 78 of the Town and Country Planning Act 1990 against a refusal to grant planning permission.
  - The appeal is made by Devine Homes PLC against the decision of Reigate & Banstead Borough Council.
  - The application Ref 17/01261/F, dated 30 May 2017, was refused by notice dated 3 August 2017.
  - The development proposed is demolition of dwelling at '10 West Drive', and erection of 9 residential dwellings on rear land afforded to properties 9-12 West Drive, associated landscaping, parking, access onto West Drive, and associated ancillary work.
- 

### **Decision**

1. The appeal is dismissed.

### **Main Issue**

2. The main issue is the effect of the proposed development on the character and appearance of the area.

### **Reasons**

3. The appeal site forms part of the rear garden areas of Nos. 9-12 West Drive, a row of detached bungalows set within large spacious plots on a residential cul-de-sac. It is located in a mature well-established residential area that has a mixed suburban character with a range of two storey and single storey properties of varying styles and ages.
4. The properties on West Drive are typically characterised by detached bungalows set back from the road behind small front gardens/driveways and larger spacious rear gardens. To the north, south and west are the garden areas and the outbuildings associated with the adjacent properties. Where outbuildings and other structures exist behind the residential properties, these are very low, small in scale, clearly subsidiary and have little impact upon the sense of space and openness in the area. This gives the area a strong unifying character and appearance, which is further enhanced by the presence of mature landscaping and established trees within the appeal site and surrounding gardens including a group of trees subject to a Tree Preservation Order. The topography of the site slopes away gently from the existing dwellings towards the garden areas and properties at the rear.
5. The proposal would involve the demolition of the existing dwelling at no. 10 West Drive and the erection of 9 no. two storey semi-detached and terraced

- dwelling with associated parking, landscaping and garden areas, which would be served via a new access road off West Drive. The proposed two storey dwelling would be set back from the boundaries of the site and would be stepped down to utilise the natural sloping topography of the site.
6. Whilst visually the design of the proposed dwellings would be acceptable, the scale and two storey form of the proposed dwellings would nevertheless be substantial in this location and the overall plot size of the dwellings would be very different to those typically found in the area. The proposed semi-detached dwellings on Plots 1 to 4 and the end terraced dwelling on Plot 9 would be positioned on the highest part of the site and located in close proximity to the rear boundary of the adjacent properties on West Drive. This would provide limited opportunity to soften the impact of development through a comprehensive landscaping scheme. As such, although they would be set back and stepped down, the proposed dwellings, by virtue of their scale and massing would appear visually cramped, compromise the sense of space and openness in the area and interrupt the established pattern of development in the area.
  7. Policy CS4 of the Reigate and Banstead Local Plan Core Strategy 2014 (CS) and Paragraph 60 of the National Planning Policy Framework (the Framework) state that it is proper to promote or reinforce local distinctiveness. In this case, the siting and layout of the proposed development would result in an urban form of development similar in appearance to a small estate rather than discreet individual development that would not amount to a subservient form of development in this location and would fail to promote or reinforce the distinctive characteristics of this established residential area. In addition, the proposed layout would result in the proposed dwellings on Plots 1 to 5 backing onto the shared open space providing limited natural surveillance to ensure safety and security contrary to the advice in the local design guidance<sup>1</sup>.
  8. The consequential harm would not in my view be sufficiently resolved by the limited effect of the proposed dwellings on the street scene. In any case, the site is viewed from the rear of a number of surrounding properties and the adjacent public rights of way. These shortcomings would be exacerbated by the proposed removal of a number of the established trees within and along the boundaries of the site which make an important contribution to the verdant character and appearance of the area. As such, irrespective of the design approach, I consider the proposed development would adversely harm rather than positively contribute to the character and appearance of the area.
  9. I have considered the appellant's arguments that the site layout and the design of the dwellings have been carefully considered in order to minimise any impacts on adjacent dwellings and the area and would be in keeping with the other properties in the area. Whilst I recognise there is some variation in heights and housing styles in the area and the use of matching materials, fenestrations, landscaping and boundary treatment would assist with integrating the proposal with the area and would reduce overall visual impact of the proposed access, car parking and hardstanding areas within the site, these aspects do not overcome the adverse effects outlined above.
  10. Consequently, I conclude that the proposed development would have a harmful effect on the character and appearance of the area. The development conflicts with saved Policies Ho9, Ho13 and Ho14 of the Reigate and Banstead Borough

<sup>1</sup> Reigate and Banstead Local Distinctiveness Design Guide SPG (March 2004) Page 39



Local Plan 2005, CS Policies CS1 and CS4 and the Reigate and Banstead Local Distinctiveness Design Guide Supplementary Planning Guidance 2004. These policies and guidance seek, amongst other things, to ensure developments are of a high quality design that reflects the existing character of the area and the local distinctiveness. In addition, it would not accord with the aims of the Framework that development should seek to secure a high quality of design (paragraph 17) and to respect the local character (paragraph 58).

## Other Matters

11. I have noted the other developments in the area drawn to my attention by the appellant. However, whilst there are some issues that are similar to the proposed development, all of the examples provided including that at Stanton Lodge and Shelveys Way, Tadworth<sup>2</sup> and Epsom Lane North, Epsom Downs<sup>3</sup>, are located on different streets with different development and locational characteristics to the appeal scheme. In any event, I am required to deal with this proposal on its own merits and such I accord them limited weight as precedents in this case.
12. The Framework places significant emphasis on the delivery of new housing and the Council has not disputed the contribution small sites can make towards new housing in the area. The appellant states that the proposal constitutes a sustainable form of development that would be well connected to existing services and facilities and provide some social and economic benefits through contributing to the supply and mix of housing, making effective and efficient use of the land in an accessible location and contributing to the local economy. The additional dwellings would make a positive, albeit modest contribution to the supply of housing in the area and the vitality and viability of the local services and the economy.
13. However, while I note the appellant's view that the scheme's design and the retention and enhancement of the mature landscaping and trees on the site would amount to environmental benefits, I have found above that taken overall the development would harm the area's character and appearance. This harm would conflict with the environmental dimension of sustainable development and, in my view, would be sufficient to outweigh the scheme's benefits when assessed against the policies in the Framework as a whole. The proposal would not therefore amount to sustainable development in the terms of the Framework.
14. I have noted the objections raised by local residents to the proposal. However, in light of my findings on the main issue of the appeal, my decision does not turn on these matters.

## Conclusion

15. For the reasons given above, and having regard to all other matters raised, I conclude that the appeal should be dismissed.

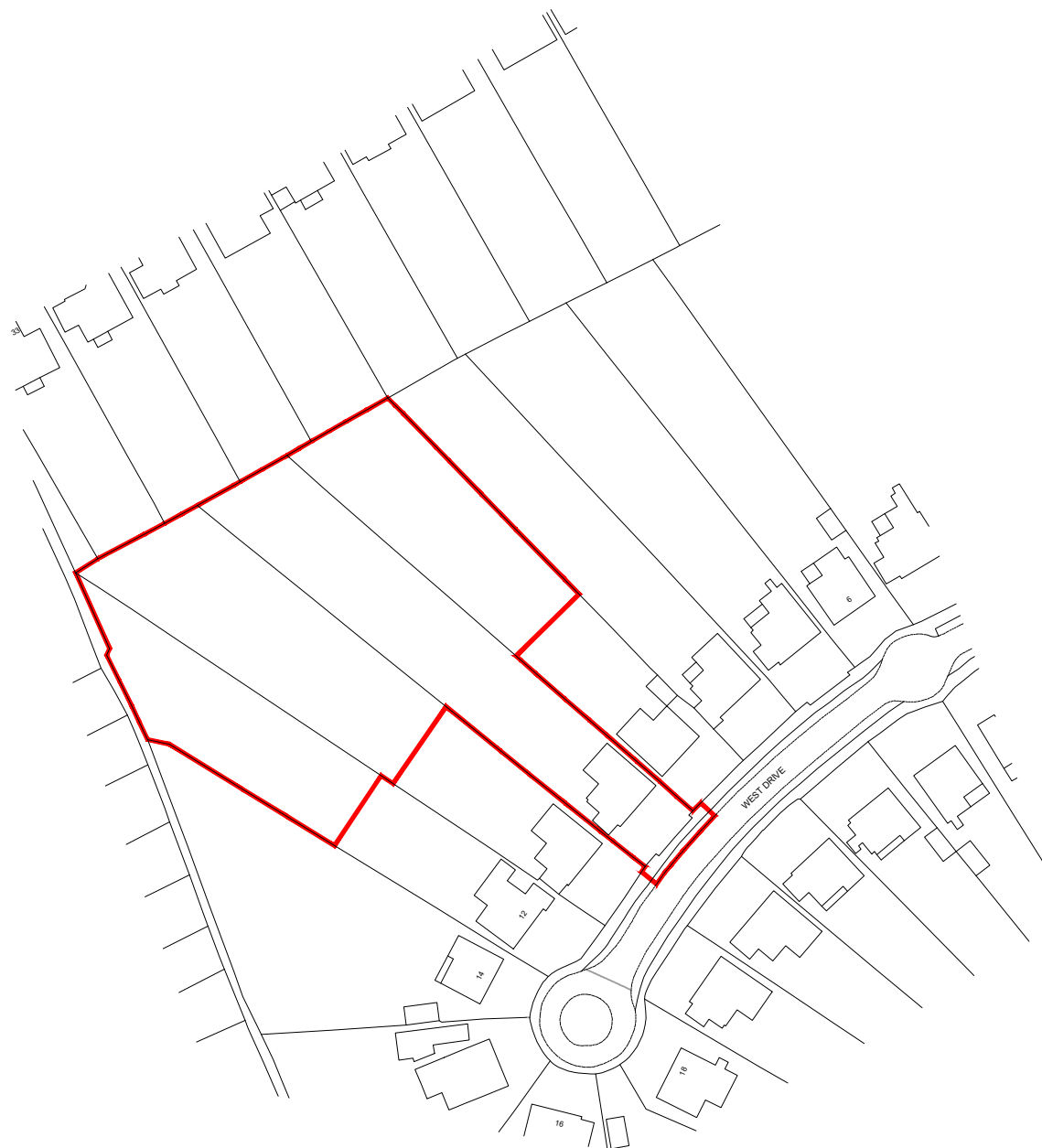
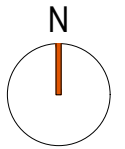
*David Troy*

INSPECTOR

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<sup>2</sup> APP/L3625/W/16/3150102

<sup>3</sup> APP/L3625/W/16/3156890

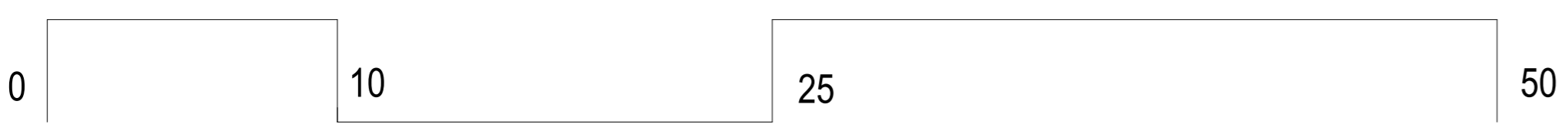
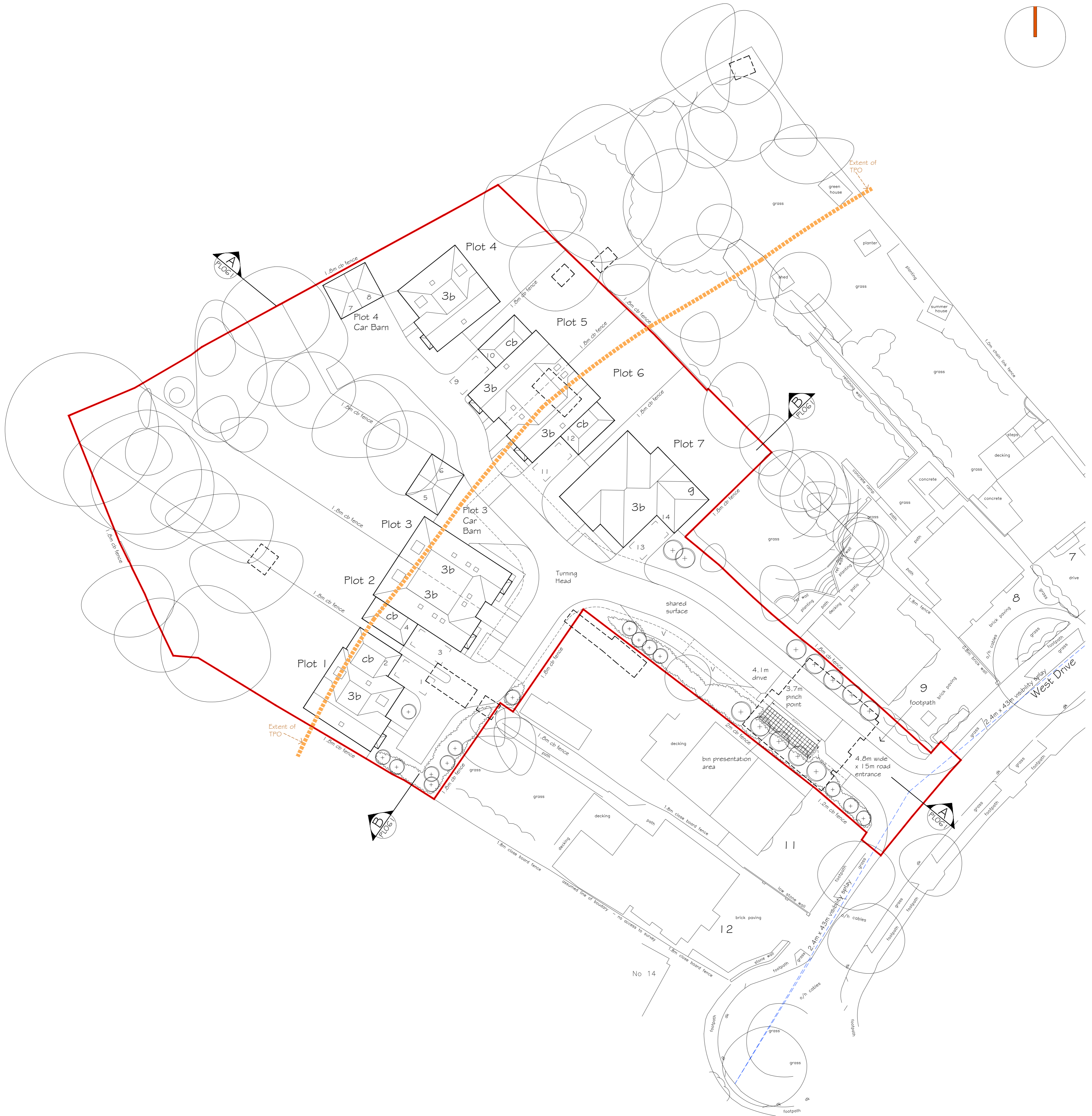
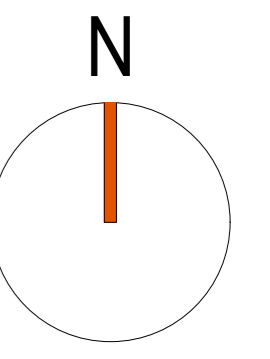


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A	Planning Issue	2020.10.27	AJC	dp																														
P1	Preliminary Issue	2020.10.21	AJC	dp																														
rev	description	date	by	ch.																														
scale:	1:1250 @ A3	client:	DEVINE HOMES PLC																															
date:	October 2020	project:	9 to 12 West Drive, Tadworth. KT20 5PA.																															
drawn:	AJC	drawing:	Site Location Plan																															
checked:	dp	status:	PLANNING APPLICATION																															





ACCOMMODATION SCHEDULE	
Application Site Area = 4666 sq.m / 0.47 hectares / 1.15 acres	
PLOT / TYPE	QUANTITY
Plots 1 & 4 - 3 Bedroom Chalet Bungalows (135.9 sq.m / 1463 sq.ft)	2
Plots 2 & 3 - 3 Bedroom Chalet Bungalows (110.2 sq.m / 1186 sq.ft)	2
Plots 5 & 6 - 3 Bedroom Chalet Bungalows (117.2 sq.m / 1261 sq.ft)	2
Plot 7 - 3 Bedroom Bungalow (115.0 sq.m / 1238 sq.ft)	1
<b>TOTAL</b>	<b>7</b>
Parking	14 spaces

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MODEL FILE REFERENCE- 17008-A-P-PL

rev.	description	date	by	ch
F	Bin presentation area re-located / fence heights with No.11 adjusted / radius to bellmouth to south side amended	2021.03.23	AJC	dp
E	Bin presentation area added/ Plots 1 and 2 garages changed to car barns/ northern footpath extended	2021.03.10	AJC	dp
D	Tree canopies amended/ bin collection point removed	2021.02.11	AJC	dp
C	Plots 2 & 3 roof plan amended	2021.01.20	AJC	dp
B	Plots 1 to 6 roof plans amended	2021.01.12	AJC	dp
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

scale: 1:250 @ A1 client: DEVINE HOMES PLC

date: October 2020 project: Land at 9 to 12 West Drive, Banstead. KT20 5PA.

drawn: AJC drawing: Proposed Site Layout

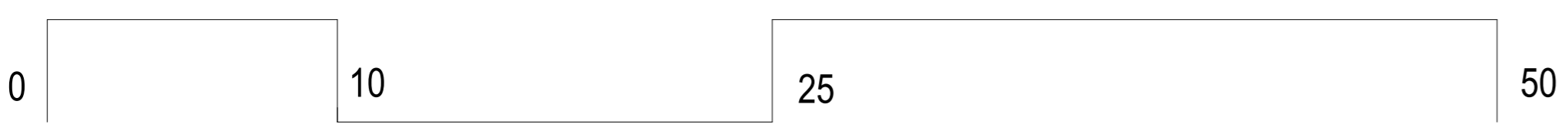
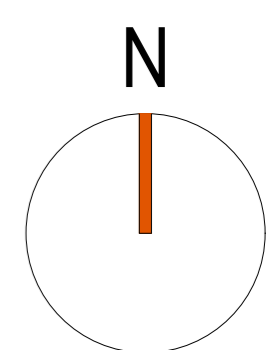
checked: dp status: PLANNING APPLICATION

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drawing No: **17008-A-PL-51** revision: **F**





ACCOMMODATION SCHEDULE	
Application Site Area = 4666 sq.m / 0.47 hectares / 1.15 acres	
PLOT / TYPE	QUANTITY
Plots 1 & 4 - 3 Bedroom Chalet Bungalows (135.9 sq.m / 1463 sq.ft)	2
Plots 2 & 3 - 3 Bedroom Chalet Bungalows (110.2 sq.m / 1186 sq.ft)	2
Plots 5 & 6 - 3 Bedroom Chalet Bungalows (117.2 sq.m / 1261 sq.ft)	2
Plot 7 - 3 Bedroom Bungalow (115.0 sq.m / 1238 sq.ft)	1
<b>TOTAL</b>	<b>7</b>
Parking	14 spaces

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MODEL FILE REFERENCE- 17008-A-P-PL

rev.	description	date	by	ch
E	Bin presentation area re-located / fence heights with No.11 adjusted / radius to bellmouth to south side amended	2021.03.23	AJC	dp
D	Bin presentation area added/ Plots 1 and 2 garages changed to car barns/ northern footpath extended	2021.03.10	AJC	dp
C	Tree canopies amended/ bin collection point removed	2021.02.11	AJC	dp
B	Plots 1 to 6 roof plans amended	2021.01.25	AJC	dp
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

scale: 1:250 @ A1 client: DEVINE HOMES PLC

date: October 2020 project: Land at 9 to 12 West Drive, Banstead. KT20 5PA.

drawn: AJC drawing: Proposed Site Layout (coloured)

checked: dp status: PLANNING APPLICATION

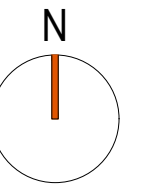
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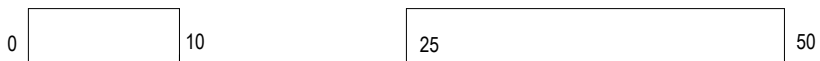
drawing No: **17008-A-PL-052** revision: **E**



# Agenda Item 9



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**MODEL FILE REFERENCE-17008-A-P-PL**

rev	description	date	by	ch.
E	Bin presentation area re-located / radius of bellmouth to south side amended	2021.03.23	AJC	dp
D	Bin presentation area added/ northern footpath extended	2021.03.10	AJC	dp
C	Bin collection point removed	2021.02.11	AJC	dp
B	Plots 1 to 6 roof plans amended	2021.01.25	AJC	dp
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

scale: 1:500 @ A3      client: DEVINE HOMES PLC

date: October 2020      project: 9 to 12 West Drive, Tadworth. KT20 5PA.

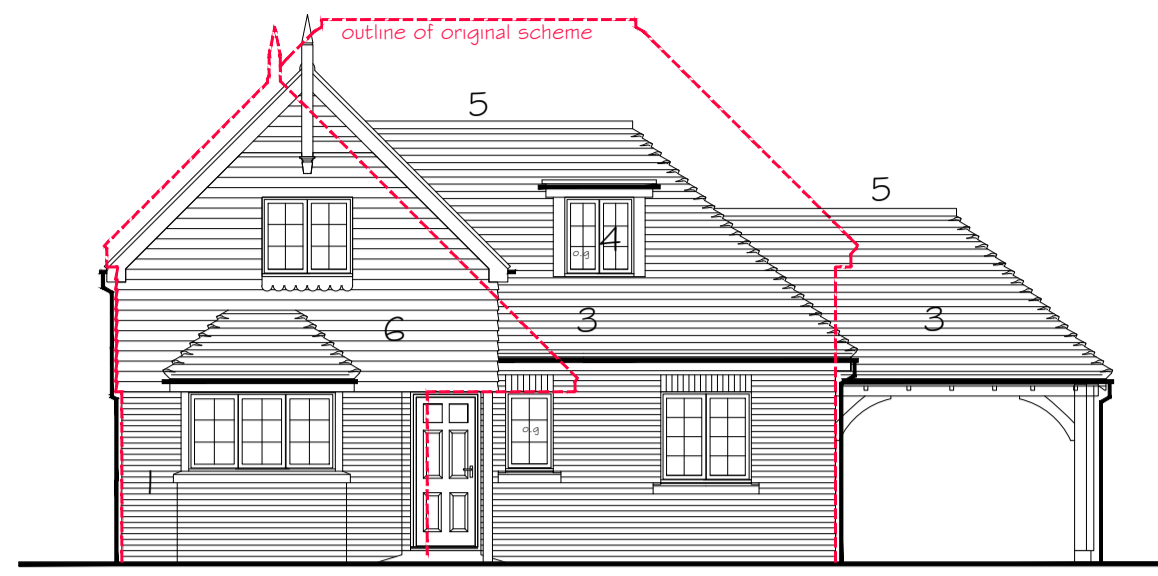
drawn: AJC      drawing: Block Plan

checked: **152**      status: PLANNING APPLICATION

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 t: 01794 830660 e: enquiries@vision-architects.org.uk

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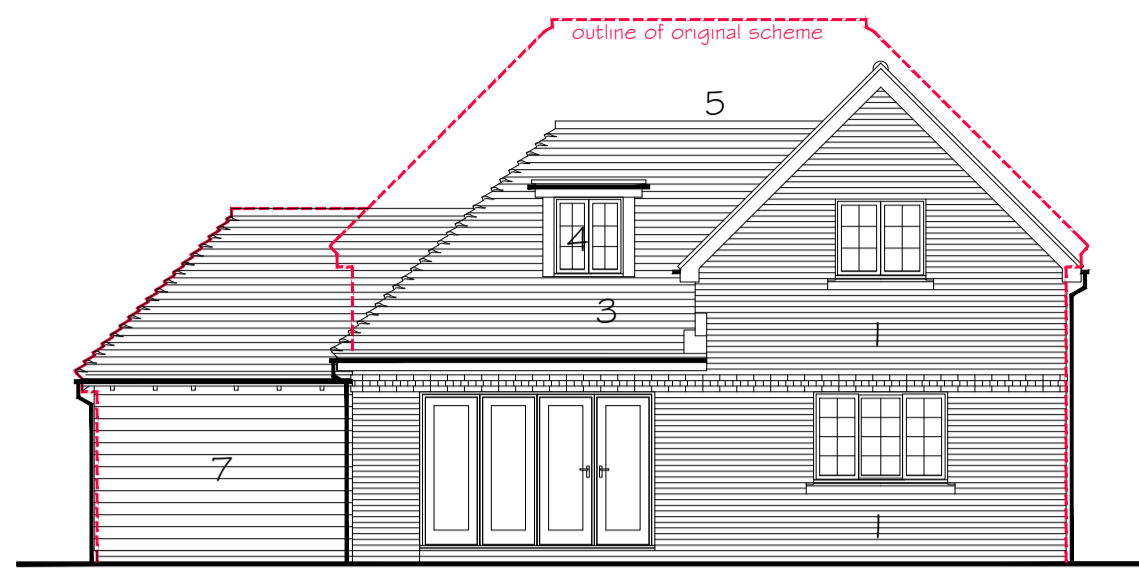
drawing No: **17008-A-PL-053**      revision: **E**



SOUTH EAST ELEVATION



NORTH EAST ELEVATION

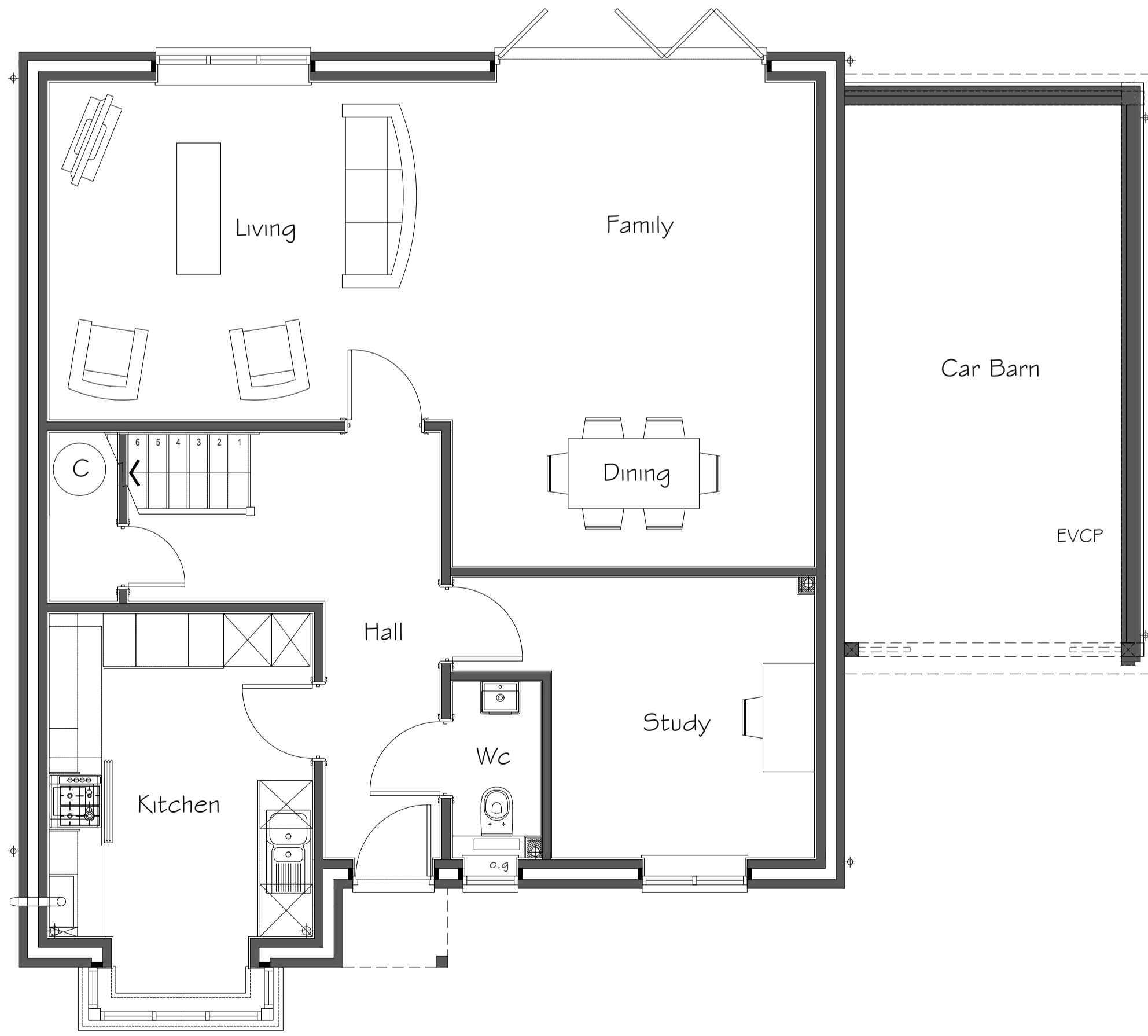
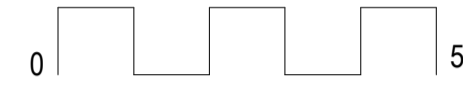


NORTH WEST ELEVATION

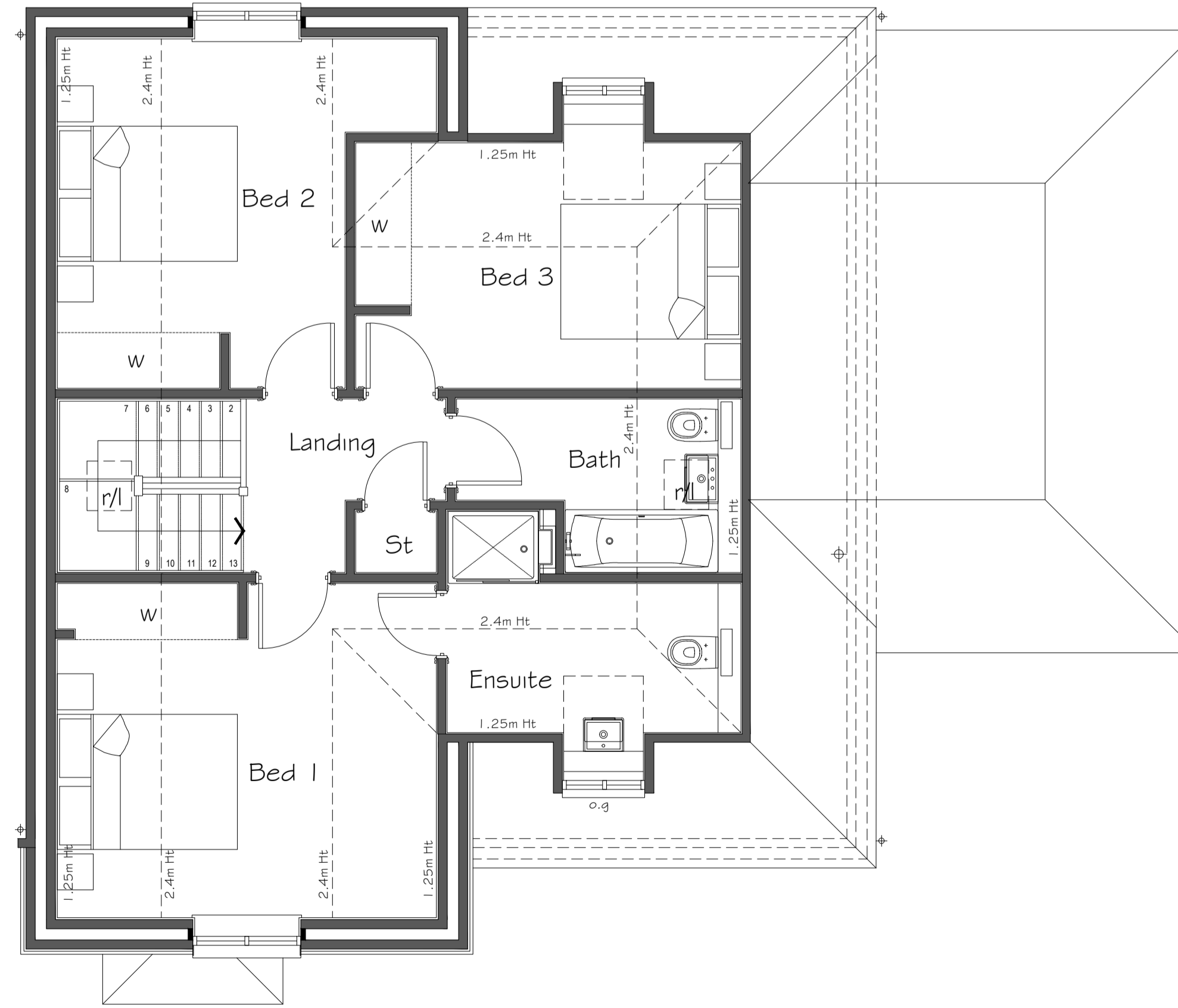


SOUTH WEST ELEVATION

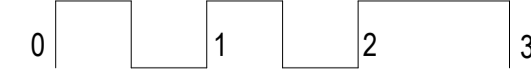
KEY TO MATERIALS	
1	Facing Brckwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN



scale:	1:50 / 1:100 @ A1	client:	DEVINE HOMES PLC
date:	October 2020	project:	9 to 12 West Drive, Tadworth, KT20 5PA.
drawn:	AJC	drawing:	Plot 1 - Proposed Floor Plans & Elevations
checked:	dp	status:	PLANNING APPLICATION

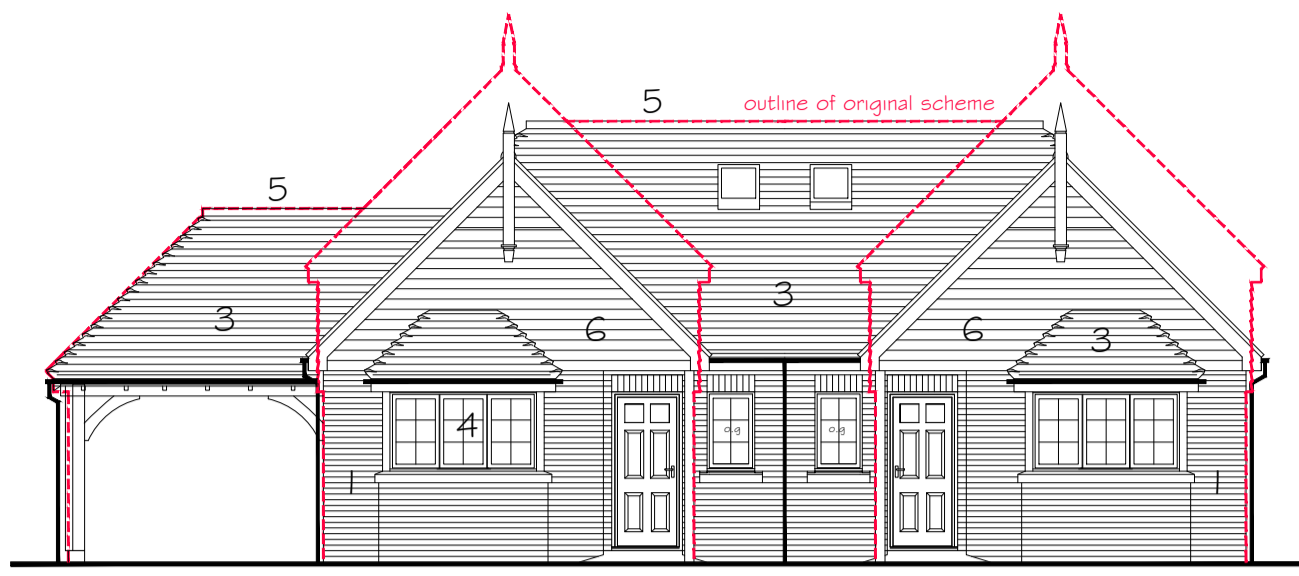
rev	description	date	by	ch
D	Ground floor plan updated with car barn.	2021.03.10	AJC	dp
C	Garage replaced with car barn.	2021.03.10	AJC	dp
B	Unit re-designed.	2021.01.25	AJC	dp
A	Planning issue	2020.10.27	AJC	dp
P1	Primary issue	2020.10.27	AJC	dp

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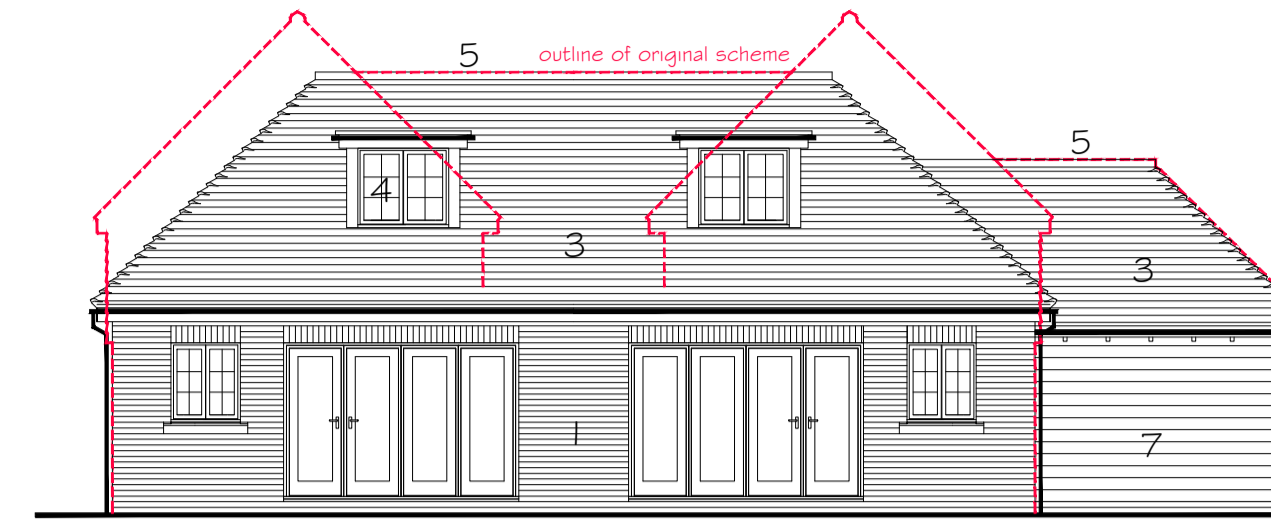
MODEL FILE REFERENCE: 17008-A-P-PL



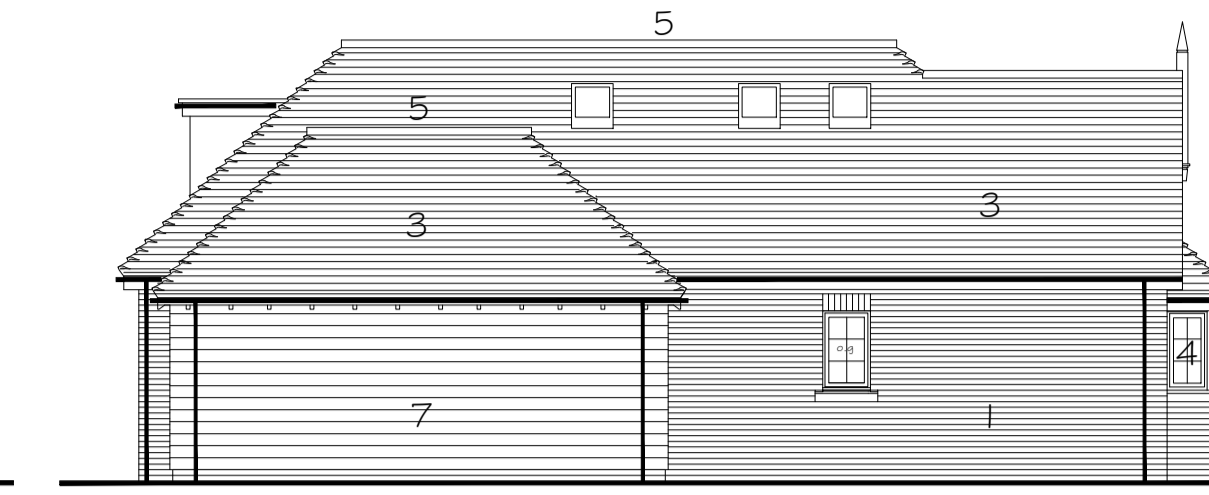
SOUTH EAST ELEVATION



NORTH EAST ELEVATION

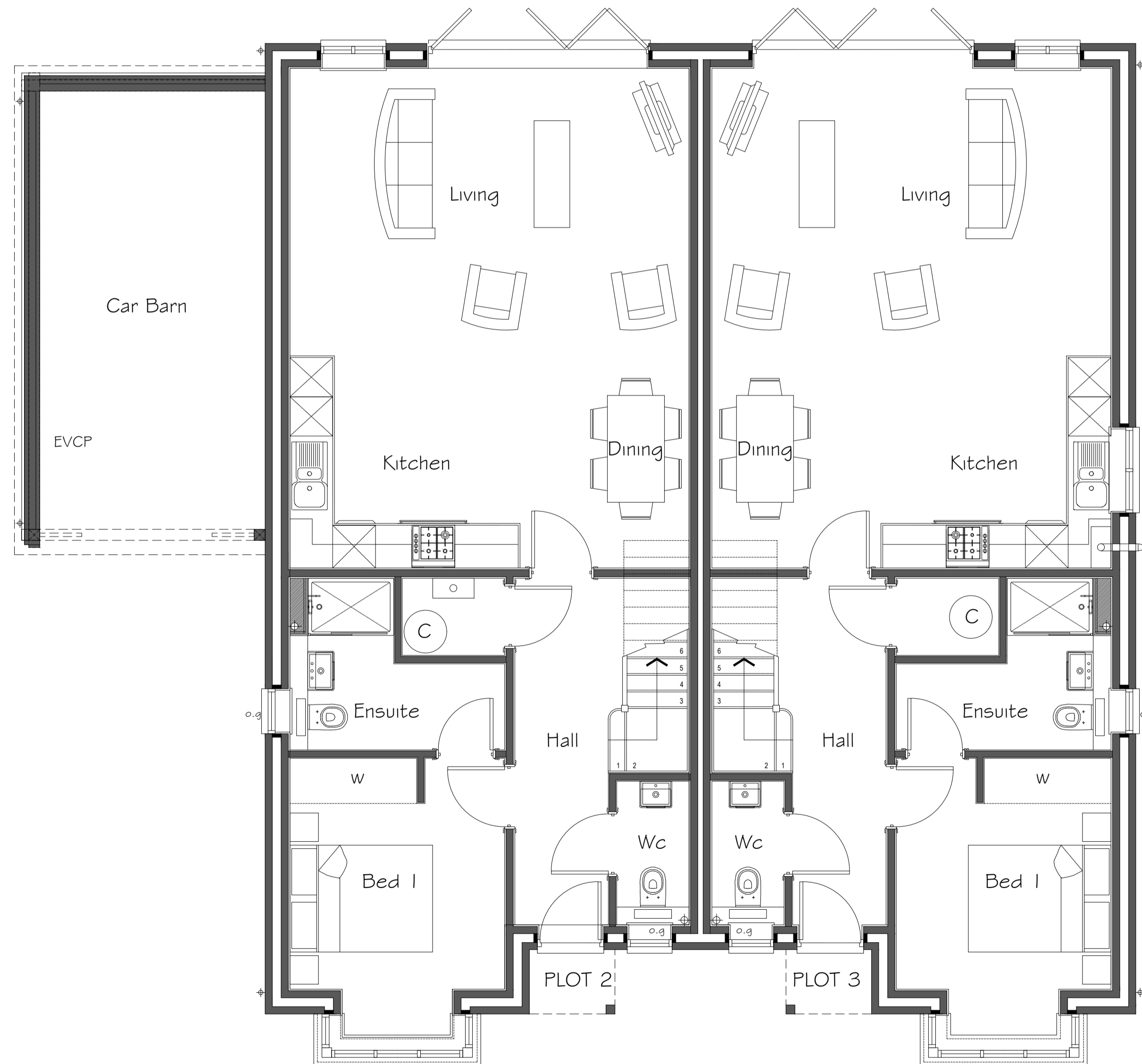
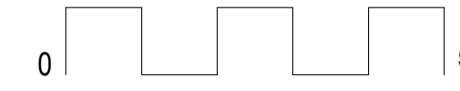


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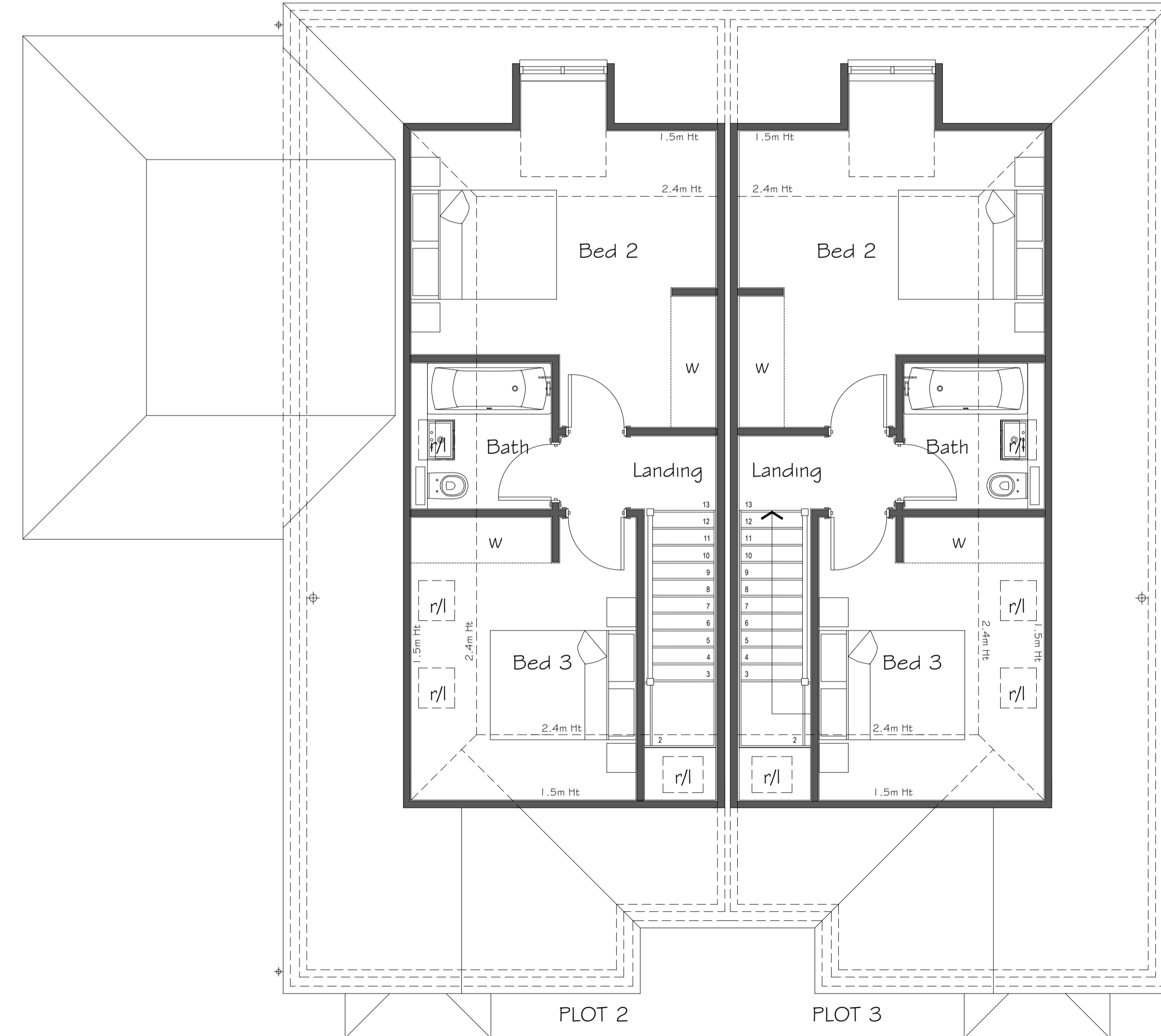


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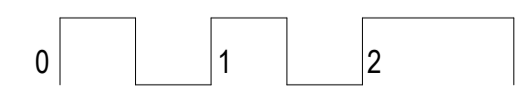
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1	Facing Brickwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN

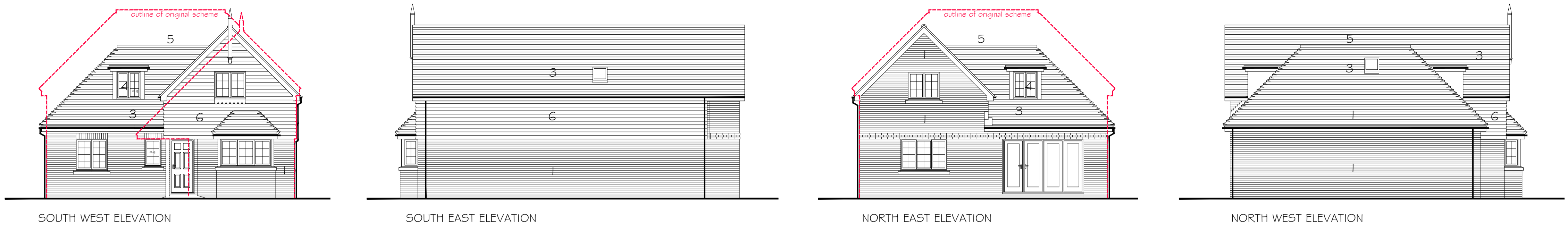


client: DEVINE HOMES PLC  
 project: 9 to 12 West Drive, Tadworth, KT20 5PA.  
 drawing: Plots 2 and 3 - Proposed Floor Plans & Elevations  
 status: PLANNING APPLICATION  
 scale: 1:50 / 1:100 @ A1  
 date: October 2020  
 draw: A/JC  
 checked: dp

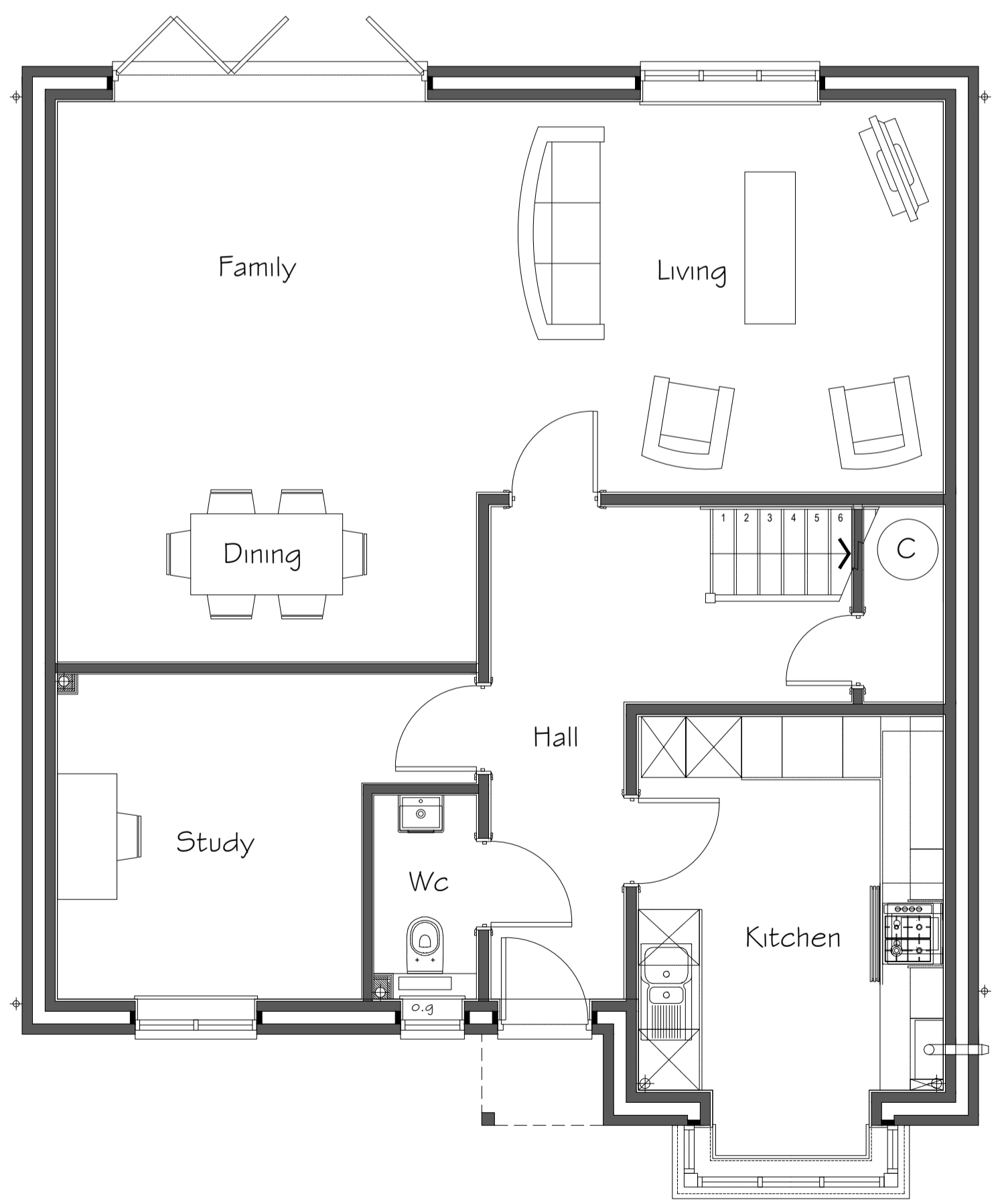
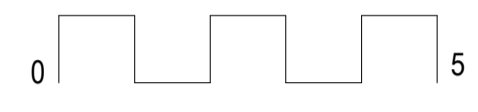
date	by	ch
2021.03.10	A/JC	dp
2021.03.10	A/JC	dp
2021.01.20	A/JC	dp
2020.10.27	A/JC	dp
2020.10.21	A/JC	dp

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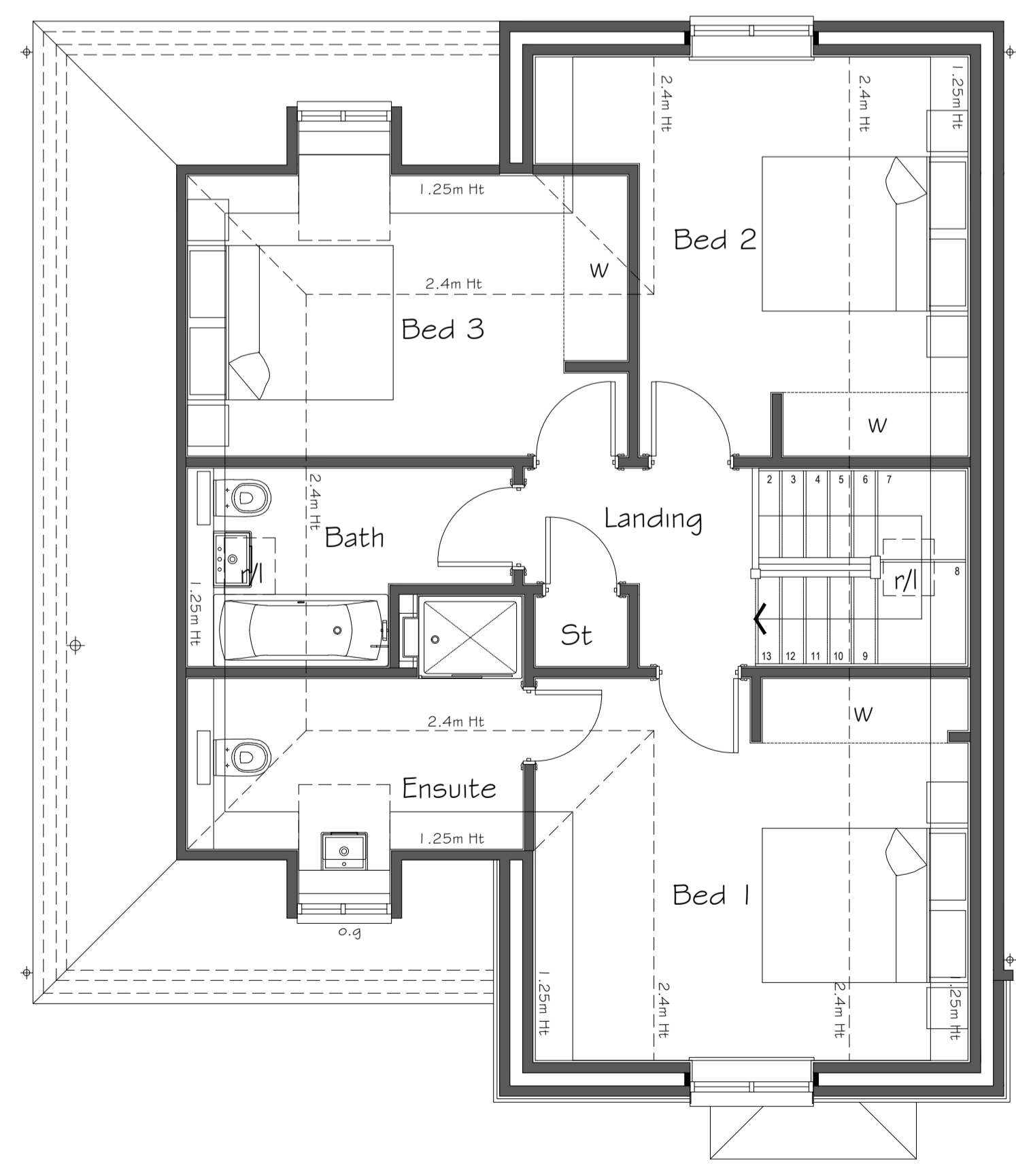




KEY TO MATERIALS	
1	Facing Brickwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



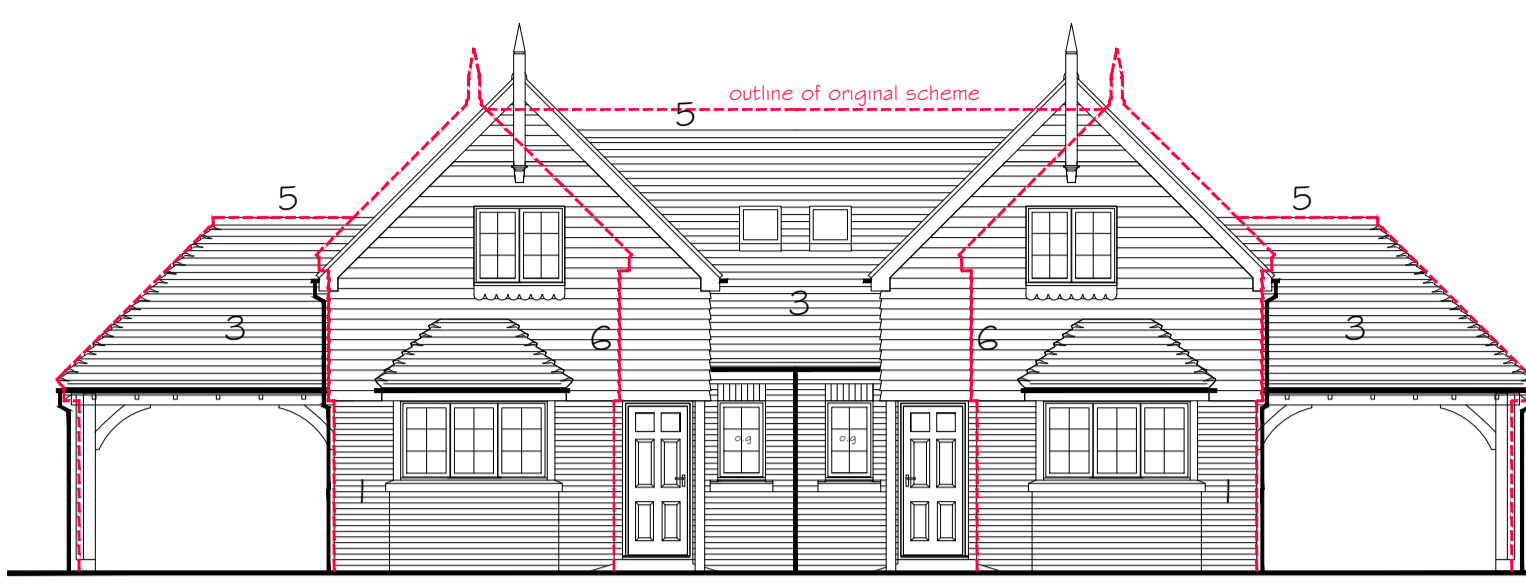
FIRST FLOOR PLAN



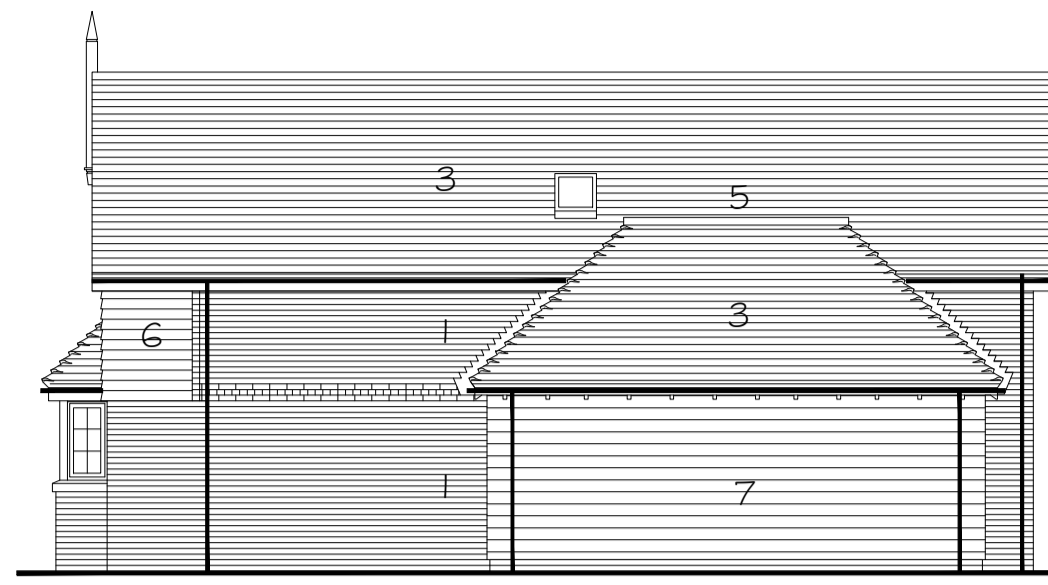
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date:	October 2020	project:	9 to 12 West Drive, Tadworth, KT20 5PA.
drawn:	AJC	drawing:	Plot 4- Proposed Floor Plans & Elevations
checked:	dp	status:	PLANNING APPLICATION

rev	description	date	by	ch
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A	Planning Issue	2020.10.27	AJC	dp
P1	Primary Issue	2020.10.21	AJC	dp

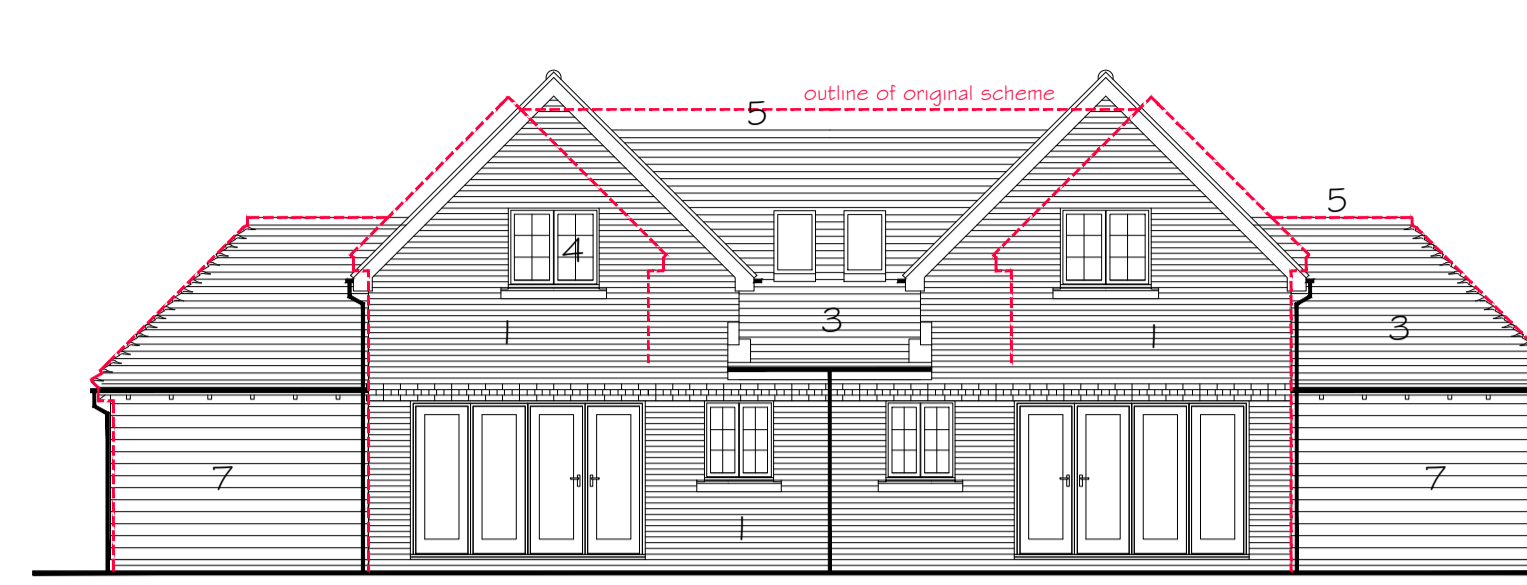
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**MODEL FILE REFERENCE:** 17008-A-PL-PL



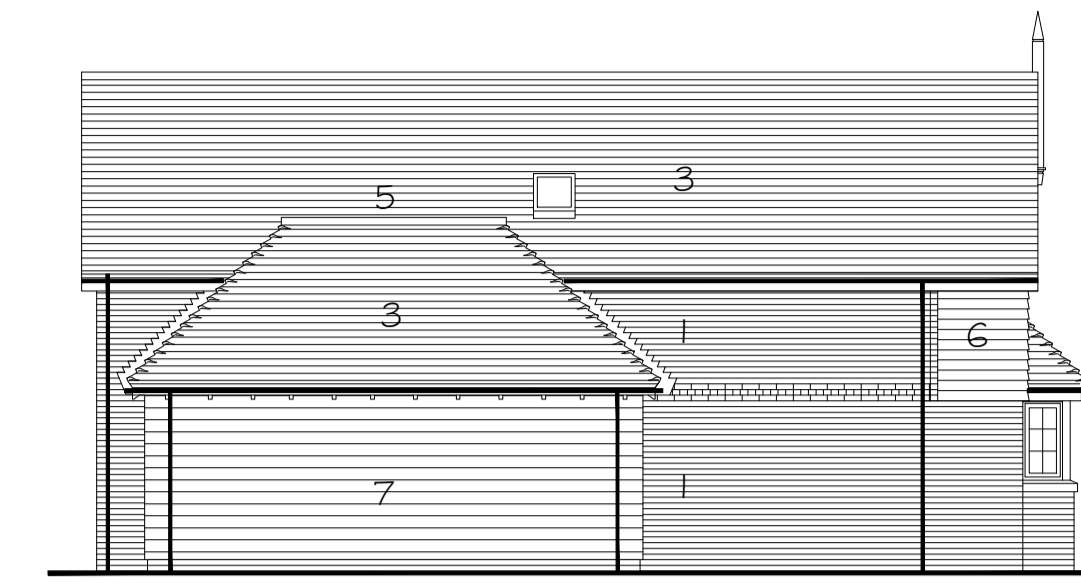
SOUTH WEST ELEVATION



SOUTH EAST ELEVATION

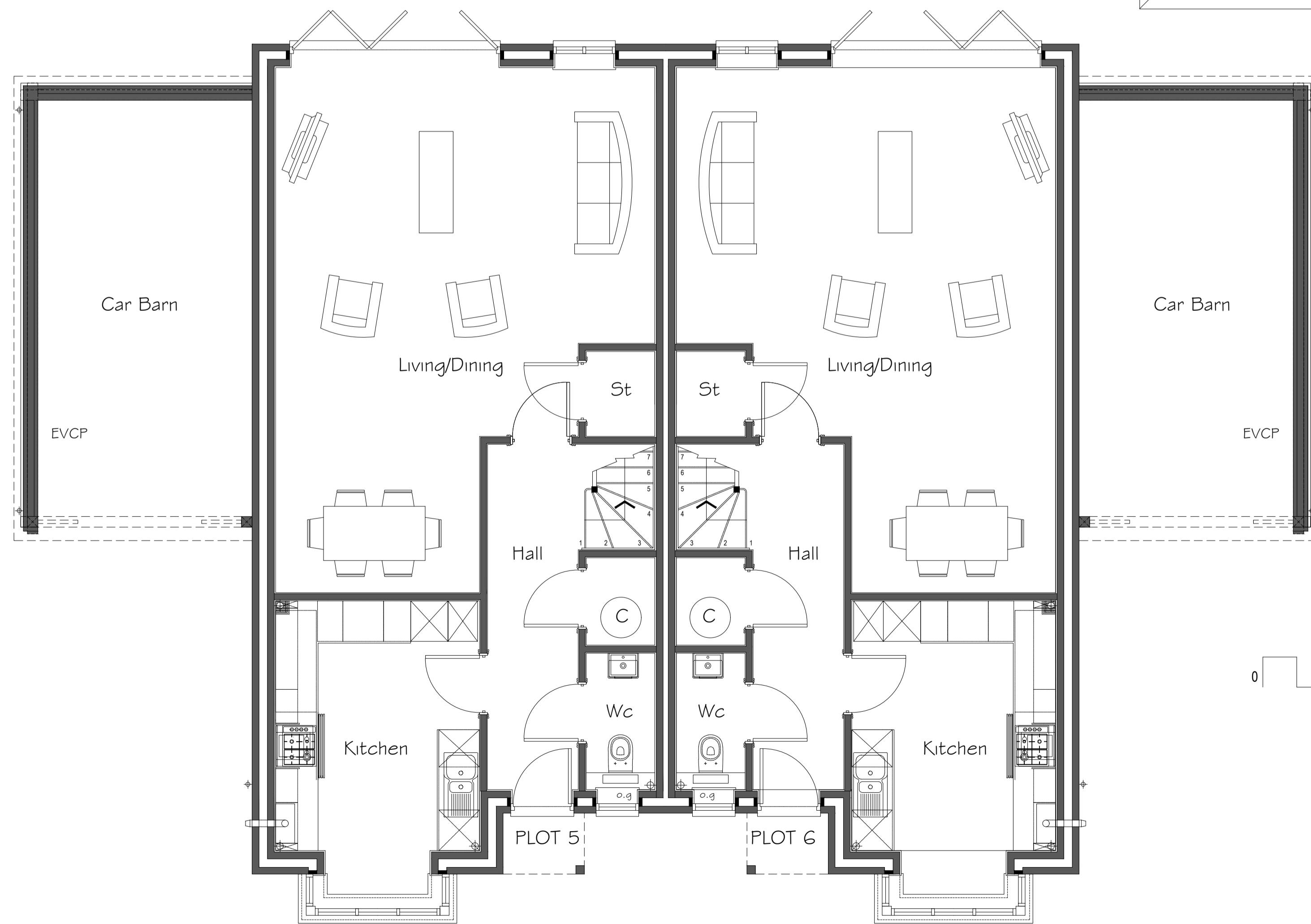
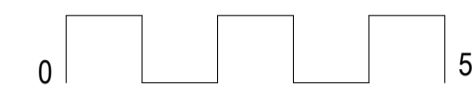


NORTH EAST ELEVATION

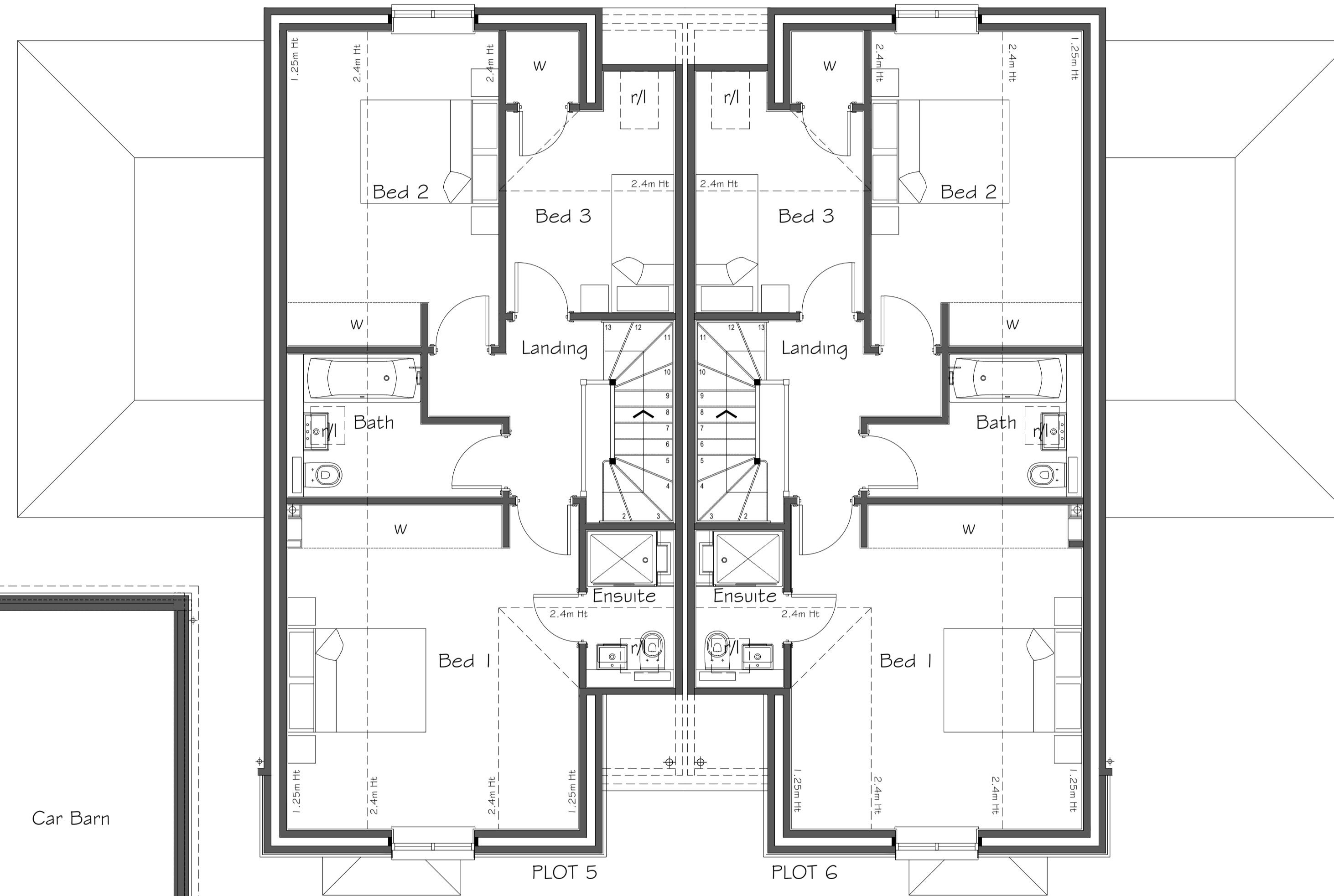


NORTH WEST ELEVATION

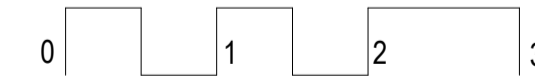
KEY TO MATERIALS	
1	Facing Brickwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



GROUND FLOOR PLAN



FIRST FLOOR PLAN



scale: 1:50 / 1:100 @ A1 client: DEVINE HOMES PLC  
 date: October 2020 project: 9 to 12 West Drive, Tadworth, KT20 5PA.  
 draw: A/JC drawing: Plots 5 and 6- Proposed Floor Plans & Elevations  
 checked: dp status: PLANNING APPLICATION

rev	description	date	by	ch
C	Garages changed to car barns on ground floor plan	2021.03.11	AJC	dp
B	Units re-designed	2021.01.12	AJC	dp
A	Planning issue	2020.10.21	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

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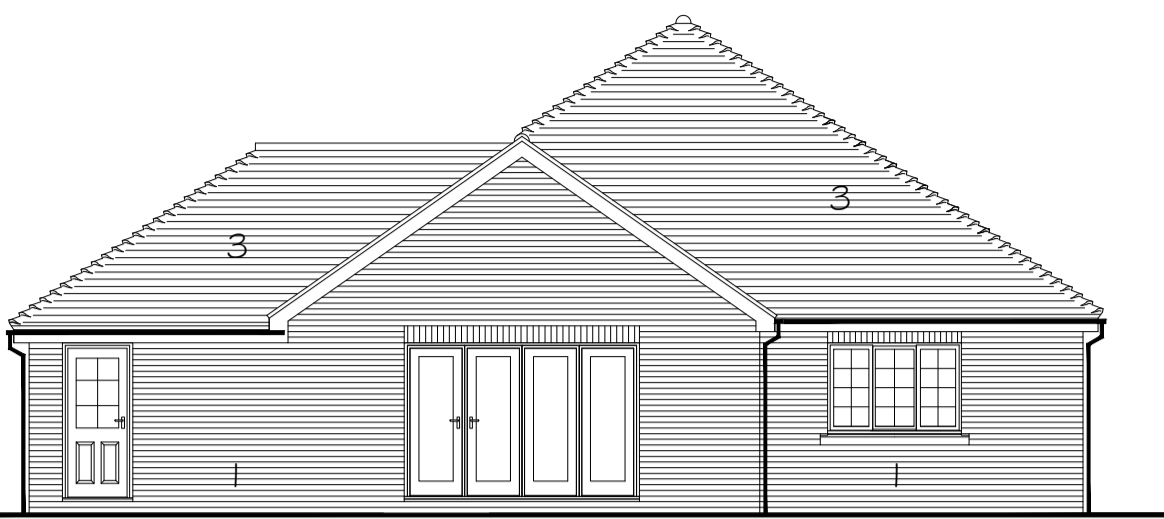




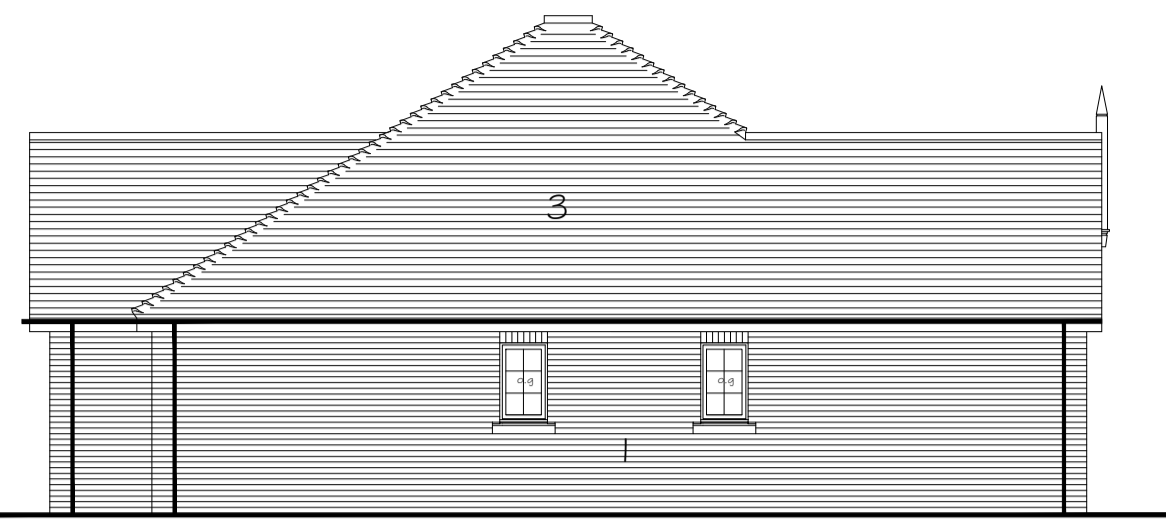
SOUTH WEST ELEVATION



SOUTH EAST ELEVATION



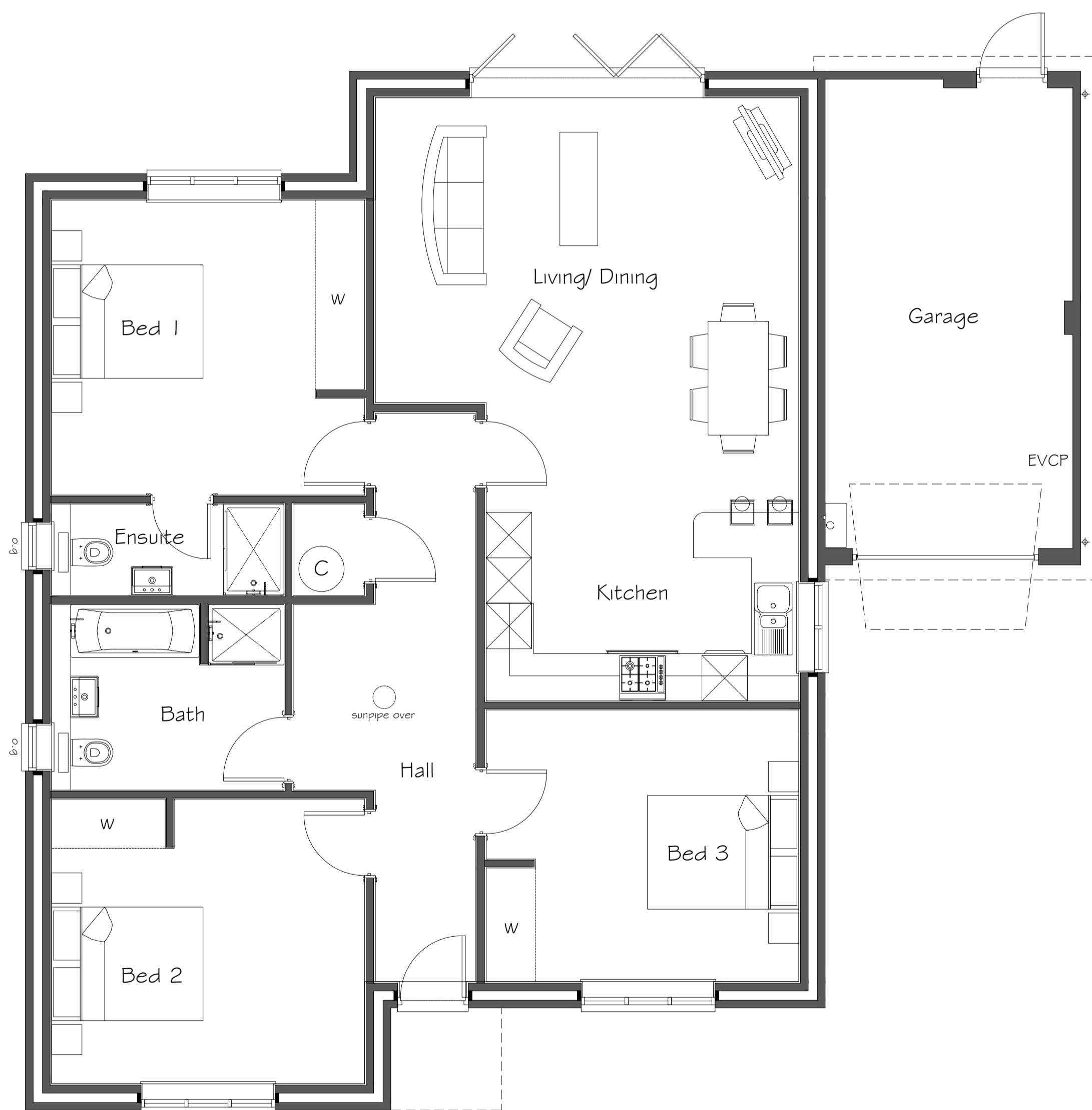
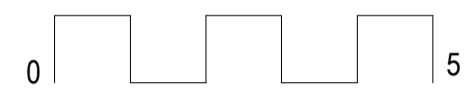
NORTH EAST ELEVATION



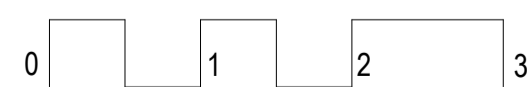
NORTH WEST ELEVATION

KEY TO MATERIALS

1	Facing Brickwork
2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



FLOOR PLAN



scale: 1:50 / 1:100 @ A1 client: DEVINE HOMES PLC  
 date: October 2020 project: 9 to 12 West Drive, Tadworth, KT20 5PA.  
 drawn: AJC drawing: Plot 7 - Proposed Floor Plans & Elevations  
 checked: dp status: PLANNING APPLICATION

2020.10.27 AJC dp  
 2020.10.21 AJC dp  
 date by ch

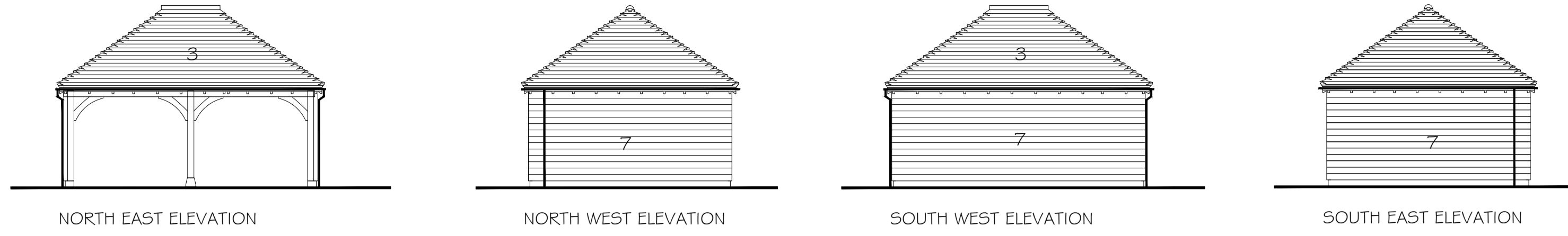
A Planning Issue  
 P1 Primary Issue  
 rev description

notes

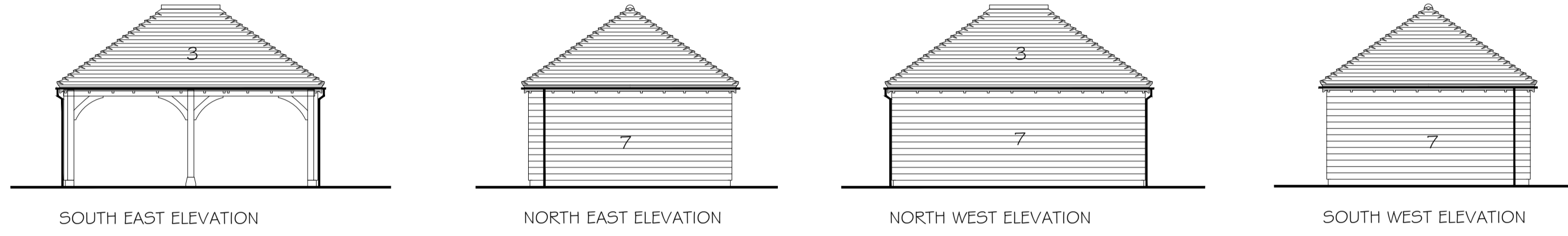
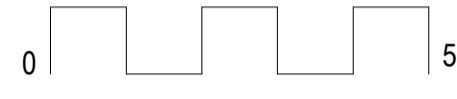
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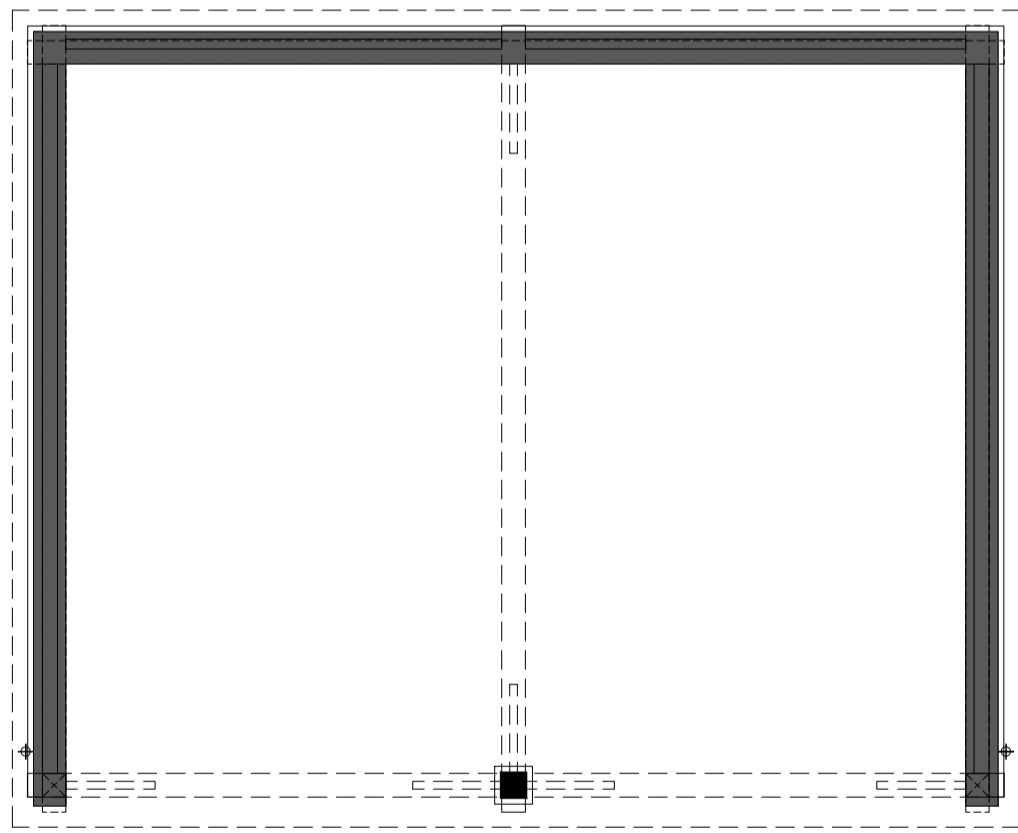
KEY TO MATERIALS	
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2	Render
3	Plain Tiles
4	uPVC Windows and Doors
5	Single Ply Membrane
6	Plain Tile Hanging
7	Timber Boarding



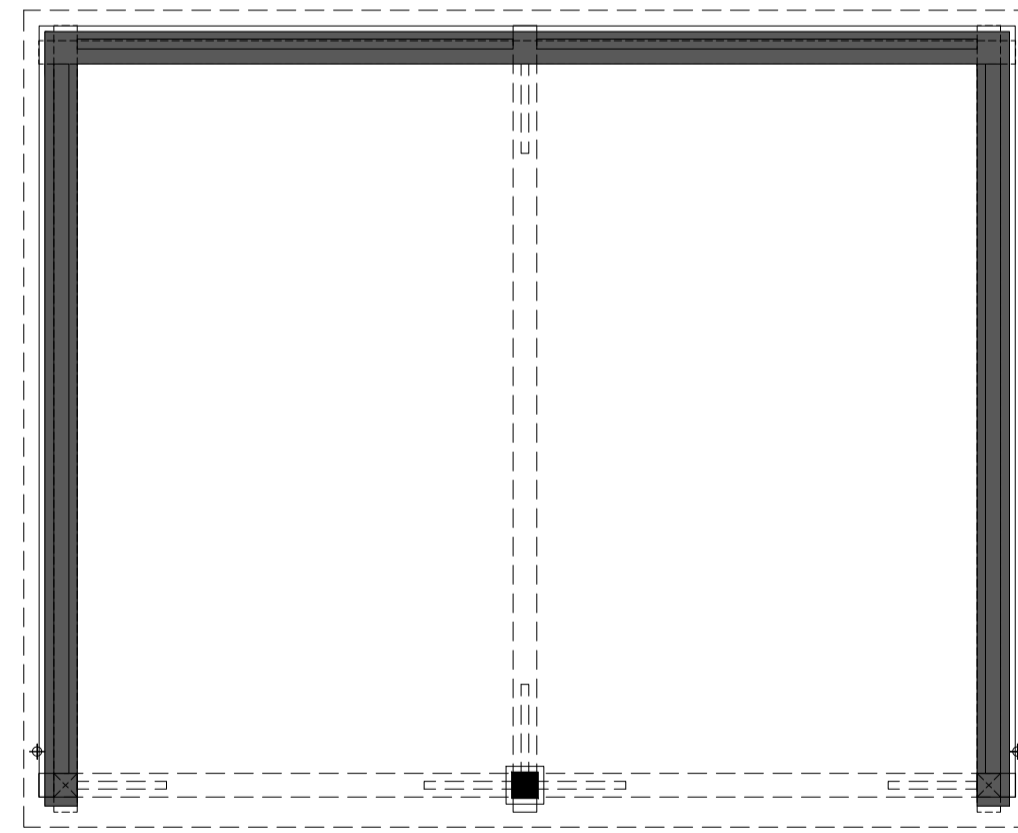
PLOT 3 CAR BARN ELEVATIONS



PLOT 4 CAR BARN ELEVATIONS



PLOT 3 CAR BARN PLAN



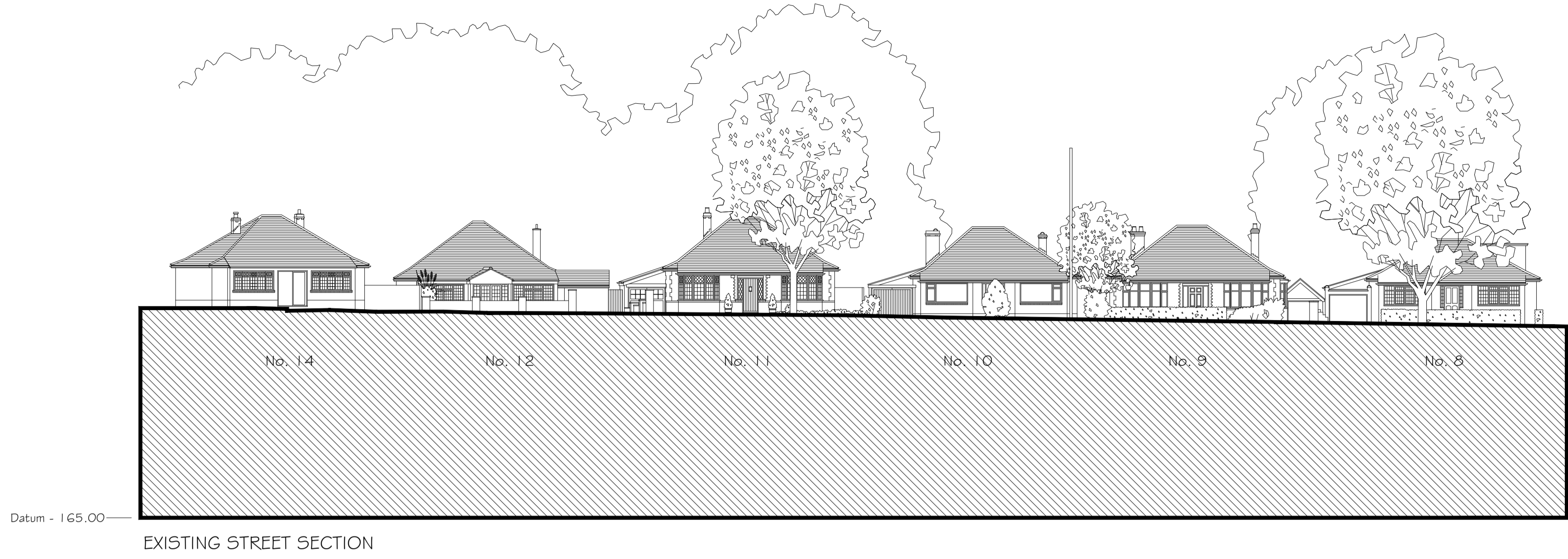
PLOT 4 CAR BARN PLAN



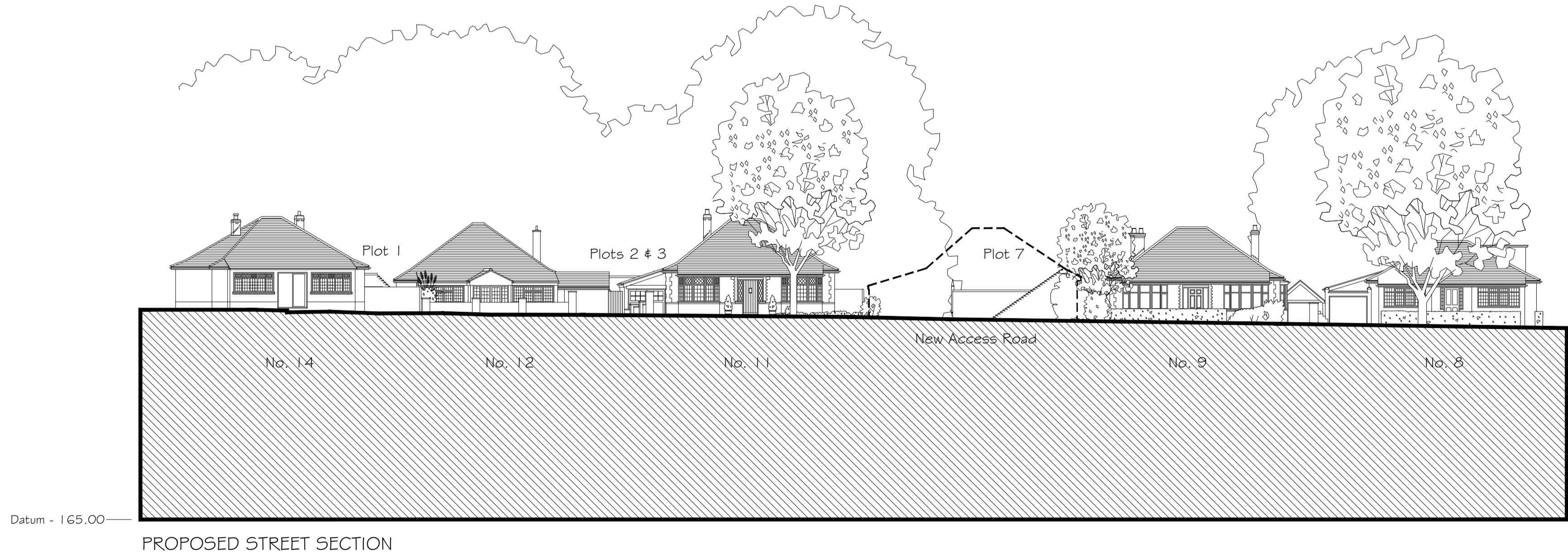
scale: 1:50 / 1:100 @ A1 client: DEVINE HOMES PLC  
 date: October 2020 project: 9 to 12 West Drive, Tadworth, KT20 5PA.  
 drawn: AJC drawing: Proposed Car Barn Plans & Elevations  
 checked: dp status: PLANNING APPLICATION

rev	description	date	by	ch
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

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**MODEL FILE REFERENCE** 17008-A-PL



EXISTING STREET SECTION



PROPOSED STREET SECTION

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rev	description	date	by	ch
B	Updated to reflect re-designed units	2021.01.25	AJC	dp
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

scale: 1:200 @ A1

date: October 2020

drawn: AJC

checked: dp

client: DEVINE HOMES PLC

project: 9 to 12 West Drive, Tadworth, KT20 5PA.

drawing: Existing and Proposed Street Sections from West Drive

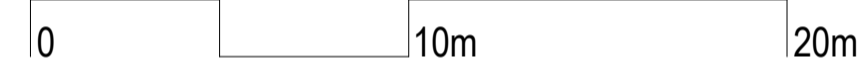
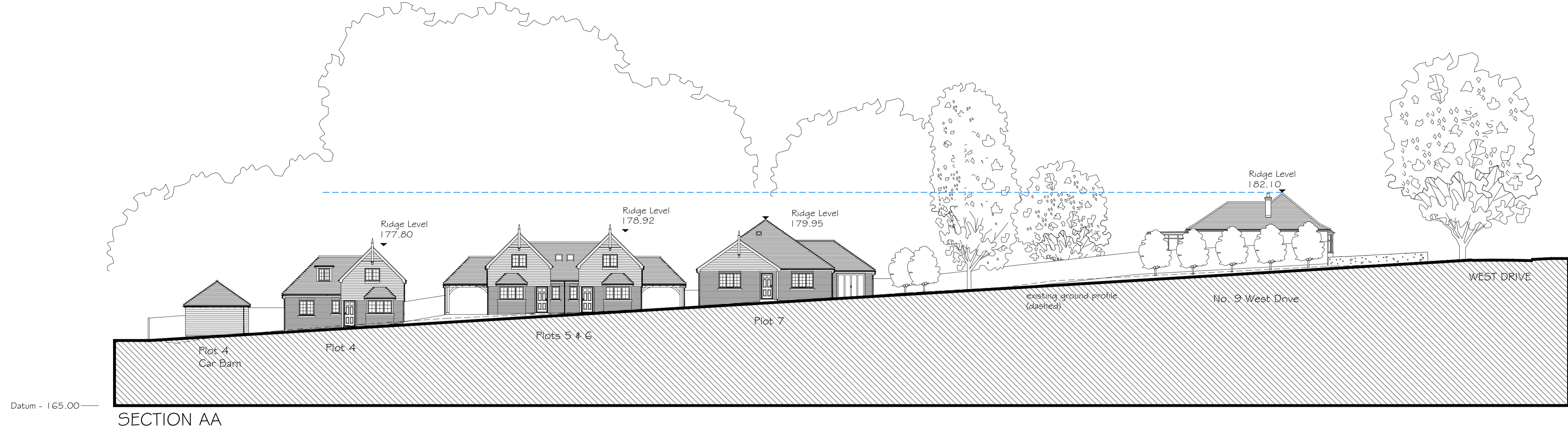
status: PLANNING APPLICATION

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drawing No: 17008-A-PL-060  
revisions: B





client:	DEVINE HOMES PLC
scale:	1:200 @ A1
project:	9 to 12 West Drive, Tadworth, KT20 5PA.
date:	October 2020
drawing:	Site Sections AA and BB
drawn:	AJC
checked:	dp
status:	PLANNING APPLICATION

rev	description	date	by	ch
E	Key Plan updated.	2021.03.23	AJC	dp
D	Section AA: Plots 1 and 2 garages replaced with car barns.	2021.03.10	AJC	dp
C	Bin collection point removed from key plan	2021.02.11	AJC	dp
B	Updated to reflect re-designed units	2021.01.25	AJC	dp
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

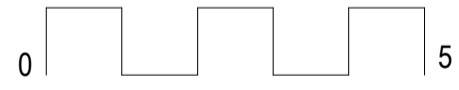
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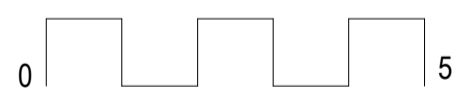
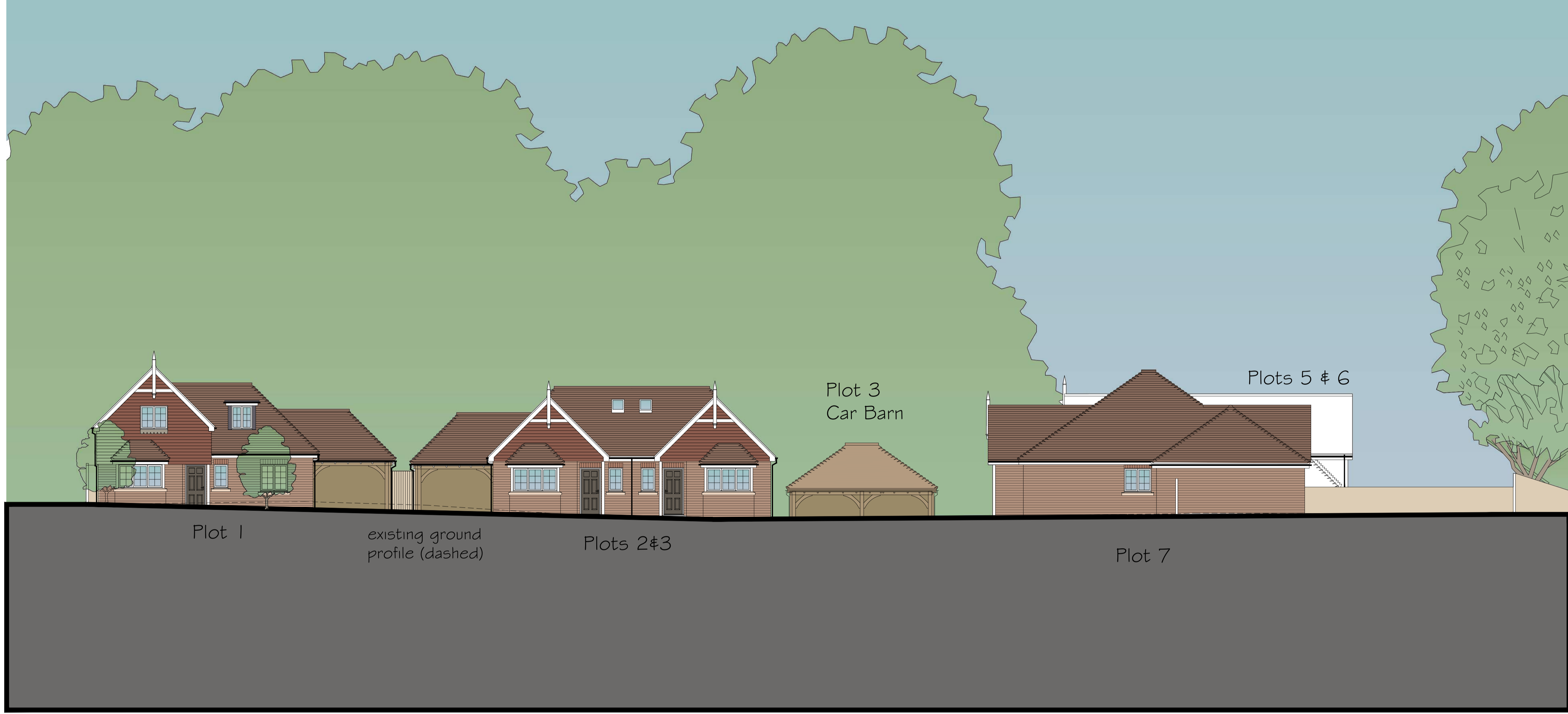
MODEL FILE REFERENCE: 17008-A-PL-PL

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rev	description
B	Updated to reflect re-designed units
A	Planning Issue
P1	Preliminary Issue

date	by	ch
2021.01.25	AJC	dp
2020.10.27	AJC	dp
2020.10.21	AJC	dp

scale: 1:100 @ A1  
 client: DEVINE HOMES PLC  
 date: October 2020  
 project: 9 to 12 West Drive, Tadworth, KT20 9PA  
 drawing: Proposed Street Section AA within Site (coloured)  
 status: PLANNING APPLICATION  
 drawn: AJC  
 checked: dp



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rev	description
C	Plot 1 and 2 garages replaced with car barns.
B	Updated to reflect re-designed units
A	Planning Issue
P1	Preliminary Issue

date	by	ch
2021.03.10	AJC	dp
2021.01.25	AJC	dp
2020.10.27	AJC	dp
2020.10.21	AJC	dp

scale: 1:100 @ A1

client: DEVINE HOMES PLC

date: October 2020

drawn: AJC

drawing: Proposed Street Section BB within Site (coloured)

checked: dp

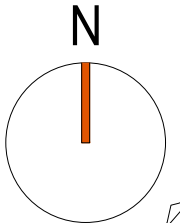
status: PLANNING APPLICATION

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drawing No: 17008-A-PL-063  
revisions: C





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MODEL FILE REFERENCE-17008-A-P-PL

rev	description	date	by	ch.
D	Bin presentation area re-located / fence heights with No.11 adjusted / radius to bellmouth to south side amended	2021.03.23	AJC	dp
C	Bin presentation area added/ northern footpath extended	2021.03.10	AJC	dp
B	Bin collection point removed	2021.02.11	AJC	dp
A	Planning Issue	2020.10.27	AJC	dp
P1	Preliminary Issue	2020.10.21	AJC	dp

scale: 1:250 @ A3  
 client: DEVINE HOMES PLC  
 date: October 2020  
 project: 9 to 12 West Drive, Tadworth. KT20 5PA.  
 drawn: AJC  
 drawing: Sightlines Plan  
 checked: dp  
 status: PLANNING APPLICATION

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 t: 01794 830660 e: enquiries@vision-architects.org.uk

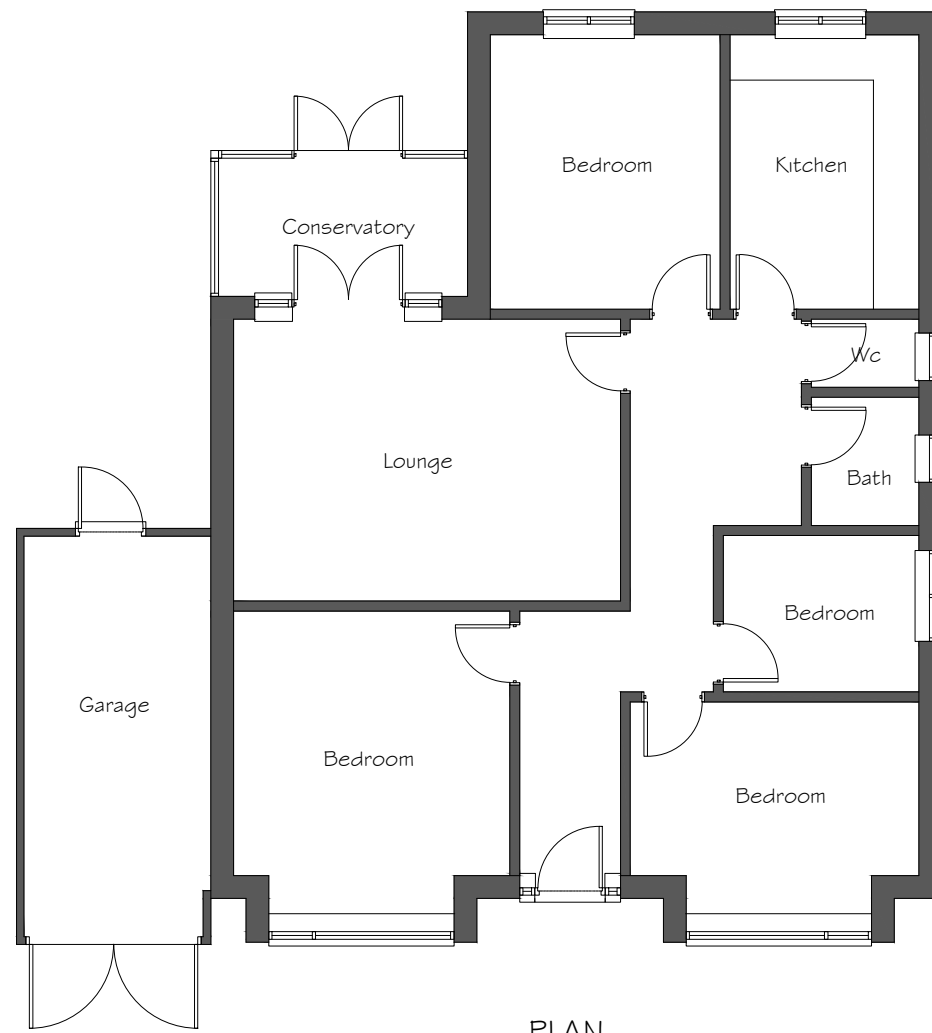
**vision**  
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drawing No: 17008-A-PL-066 revision: **D**

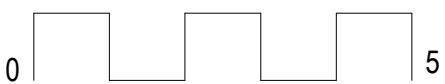
# Agenda Item 9



SOUTH EAST (FRONT) ELEVATION

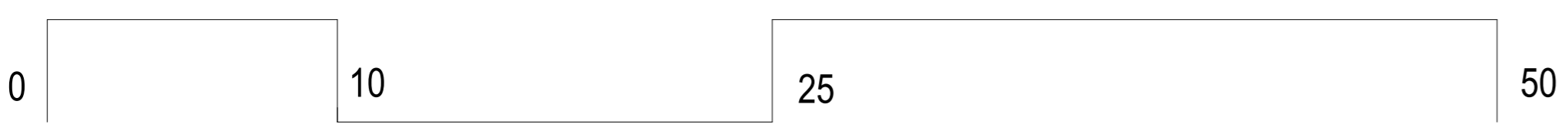
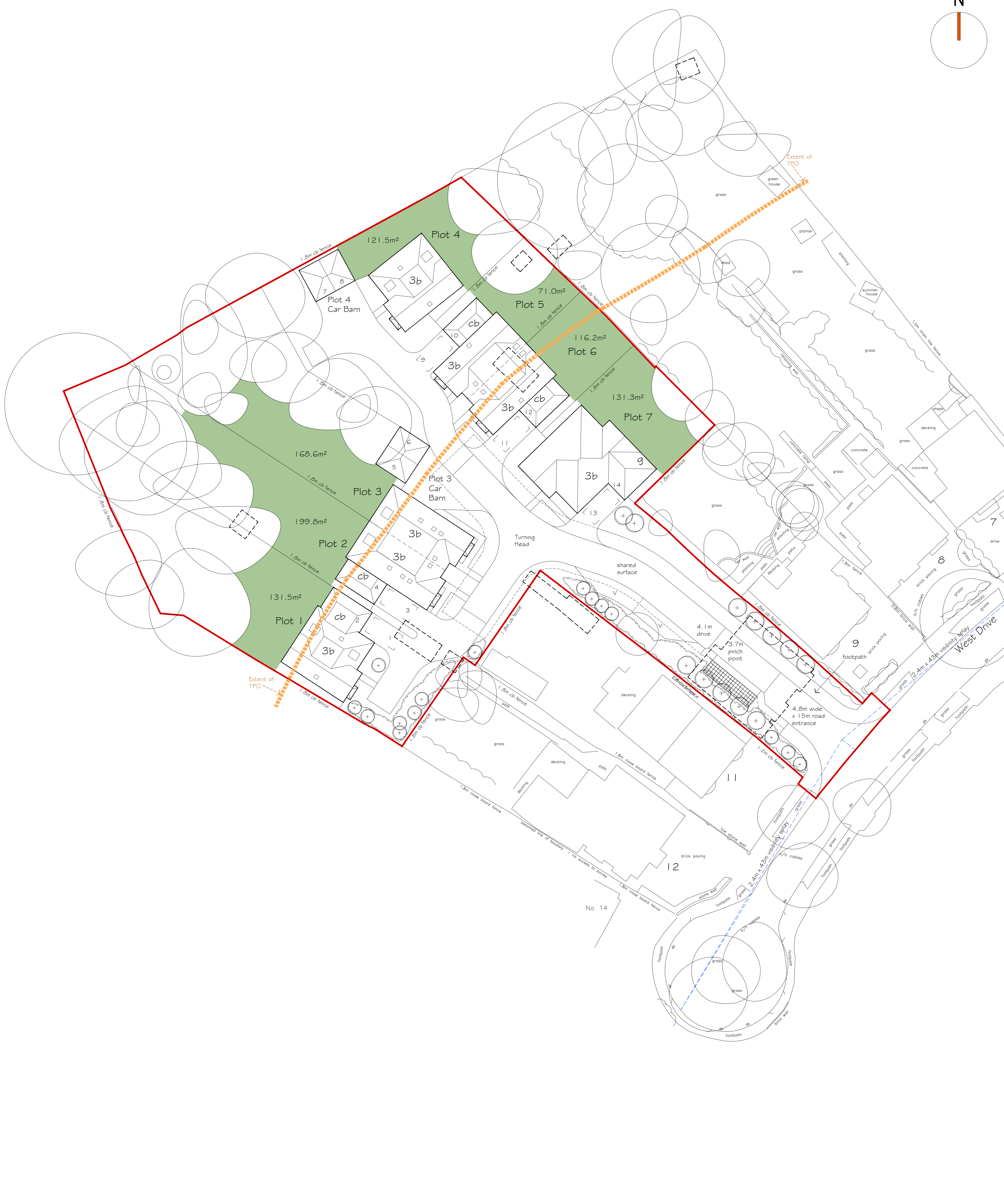
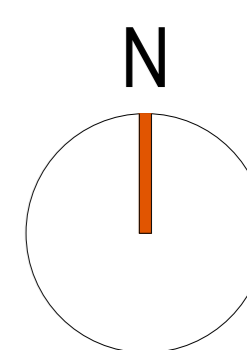


PLAN



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A	Planning Issue	2020.10.27	AJC	dp														
P1	Preliminary Issue	2020.10.21	AJC	dp														
rev	description	date	by	ch.														





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MODEL FILE REFERENCE- 17008-A-P-PL

rev.	description	date	by	ch
C	Bin presentation area re-located / fence heights with No.11 adjusted / radius to bellmouth to south side amended	2021.03.23	AJC	dp
B	Bin presentation area added/ northern footpath extended/ Plots 1 and 2 garages changed to car barns	2021.03.10	AJC	dp
A	Planning Issue	2021.02.11	AJC	dp
			by	ch

scale: 1:250 @ A1 client: DEVINE HOMES PLC

date: February 2021 project: Land at 9 to 12 West Drive, Banstead. KT20 5PA.

drawn: AJC drawing: Garden Area Plan

checked: dp status: PLANNING APPLICATION

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t: 01794 830660 e: enquiries@vision-architects.org.uk

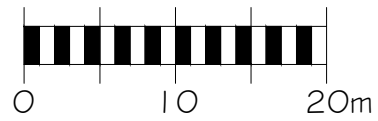
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drawing No: 17008-A-PL-69 revision: C

REFUSED LAYOUT - 17/01261/F




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C	Bin collection point added	21.07.17	AJC	AJC																								
B	Plots 3&4 and 7 to 9 amended/ shared open space, visitor parking and parking space to Plot 5 added-Planning Issue	23.05.17	AJC	AJC																								
A	Planning Issue	24.03.17	AJC	AJC																								
P1	Preliminary Issue	23.03.17	AJC	AJC																								
rev	description	date	by	ch.																								



 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE
	<b>DATE:</b>	14 April 2021
	<b>REPORT OF:</b>	HEAD OF PLANNING
	<b>AUTHOR:</b>	Kate Beith
	<b>TELEPHONE:</b>	01737 276171
	<b>EMAIL:</b>	Kate.Beith@reigate-banstead.gov.uk
<b>AGENDA ITEM:</b>	10	<b>WARD:</b> <i>Hooley, Merstham and Netherne</i>

<b>APPLICATION NUMBER:</b>	21/00050/F	<b>VALID:</b>	18 February 2021
<b>APPLICANT:</b>	MS UK Property Ltd	<b>AGENT:</b>	Michael Blacker Partnership
<b>LOCATION:</b>	<b>129 AND 131 BLETCHINGLEY ROAD, MERSTHAM</b>		
<b>DESCRIPTION:</b>	<b>Proposed vehicle crossovers. As amended on 18/02/2021.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

**This application is referred to Committee in accordance with the Constitution as the Applicant's Agent is a Borough Councillor.**

## SUMMARY

The application seeks permission for two separate vehicular crossovers with dropped kerbs to Nos.129 (which is divided into two flats) and 131 Bletchingley Road. Planning permission is required as the road is classified as a 'C' road and due to the associated works to the flats.

The proposed works will result in a slight change to the appearance of the street scene but overall, the impact is not considered to be harmful to the character of the area or to the amenities of neighbouring dwellings, with other similar examples having already been permitted and constructed in the vicinity.

The County Highways Authority have raised no objection to the proposal subject to recommended conditions.

## RECOMMENDATION

Planning permission is **GRANTED** subject to conditions.

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17<sup>th</sup> April 2021

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## Consultations:

Highway Authority: No objection subject to standard conditions and informatives.

Raven Housing Trust: No specific comments, easements have been agreed to.

## Representations:

Letters were sent to neighbouring properties on 2<sup>nd</sup> February 2021 and 22<sup>nd</sup> February 2021 following the submission of amended plans. No representations have been received.

## 1.0 Site and Character Appraisal

- 1.1 The application site is formed of two terraced dwellings, one of which (no.129) is sub divided into two flats (Flat 1 and Flat 2). The properties are set in relatively modest plots on the north side of Bletchingley Road. They are set back from the highway, with a wide grass verge owned by Raven Housing Trust situated between the highway and the front gardens of these properties. The site is fairly flat throughout. There are no trees likely to be affected by the proposal.
- 1.2 The surrounding area consists of predominantly semi-detached and terraced residential properties of similar age and style. A number of properties have had similar dropped kerbs and vehicular accesses constructed.

## 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: The opportunity did not arise because the applicant did not approach the Local Planning Authority before submitting the application.
- 2.2 Improvements secured during the course of the application: None sought but amended plans with straight rather than splayed accesses were submitted at the request of Raven Housing Trust.
- 2.3 Further improvements could be secured: Highways safety conditions attached.

## 3.0 Relevant Planning and Enforcement History

- |     |            |   |   |
|-----|------------|---|---|
| 3.1 | 16/02742/F | No. 129 - Conversion from HMO to 2 no. 1bed apartments. | Approved with Conditions<br>18 <sup>th</sup> January 2017                 |
| 3.2 | 16/03011/F | Nos. 127 and 129 – Vehicular crossovers                 | Approved with Conditions – Not implemented<br>13 <sup>th</sup> March 2017 |

## 4.0 Proposal and Design Approach

- 4.1 This is a full application for a dropped kerb to the highway for each property. The dropped kerb would be 3.6m wide and with a vehicular access formed of 'grasscrete'. Each access would be 3m wide and would cross the existing approx. 9.9m deep grass verge to the front gardens of both these properties.
- 4.2 The proposed vehicular crossovers would enable the residents of these properties to access off street parking where the front gardens of both houses have been previously laid to hardstanding. There would be 2 parking spaces provided to both sites.

## 5.0 Policy Context

### 5.1 Designation

Urban Area

### 5.2 Reigate and Banstead Core Strategy

CS4 (Valued Townscapes and Historic Environment)

### 5.2 Reigate & Banstead Development Management Plan 2019

DES1 (Design of new development)  
TAP1 (Access, parking and servicing)

### 5.3 Other Material Considerations

National Planning Policy Framework

Supplementary Planning Guidance

Householder Extensions and Alterations

Other

Human Rights Act 1998

## 6.0 Assessment

- 6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development.
- 6.2 The main issues to consider are:
- Impact on local character
  - Neighbour amenity
  - Highway matters

### Impact on local character

- 6.3 The proposed alterations would have little detrimental impact on the character

# Agenda Item 10

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21/00050/HHOLD

of the area as no built volume would be created. The proposal specifies 'grasscrete' for the crossover which is favourable because it preserves some of the natural green appearance of the grass verge. There are other similar examples of crossovers/dropped kerbs in the area which have been granted planning permission.

- 6.4 Overall, the proposal is considered acceptable in terms of its design and character impact and accords with policies DES1 and TAP1.

### Neighbour amenity

- 6.5 Whilst giving rise to a degree of change to the street scene, the proposed scheme would not adversely affect the amenity of neighbouring properties and complies with policy DES1 in this regard.

### Highway matters

- 6.6 A new access for each property onto a classified road would be formed and the proposal would provide access for off street parking for 2 vehicles for both properties. Both front gardens have been previously laid to hardstanding.
- 6.7 The County Highway Authority was consulted on the application and has assessed the proposal in terms of highways safety, capacity and policy grounds and had no objections to the proposal subject to a number of conditions which are reflected in the recommendation below.

## **CONDITIONS**

1. The development hereby permitted shall be carried out in accordance with the following approved plans.

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

<b>Plan Type</b>	<b>Reference</b>	<b>Version</b>	<b>Date Received</b>
Location Plan	SK01	B	18.02.2021
Block Plan	01	A	18.02.2021
Proposed Plans	02	A	18.02.2021

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004

3. The materials to be used in the construction of accesses shall be as specified.

Reason: To ensure that the development hereby permitted is only constructed using the appropriate external materials or suitable alternatives in the interest of the visual amenities of the area with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

4. The proposed vehicular accesses to Bletchingley Road shall be constructed in accordance with the approved plans and thereafter shall be permanently maintained.

Reason: To ensure that the development should not prejudice highway safety nor cause inconvenience to other highway users and in order to meet the objectives of the NPPF (2019), and to satisfy policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

5. Space shall be laid out within the site in accordance with the approved plans for vehicles to be parked. Thereafter the parking areas shall be retained and maintained for their designated purpose.

Reason: To ensure that the development should not prejudice highway safety nor cause inconvenience to other highway users and in order to meet the objectives of the NPPF (2019), and to satisfy policy TAP1 of the Reigate and Banstead Development Management Plan 2019.

## INFORMATIVES

1. The permission hereby granted shall not be construed as authority to carry out any works on the highway. The applicant is advised that prior approval must be obtained from the Highway Authority, Local Highway Services Group (0300 200 1003), before any works are carried out on any footway, footpath, carriageway, or verge to form a vehicle crossover to install dropped kerbs. Please see: [www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs](http://www.surreycc.gov.uk/roads-and-transport/road-permits-and-licences/vehicle-crossovers-or-dropped-kerbs)
2. The developer is advised that as part of the detailed design of the highway works required by the above conditions, the County Highway Authority may require necessary accommodation works to street lights, road signs, road markings, highway drainage, surface covers, street trees, highway verges, highway surfaces, surface edge restraints and any other street furniture/equipment.

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3. Section 59 of the Highways Act permits the Highway Authority to charge developers for damage caused by excessive weight and movements of vehicles to and from a site. The Highway Authority will pass on the cost of any excess repairs compared to normal maintenance costs to the applicant/organisation responsible for the damage.
4. The applicants are advised that any new hardstanding on their property in excess of 5 square metres, ought to be constructed with permeable paving or drain entirely within the site and not allow overflow of water onto the highway or other land outside their ownership.

## **REASON FOR PERMISSION**

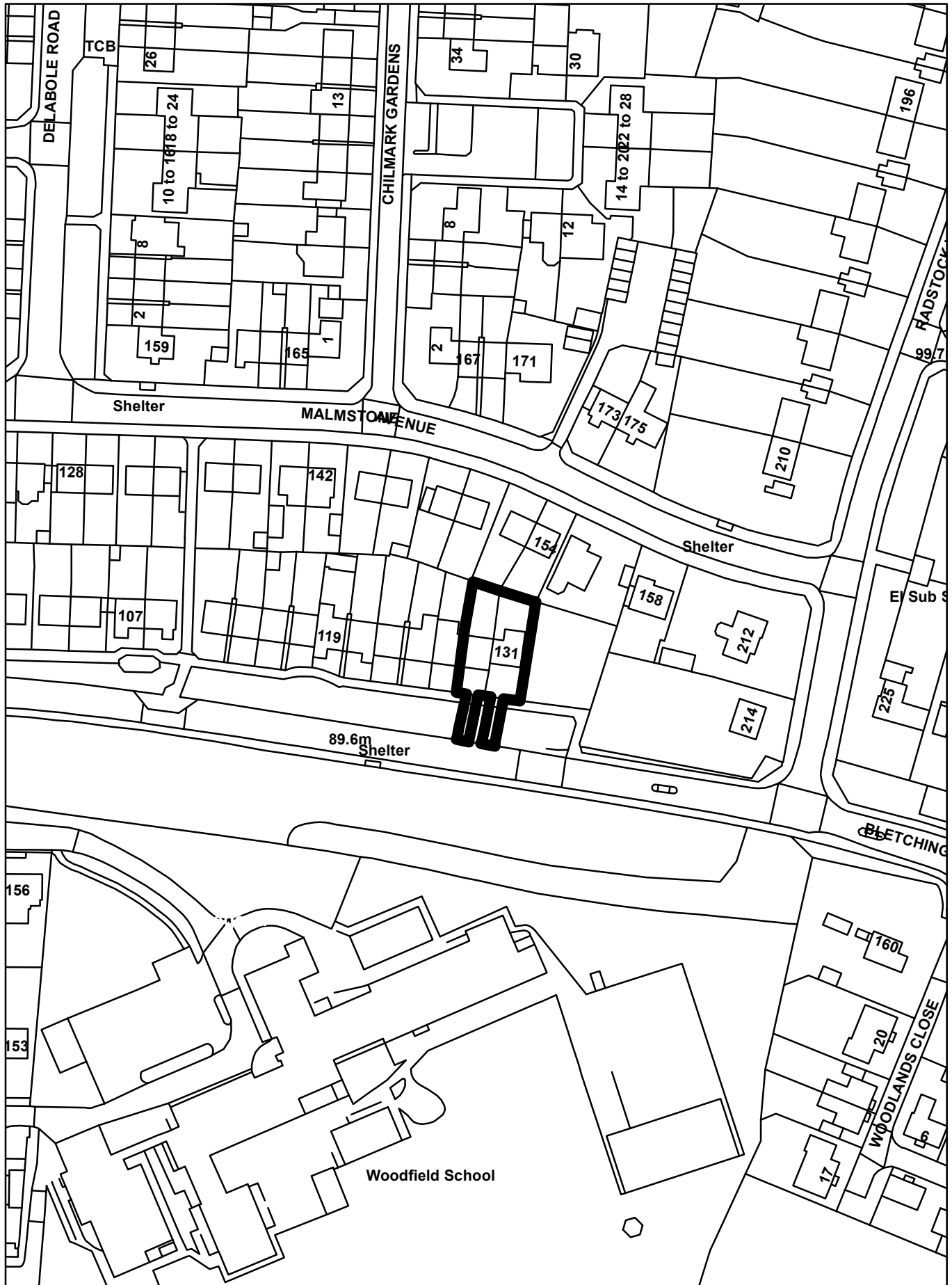
The development hereby permitted has been assessed against development plan policy DES1, TAP1 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.



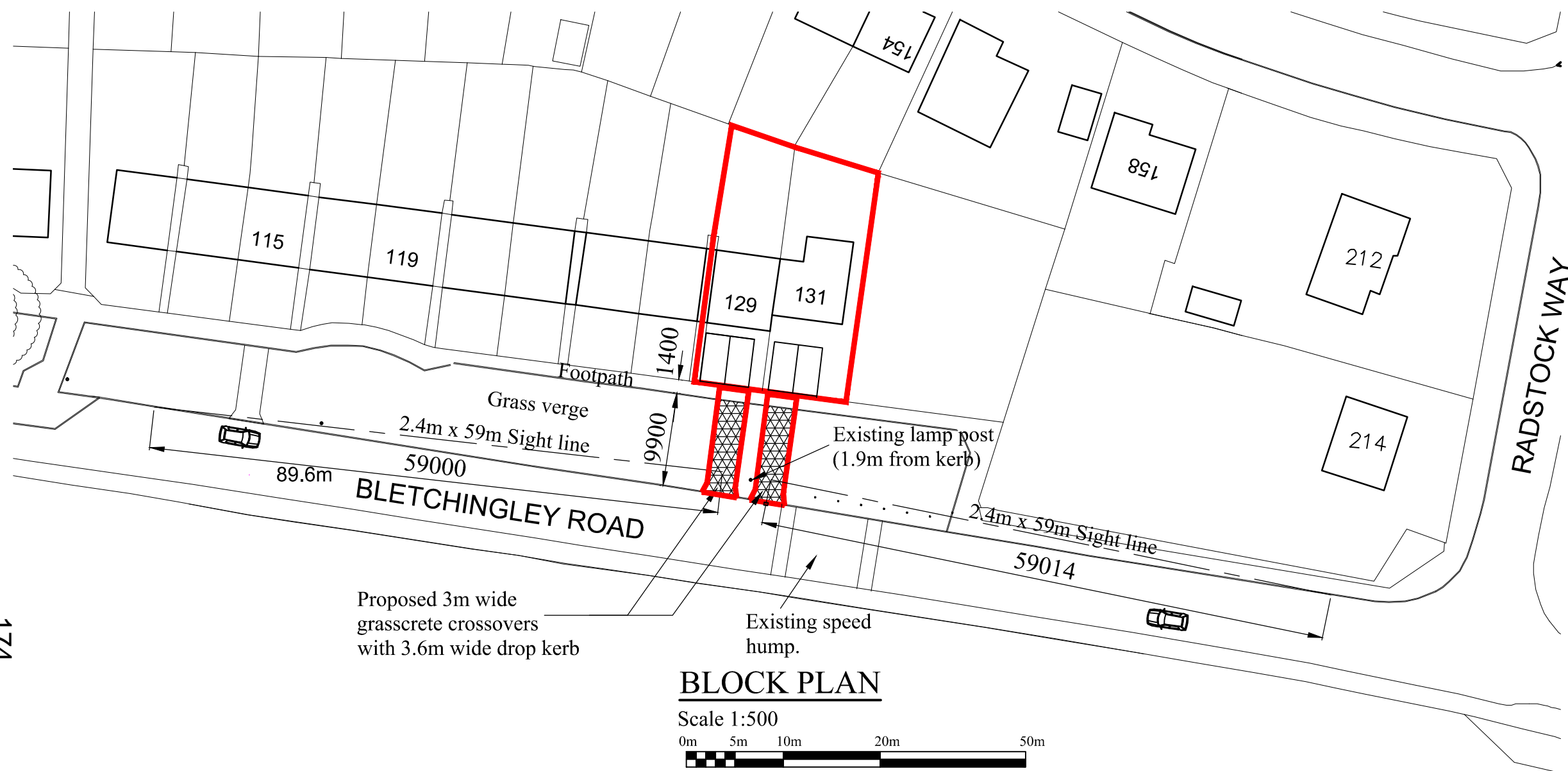
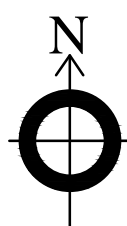
21/00050/F - 129 And 131 Bletchingley Road, Merstham

Agenda Item 10



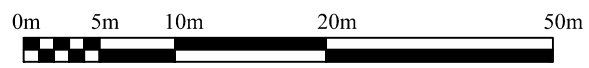
**General Notes.**

1. All concrete to have a minimum cube crushing strength of:  
 Mass Concrete = 25 N/mm<sup>2</sup>. at 28 days.  
 Reinforced Concrete = 35 N/mm<sup>2</sup>. at 28 days.  
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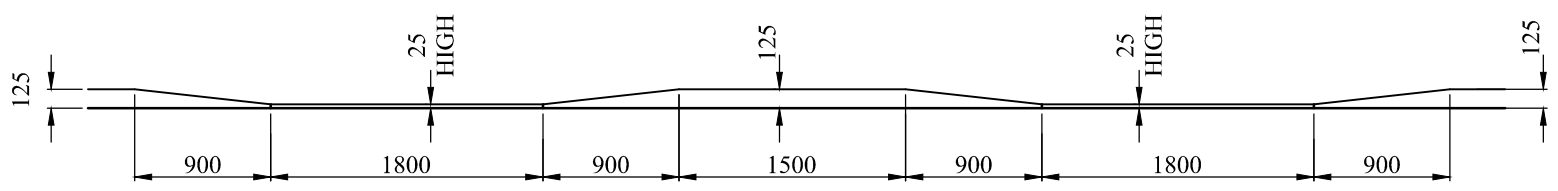


**BLOCK PLAN**

Scale 1:500



174

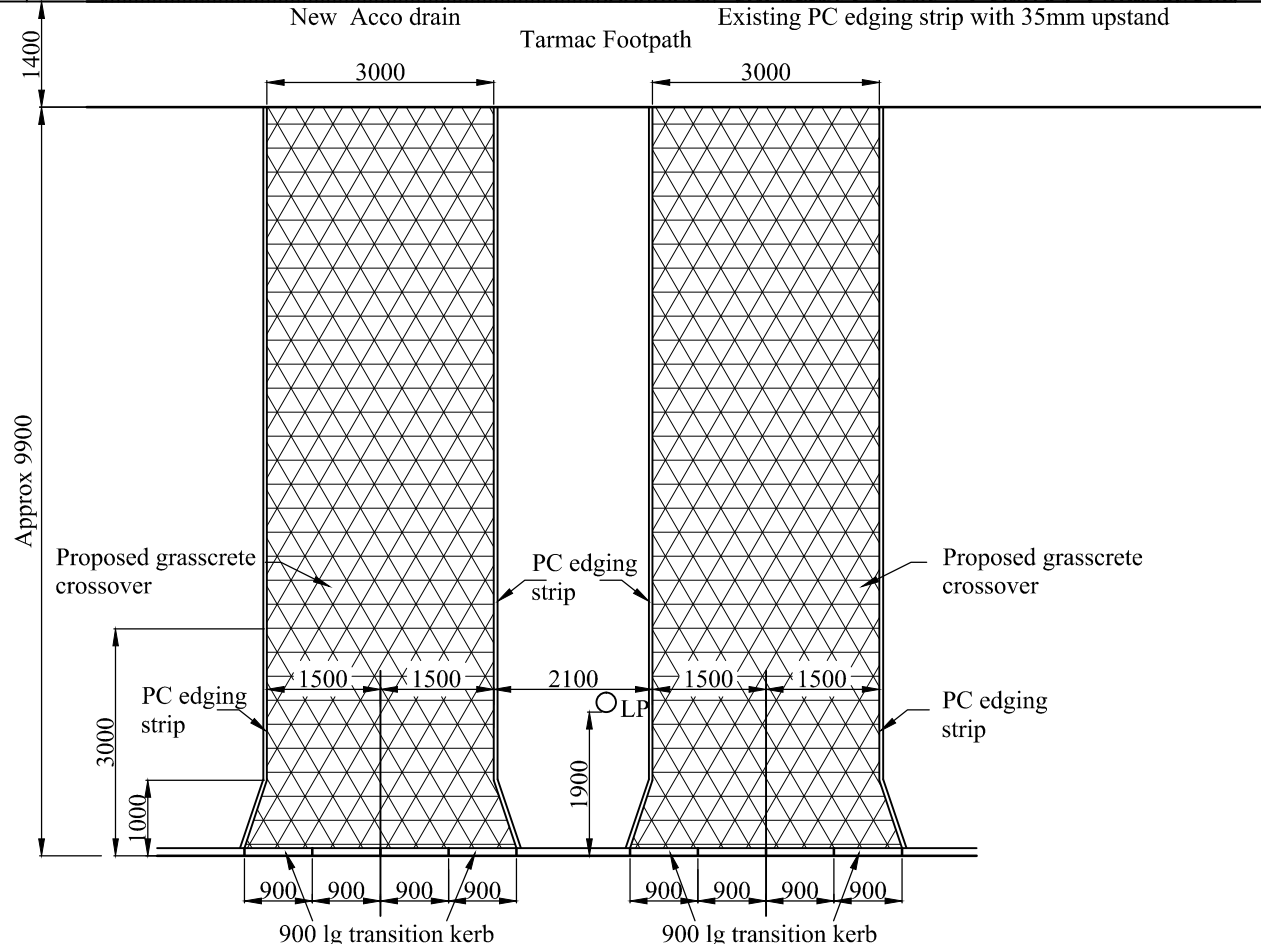
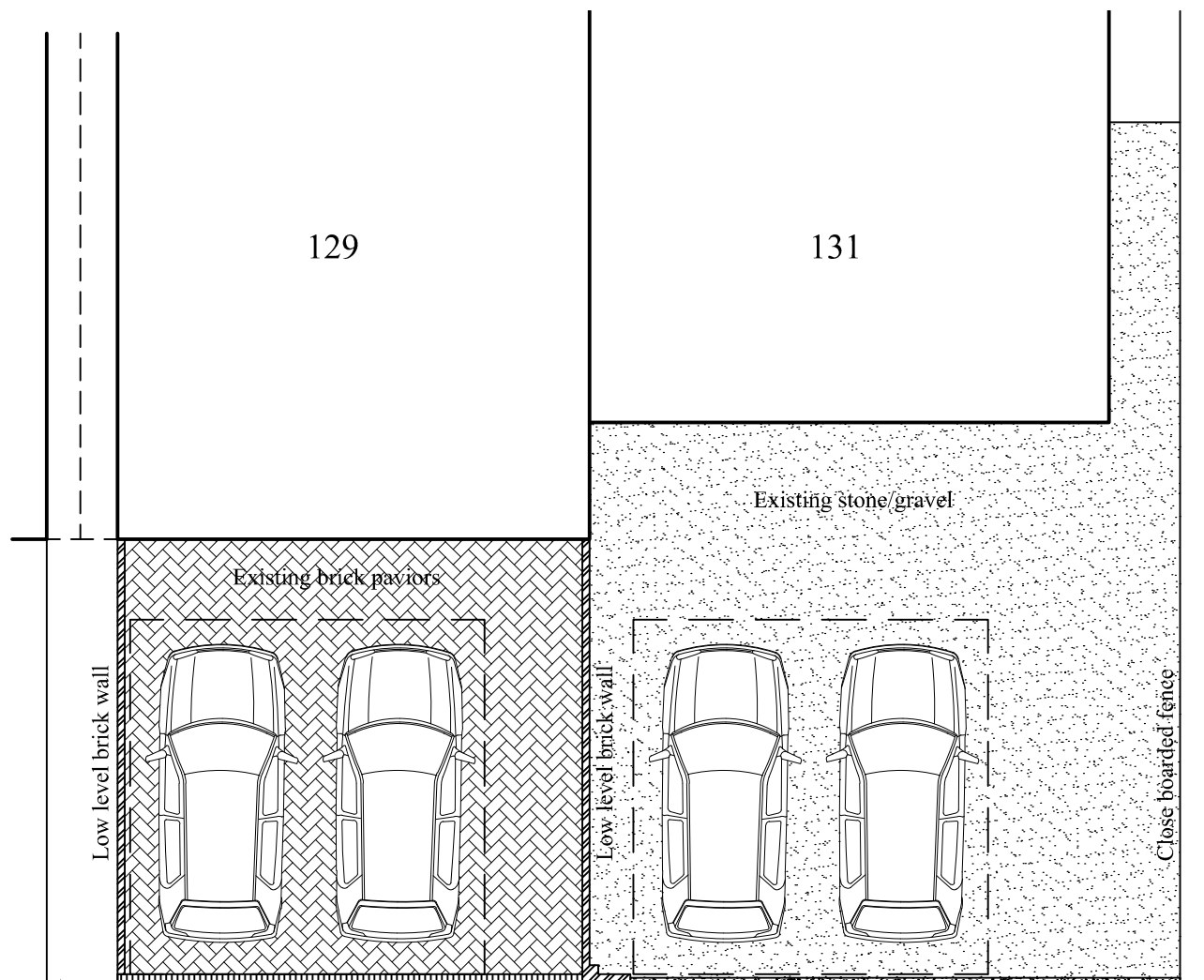


**ELEVATION ON DROPPED KERB**



A	08/02/2021	WIDTH OF CROSSOVERS ALTERED
REVISION DATE		
Drawing		
<b>PROPOSED VEHICULAR ACCESS HIGHWAY CROSSOVER</b>		
Project		
<b>129 AND 131 BLETCHINGLEY ROAD MERSTHAM SURREY</b>		
Client		
<b>MS UK PROPERTY LTD</b>		
Architect		
<b>Michael Blacker Partnership</b> <small>Michael S. Blacker CEng, F.I.S.T.R.U.C.E., Andrew Blacker B.Sc.(Hons) CEng, M.I.C.E.</small> <b>CONSULTING STRUCTURAL &amp; CIVIL ENGINEERS</b> <small>No1 MARK STREET, REIGATE, SURREY RH2 0BL          E-MAIL: engs@blacker.co.uk          TELEPHONE 01737 244886 FACSIMILE 01737 224556</small>		
<small>THE COPYRIGHT IN THIS DRAWING IS THE PROPERTY OF THE MICHAEL BLACKER PARTNERSHIP AND NOTHING IN THIS DRAWING OR ANY PART OF IT, OR ANY INFORMATION CONTAINED THEREIN, SHALL BE REPRODUCED OR TRANSMITTED IN ANY FORM OR BY ANY MEANS, WITHOUT THE WRITTEN PERMISSION OF THE MICHAEL BLACKER PARTNERSHIP.</small>		
Scale	Drawn	Checked
1:500 & 1:50 @ A3	GG	-
Date	Job No.	Rev
JAN 2021	4641	01 A

175



**PLAN ON VEHICLE CROSSOVER AND DROPPED KERB**

Scale 1:100 0m 1m 2m 3m 4m 5m



**EXISTING ELEVATION ON BACK OF FOOTPATH  
PROPOSED ELEVATION UNALTERED**

Scale 1:100 0m 1m 2m 3m 4m 5m

**General Notes.**

1. All concrete to have a minimum cube crushing strength of:  
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Reinforced Concrete = 35 N/mm<sup>2</sup>. at 28 days.  
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9. All work to be carried out to the approval of the local Authority District Surveyor or Building Inspector.

REVISION	DATE	DESCRIPTION
A	18/02/2021	WIDTH OF CROSSOVERS ALTERED
Drawing		
<b>PROPOSED VEHICULAR ACCESS HIGHWAY CROSSOVER</b>		
Project		
<b>129 AND 131 BLETHINGLEY ROAD MERSTHAM SURREY</b>		
Client		
<b>MS UK PROPERTY LTD</b>		
Architect		
<b>Michael Blacker</b>		
<small>Michael S. Blacker CEng F13StructE, Andrew Blacker BSc(Hons) CEng MICE, CONSULTING STRUCTURAL &amp; CIVIL ENGINEERS No 1 MARK STREET, REIGATE, SURREY RH2 0BL E-MAIL: engs@blacker.co.uk TELEPHONE 01737 248886 FACSIMILE 01737 224556</small>		
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Date	JAN 2021	Checked -
Job No.	4641	Rev
Drg. No.	02	A

Agenda Item 10

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# Agenda Item 11

Planning Committee  
14<sup>th</sup> April 2021

Agenda Item: 11  
20/02840/HHOLD

 <b>Reigate &amp; Banstead</b> BOROUGH COUNCIL Banstead   Horley   Redhill   Reigate	<b>TO:</b>	PLANNING COMMITTEE	
	<b>DATE:</b>	14 April 2021	
	<b>REPORT OF:</b>	HEAD OF PLANNING	
	<b>AUTHOR:</b>	Matthew Lambert	
	<b>TELEPHONE:</b>	01737 276659	
	<b>EMAIL:</b>	Matthew.Lambert@reigate-banstead.gov.uk	
<b>AGENDA ITEM:</b>	11	<b>WARD:</b>	Horley Central and South

<b>APPLICATION NUMBER:</b>	21/00388/HHOLD	<b>VALID:</b>	02 March 2021
<b>APPLICANT:</b>	Mrs Joanna Shipton	<b>AGENT:</b>	No details submitted
<b>LOCATION:</b>	<b>20 CHEYNE WALK, HORLEY, RH6 7PF</b>		
<b>DESCRIPTION:</b>	<b>Single storey flat roof extension onto detached bungalow, extension will span the whole width of the property and extend out to a maximum of 4 metres.</b>		
<b>All plans in this report have been reproduced, are not to scale, and are for illustrative purposes only. The original plans should be viewed/referenced for detail.</b>			

**This application is referred to Committee in accordance with the Constitution as the applicant is related to a Borough Councillor.**

## SUMMARY

The proposal seeks planning permission for the erection of a single-storey extension to the rear of the existing bungalow

The context of the area is residential, comprising wide, detached bungalows, set within rectangular plots. Some of these have benefitted from rear extensions, similar to the proposed.

The addition would be constructed out of red brickwork to match the existing brick type. It would have an insulated flat roof, which would adjoin the eaves of the roof pitch of the house, and it would feature UPVC fenestration to the rear. It would span the complete width of the rear of the bungalow. Owing to narrow boundaries of 0.9m on either side, there would be very limited overall visibility of the proposal from the streetscene of Cheyne Walk. The proposed depth from the rear wall would measure 4m. This is 0.5m greater than the Council's recommendation for this type of proposal, as set out within the Householder Extensions and Alterations Supplementary Planning Guidance. However, owing to the complementary design approach, limited visibility, and negligible impact upon the amenity of either neighbouring property, the proposal is considered to be acceptable.

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Planning Committee  
17<sup>th</sup> March 2021

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## **RECOMMENDATION**

Planning permission is **GRANTED** subject to conditions.



## Consultations:

Environmental Protection Officer: Recommends an informative owing to potential historic ground contamination

Horley Town Council: Raise no objections

## Representations:

Letters were sent to neighbouring properties on 04 March 2021. No representations have been received.

### 1.0 Site and Character Appraisal

- 1.1 The application site is a detached bungalow built in approximately the early 1960s. It is set in a rectangular shaped plot that is fairly flat throughout. There are no trees likely to be affected by the proposal.
- 1.2 The surrounding area consists of residential properties of a similar age and slightly varying styles; a number of properties have been extended, to the typically to the roof, and the rear.

### 2.0 Added Value

- 2.1 Improvements secured at the pre-application stage: None sought.
- 2.2 Further improvements could be secured: Materials to match existing.

### 3.0 Relevant Planning and Enforcement History

- 3.1 93/12390/F                      Single storey rear extension and conservatory                      Granted 29/12/1993  
(Permission not implemented)

### 4.0 Proposal and Design Approach

- 4.1 This is a full application for a single-storey rear extension to the dwelling. The addition would span the width of the existing dwelling. Internally, it would accommodate increased living space and additional bedrooms.
- 4.2 The proposed extension would be built out of matching materials and would be provided with a flat roof.

### 5.0 Policy Context

#### 5.1 Designation

Urban Area

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## 5.2 Reigate and Banstead Core Strategy

CS1 (Presumption in favour of sustainable development)

## 5.2 Reigate & Banstead Development Management Plan 2019

DES1 (Design of new development)

## 5.3 Other Material Considerations

National Planning Policy Framework

National Planning Policy Guidance

Supplementary Planning Guidance

Householder Extensions and  
Alterations

Other

Human Rights Act 1998

## 6.0 **Assessment**

6.1 The application site is situated within the urban area where there is a presumption in favour of sustainable development.

6.2 The main issues to consider are:

- Impact on local character
- Neighbour amenity

### Impact on local character

6.3 The Council's Development Management Plan Policy DES1 expects proposals to have due regard to the layout, density, plot sizes, building siting, scale, massing, height, and roofscapes of the surrounding area, the relationship to neighbouring buildings, and important views into and out of the site. The Householder Extensions and Alterations Supplementary Planning Guidance (SPG) 2004 states that single storey rear extensions should be proportionate to the size of the property and garden, they should comply with the depth guidance recommended for the property, type, and they should use the same roof design and where possible the same roof pitch as the main property,

6.4 The proposal is considered acceptable in terms of design. Owing to its positioning wholly to the rear of the dwelling, it would be highly concealed from the streetscene, with only partial visibility of either side flank visible from the narrow boundaries either side. This level of visibility would be acceptable, given the context of the area, where many neighbours have undertaken similar extensions. This is the case at the neighbour to the north, no.18 Cheyne Walk. Given the nature of the existing dwelling, and the plot in which it is situated, the proposal is also considered proportionate in size.

- 6.5 The proposal would not feature a roof of the same pitch of the dwelling, it would also fail to accord with the recommended depth guidelines for this type of proposal, exceeding them by 0.5m. However, given the overall low visibility as discussed in the section above, these aspects are not considered to result in harm to the broader character of the locality. Following the materials condition worded in the next section of this report, the proposal is considered to be acceptable with regard to design and character.

### Neighbour amenity

- 6.6 Both the council's Householder Extensions and Alterations SPG in addition to Policy DES1 of the Development Management Plan expect any proposal to have due regard to the amenity of neighbouring properties. The key residential amenity to consider in this instance would be the detached neighbours to the north, and to the south. These are no.18 Cheyne Walk, and no.22 Cheyne Walk respectively.
- 6.7 The front building line of no.18 is set roughly 0.70m forward of that of no.20. This neighbour has benefitted from a similar flat-roof single-storey rear extension, approved under application 88/17410/F. This extension has a depth of 3.65m. Taking this into account, the proposal at no.20 would project a distance of around 1.05m beyond the rear extension at no.18. Whilst this would give rise to a change in relationship between the two sites, there is not considered to be an onset of an overbearing impact, nor any loss of light or outlook to this neighbour as a direct result of the proposal. The proposal, nor the extension at no.18 on its southern side feature side facing windows. As such, there is neither considered to be any overlooking or loss of privacy impact.
- 6.8 The impact toward no.22 has the potential to be greater. The Council's Householder Extensions and Alterations SPG under section 5.2.2 recommends that single storey rear extensions situated astride a boundary should not exceed 3.5m in depth. As previously noted, the extension would not lie directly astride the boundary; with a spacing of 0.9m, but the overall depth of 4.0m would be considerable. Section 4.4 of the SPG recommends that in cases when the affected property is to the side of a new building, the 45° assessment should be undertaken. If the relevant 45° line were to intersect the centre of any rear windows serving a primary outlook to no.22, the impact on this side would be overbearing. On the north-eastern side of no.22; the closest to the proposal, lies a glazed conservatory, with no planning history available. The horizontal 45° line when taken from the north-westernmost corner of the proposed extension would directly intersect this conservatory, however the intersection would not occur to the centre. Therefore, in this instance the impact would not be overbearing. The proposal would not feature any side-facing windows, facing no.22. As such, there is neither considered to be any overlooking or loss of privacy impact on this side.
- 6.9 It is not considered that the proposal, owing to spacing of 34m, would have any impact on the neighbours to the rear. In light of this, the development

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would be acceptable with regard to residential amenity, in line with policy DES1 and the Householder Extensions and Alterations SPG.

## CONDITIONS

1. The development hereby permitted shall be carried out in accordance with the following approved plans.

Reason: To define the permission and ensure the development is carried out in accord with the approved plans and in accordance with National Planning Practice Guidance.

Note: Should alterations or amendments be required to the approved plans, it will be necessary to apply either under Section 96A of the Town and Country Planning Act 1990 for non-material alterations or Section 73 of the Act for minor material alterations. An application must be made using the standard application forms and you should consult with us, to establish the correct type of application to be made.

Plan Type	Reference	Version	Date Received
Site Plan	TQRQM21028214327212		17.02.2021
Proposed Development Plan	TQRQM21028214327212		02.03.2021
Existing Floor Plan	4631 01		17.02.2021
Existing Elevations	4631 02		17.02.2021
Site Plan Showing Tree Locations	TQRQM21028214327212		17.02.2021
Location Plan	TQRQM21028220320478		17.02.2021
Proposed Floor Plans	4631 03		17.02.2021
Proposed Elevations	4631 04		17.02.2021

2. The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Reason: To comply with Section 91(1) of the Town and Country Planning Act 1990 as amended by Section 51 (1) of the Planning and Compulsory Purchase Act 2004

3. The materials to be used in the construction of the external surfaces of the extension (other than materials used in the construction of a conservatory) must be of similar appearance to those used in the in the construction of the exterior of the existing building.

Reason: To ensure that the development hereby permitted is only constructed using the appropriate external facing materials or suitable alternatives in the interest of the visual amenities of the area with regard to Reigate and Banstead Development Management Plan 2019 policy DES1.

## INFORMATIVES

1. Your attention is drawn to the safety benefits of installing sprinkler systems as an integral part of new development. Further information is available at [www.firesprinklers.org.uk](http://www.firesprinklers.org.uk).
2. The applicant is encouraged to provide renewable technology within the development hereby permitted in order to reduce greenhouse gas emissions. Further information can be found on the Council website at : [Climate Change Information](#).

## REASON FOR PERMISSION

The development hereby permitted has been assessed against development plan policy DES1 and material considerations, including third party representations. It has been concluded that the development is in accordance with the development plan and there are no material considerations that justify refusal in the public interest.

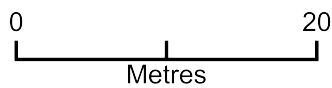
The Local Planning Authority has acted positively and proactively in determining this application by assessing the proposal against all material considerations, including planning policies and any representations that may have been received and subsequently determining to grant planning permission in accordance with the presumption in favour of sustainable development where possible, as set out within the National Planning Policy Framework.

Agenda Item 11  
21/00388/HHOLD - 20 Cheyne Walk, Horley





## Proposed Development Plan 20 Cheyne Walk RH6 7PF

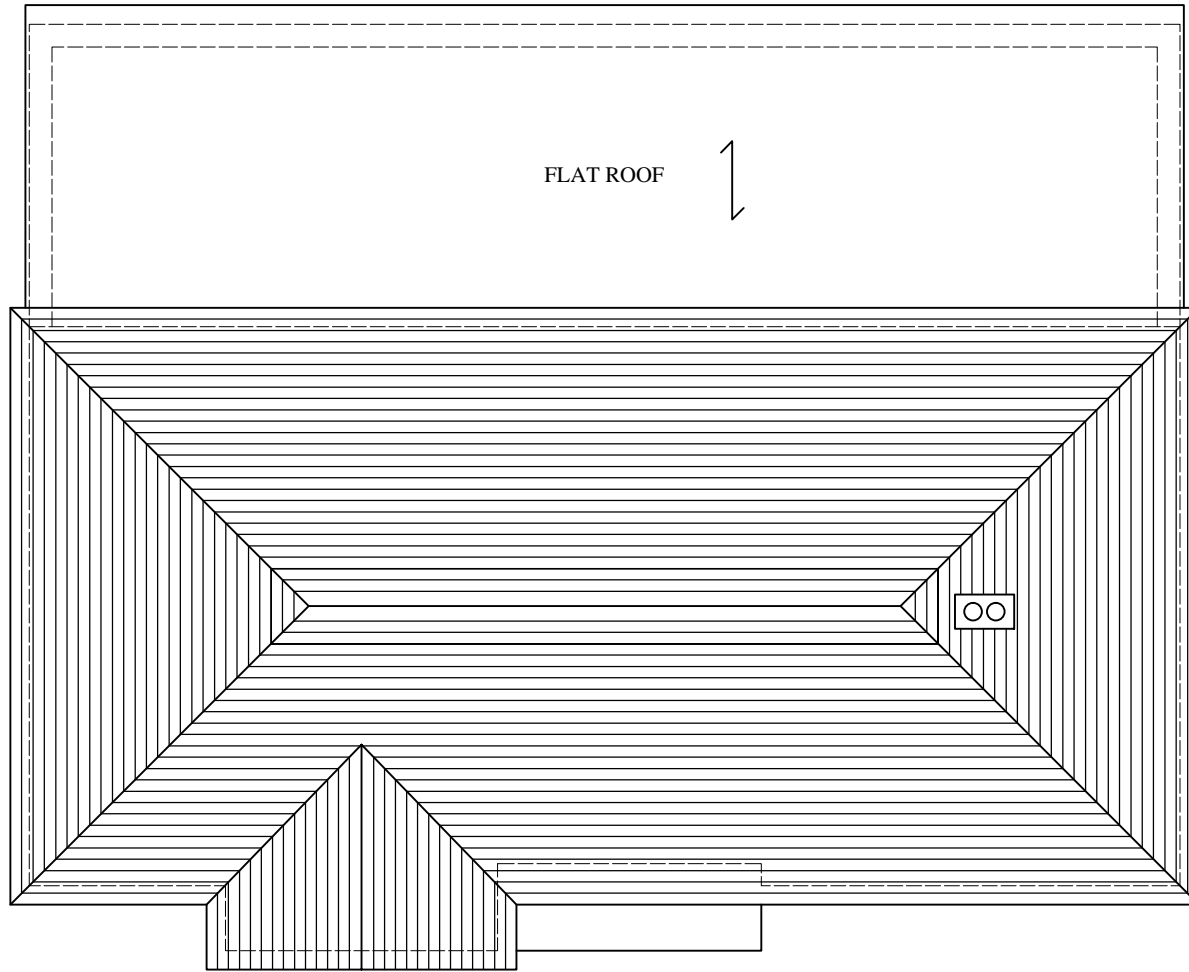


Plan Produced for: Joanna Shipton

Date Produced: 28 Jan 2021

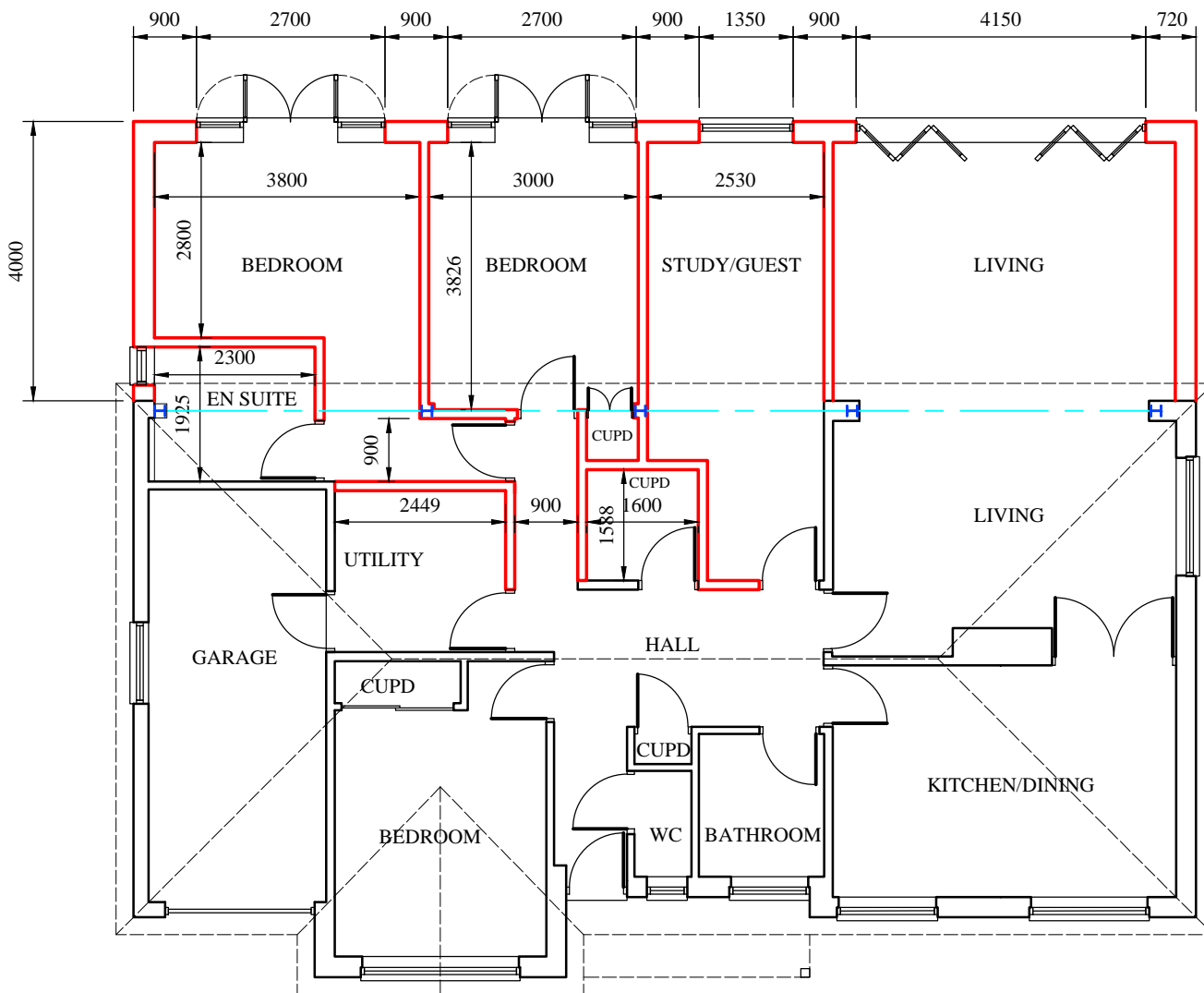
Plan Reference Number: TQRQM21028214327212

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FLAT ROOF

ROOF PLAN



ALTERNATIVE No5 GROUND FLOOR PLAN



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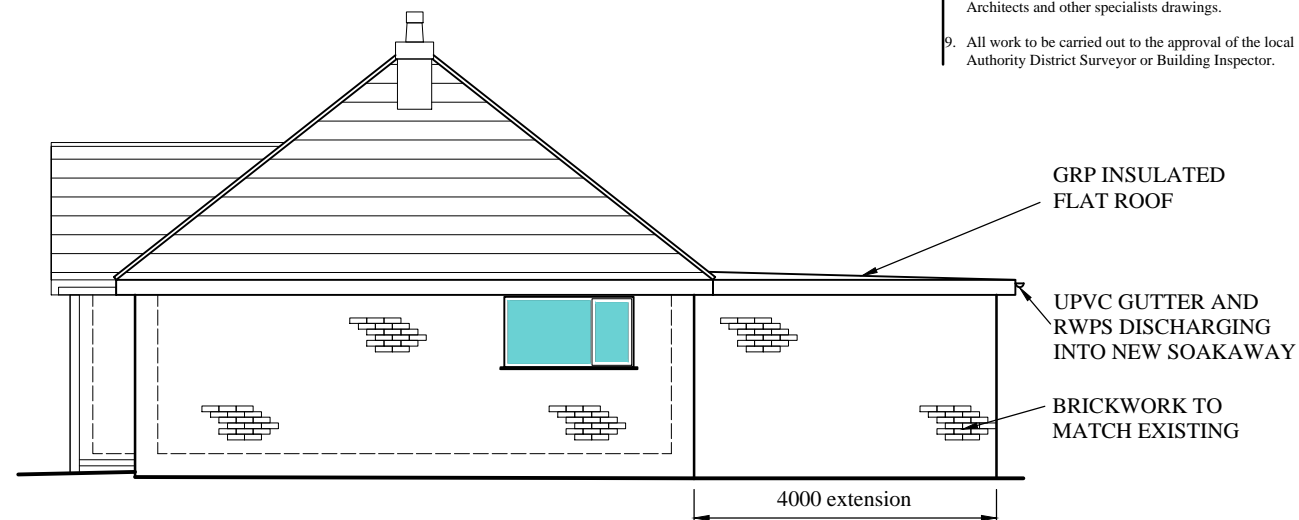
REV	DATE	REAR OPENINGS TO EXTENSION ALTERED
Drawing		
<b>PROPOSED FLOOR PLANS</b>		
Project		
<b>20 CHEYNE WALK HORLEY</b>		
Client		
<b>MR &amp; MRS SHIPTON</b>		
Architect		
<b>Michael Blacker Partnership</b>		
<small>Michael S. Blacker C.Eng. F.I.Struct.E. Andrew Blacker B.Sc.(Hons) C.Eng. M.I.C.E.</small>		
<b>CONSULTING STRUCTURAL &amp; CIVIL ENGINEERS</b>		
<small>No1 MARK STREET, REKATE, SURREY RH2 0BL. E - MAIL <a href="mailto:eng@blacker.co.uk">eng@blacker.co.uk</a> TELEPHONE 01737 244886 FACSIMILE 01737 224556</small>		
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Scale	1:100 @ A3	Drawn GRC
Date	NOV 2020	Job No. 4631
		Dwg. No. 03
		Rev. A

**General Notes.**

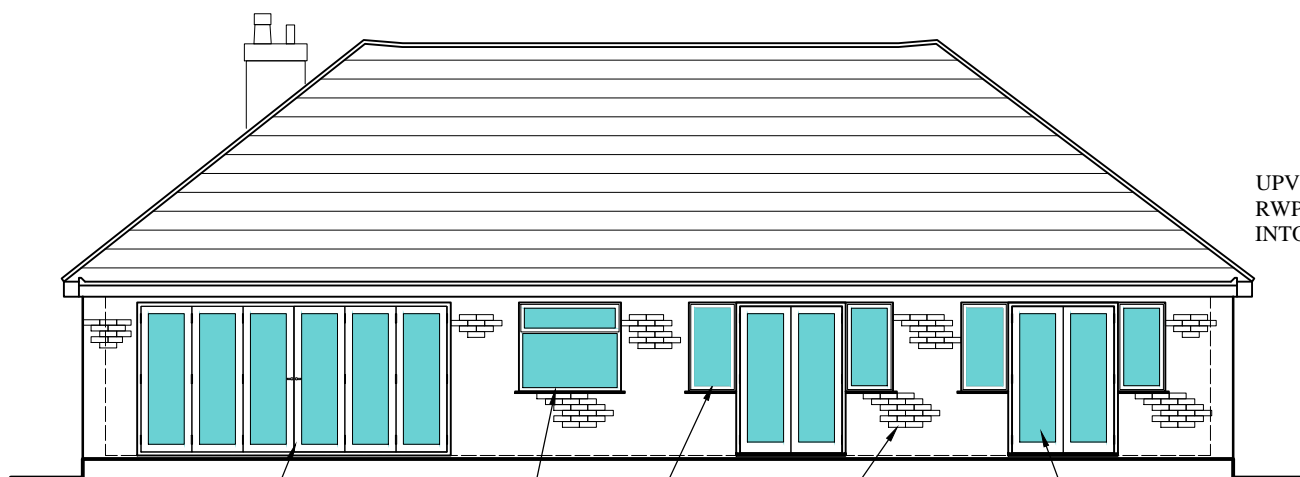
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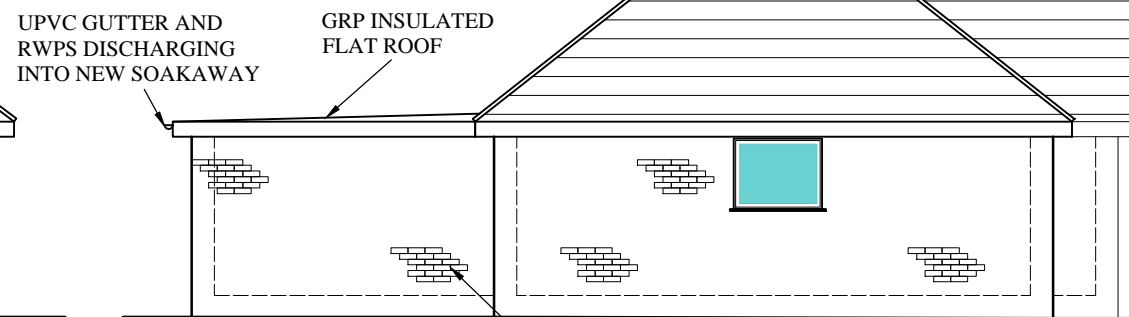
**FRONT ELEVATION  
UNALTERED**



**RIGHT HAND SIDE ELEVATION**



**REAR ELEVATION**



**LEFT HAND SIDE ELEVATION**

REV	DATE	DESCRIPTION
A	12:02:21	OPENINGS IN REAR ELEVATION ALTERED
<b>PROPOSED ELEVATIONS</b>		
<b>Project</b>		
20 CHEYNE WALK HORLEY		
<b>Client</b>		
MR & MRS SHIPTON		
<b>Architect</b>		
Michael Blacker Partnership		
CONSULTING STRUCTURAL & CIVIL ENGINEERS		
No 1 MARK STREET, REIGATE, SURREY RH2 0BL		
E - MAIL engs@blacker.co.uk		
TELEPHONE 01737 244886 FACSIMILE 01737 224566		
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Scale	1:100 @ A3	Drawn GRC
Date	NOV 2020	Job No. 4631
		Dwg. No. 04
		Rev. A



Agenda Item 1

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